



## **Town of Arlington, MA Redevelopment Board**

### **Agenda & Meeting Notice February 24, 2025**

Per Board Rules and Regulations, public comments will be accepted during the public comment periods designated on the agenda. Written comments may be provided by email to [cricker@town.arlington.ma.us](mailto:cricker@town.arlington.ma.us) by Monday, February 24, 2025, at 3:00 pm. The Board requests that correspondence that includes visual information should be provided by Monday, February 24, 2025, at 10:00 am. Please note that all times are estimates; individual agenda items may occur earlier or later than the time noted.

The Arlington Redevelopment Board will meet Monday, February 24, 2025 at 7:30 PM in the **Arlington Community Center, Main Hall, 27 Maple Street, Arlington, MA 02476**

#### **1. Review Meeting Minutes**

7:30 pm      The Board will review and vote on meeting minutes from January 27, 2025.

#### **2. Public Hearing: Docket #3838, 15 Ryder St**

7:35 pm      Notice is herewith given that an application has been filed on January 27, 2025, by Thomas Allen, ABC Beer Inc, dba Arlington Brewing Company, 251 Lowell St, Arlington, MA, 02474, to open Special Permit Docket #3838 in accordance with the provisions of MGL Chapter 40A § 11, and the Town of Arlington Zoning Bylaw Sections 3.3, Special Permits, and 3.4, Environmental Design Review. The applicant proposes a change of use to a brewery and restaurant including a taproom, as well as renovation of a portion of the property located at 15 Ryder St, Arlington, MA, in the I Industrial District. The opening of the Docket is to allow the Board to review and approve the application under Section 3.4, Environmental Design Review.

- Applicant will be provided 10 minutes for an introductory presentation.
- DPCD staff will be provided 5 minutes for an overview of their Public Hearing Memorandum.
- Members of the public will be provided time to comment.
- Board members will discuss Docket and may vote.

#### **3. Public Hearing: Warrant Articles for 2025 Annual Town Meeting**

8:20 pm      The Board will hear the proposed zoning amendments. The public hearing will include time both for public comment and for deliberation and discussion by the Board.

### **ARTICLE 28 ZONING BYLAW AMENDMENT / DEFINITION OF LOT COVERAGE**

To see if the Town will vote to amend Section 2: Definitions, of the Zoning Bylaw, to add a new definition, Lot Coverage; or take any action related thereto.

**ARTICLE 29**  
**ZONING BYLAW AMENDMENT / PARKING IN RESIDENTIAL DISTRICTS**

To see if the Town will vote to amend Section 6.1.10.A of the Zoning Bylaw, to amend and clarify standards for the location of parking in Residential districts; or take any action related thereto.

**ARTICLE 30**  
**ZONING BYLAW AMENDMENT / SCREENING AND BUFFER REQUIREMENTS**

To see if the Town will vote to remove duplicate material by combining Section 5.3.7 and 5.3.21 of the Zoning Bylaw into Section 5.3.7 and thus deleting Section 5.3.21, revise Section 5.3.7 for clarity, move a subsection to Section 5.3.13, and amend a reference in Section 5.6.2; or take any action related thereto.

**ARTICLE 31**  
**ZONING BYLAW AMENDMENT / REDEVELOPMENT BOARD JURISDICTION**

To see if the Town will vote to amend Section 3.4.2 of the Zoning Bylaw, to revise the special permit and environmental design review applicability for certain properties abutting the Minuteman Bikeway; or take any action related thereto.

**4. Discussion of B1 Rezoning**

9:15 pm      The Board will discuss the B1 Rezoning warrant article.

**5. Open Forum**

9:30 pm      Except in unusual circumstances, any matter presented for consideration of the Board shall neither be acted upon, nor a decision made, the night of the presentation. There is a three-minute time limit to present a concern or request.

**6. New Business**

9:45 pm

**7. Adjourn**

9:45 pm      (Estimated)

**8. Correspondence**

15 Ryder St:

- R. Mascarenhas, 2/10/2025
- D. King, 2/11/2025
- S. Mohan, 2/17/2025
- Arlington Eats, 2/18/2025
- C. Klein, 2/18/2025



- Zoning Board of Appeals, 2/18/2025
- M. Adamik, 2/19/2025
- T. Brennan, 2/19/2025
- D. Buske, 2/19/2025
- E. Campbell, 2/19/2025
- K. Depardieu, 2/19/2025
- C. Hartshorne, 2/19/2025
- S. Huber, 2/19/2025
- P. & A. Juodawlkis, 2/19/2025
- L. Marshall, 2/19/2025
- K. Moon, 2/19/2025
- M. Plamondon, 2/19/2025
- B. Sandrof, 2/19/2025
- G. Smith, 2/19/2025
- E. Sohn, 2/19/2025
- V. Stabinger, 2/19/2025
- H. Tatlock, 2/19/2025
- J. Winston, 2/19/2025
- P. Smith, 2/20/2025
- Residents of Beck & Ryder - 2/21/2025
- D. Seltzer - 2/22/2025
- C. Campbell - 2/23/2025
- L. Gitelson - 2/23/2025
- K. Lawrence - 2/23/2025
- L. Magoun - 2/23/2025
- M. Basham - 2/24/2025
- D. Brownell - 2/24/2025
- M. Contreras - 2/24/2025
- N. Green - 2/24/2025

Warrant Article Hearings:

- L. Wiener, 2/11/2025
- M. Marx, 2/14/2025



## Town of Arlington, Massachusetts

---

### Review Meeting Minutes

#### Summary:

7:30 pm            The Board will review and vote on meeting minutes from January 27, 2025.

#### ATTACHMENTS:

Type	File Name	Description
▢ Meeting Minute (draft)	02102025_DRAFT_Minutes_Redevelopment_Board.pdf	02102025 DRAFT Minutes Redevelopment Board

**Arlington Redevelopment Board**  
**Monday, February 10, 2025, at 7:30 PM**  
**Community Center, Main Hall**  
**27 Maple Street, Arlington, MA 02476**  
**Meeting Minutes**

This meeting was recorded by ACMi.

**PRESENT:** Rachel Zsebery (Chair), Eugene Benson, Shaina Korman-Houston, Kin Lau, Stephen Revilak

**STAFF:** Claire Ricker, Director of Planning and Community Development; Sarah Suarez, Assistant Director of Planning and Community Development

---

The Chair called the meeting of the Board to order.

The Chair opened with **Agenda Item 1 – Review Meeting Minutes.**

January 27, 2025, minutes – The Board members made no changes to the draft minutes. The Chair requested a motion to approve the minutes as submitted. Mr. Lau so moved, Mr. Benson seconded, and the Board voted unanimously in favor.

The Chair moved to **Agenda Item 2 – Discussion of Affordable Housing Overlay District Warrant Article.**

The Chair clarified that this is not a public hearing, but a working session with the citizen petitioners. She noted that this article was presented at the last meeting and asked the petitioners if they had anything to add or highlight. Sanjay Newton, Town Meeting Member and member of the citizen working group, and Erica Schwarz, Executive Director of the Housing Corporation of Arlington (HCA), were in attendance. Mr. Newton explained that there have been three projects in Arlington in the last fifteen years, all developed by HCA, which would have been able to be built by right under this amendment to the Zoning Bylaw. They all included Town funding but were largely funded by state and federal subsidies.

Mr. Lau noted that what would be allowed under this amendment is similar to what is allowed under 40B. He asked Ms. Schwarz what would be the advantage to a developer of the proposed amendment versus using 40B. Ms. Schwarz replied that it would save time and money by providing more clarity and less risk about the approvals process. She noted that although HCA has been able to develop in Arlington, other affordable housing developers are not coming to Arlington, at least in part because the 40B process is lengthy and uncertain, and there is no more direct path to zoning approval.

Mr. Lau asked if other towns or cities have implemented something similar to this proposed amendment. Mr. Newton replied that Cambridge has an affordable housing overlay, and they have had a lot of success in developing affordable housing as a result. The working group spent a lot of time looking at what Cambridge has done to develop their proposal, although they modified it significantly to fit the context of Arlington. Mr. Newton said that he believes that Somerville also has an affordable housing overlay, but it is newer so has had less development under it.

The Chair noted that Cambridge and Somerville are cities and much larger than Arlington, and she is not sure that their model makes sense in a town on the scale of Arlington. Her biggest concern is the scale of this proposal running across the entire town. She is specifically concerned about the business and industrial districts. Through all the work done on MBTA Communities, the Board has heard from the town that business districts need to be protected and enhanced. She is also concerned about allowing single room occupancy buildings, which are not currently allowed in Arlington, as well as the range of projects that would be allowed in every residential district. This proposal contradicts other areas of the Zoning Bylaw, such as the fact that the ability to build new single- and two-family dwellings in the business districts has been eliminated. Mr. Newton said that their intention was not to allow new single- and two-family dwellings in any areas where they are not already allowed, so if the draft does not clearly say that, it can be changed.

The Chair also expressed concern about the proposed heights allowed. She thinks that they are too aggressive. The Board spent a lot of time thinking about the heights that should be allowed in the Neighborhood Multi-Family Housing Overlay District, and this proposal exceeds those allowed heights.

Ms. Korman-Houston said that she is generally comfortable with the scope of the proposal, but she has a number of specific questions. She noted that in the chart showing allowable heights, it appears that larger lots require less height. Mr. Newton said that heights are based on the underlying zoning district, with an additional two stories allowed, except in R0, R1, and R2, where only one additional story is allowed. Ms. Korman-Houston also noted that it appears that a development could have a side yard setback of zero in residential districts, which she thinks is problematic. Mr. Newton said that was not their intention. Ms. Korman-Houston said that she thinks it is appropriate to take a liberal approach to both setbacks and open space, while respecting the surrounding district. She said that it appears that dormers would not be allowed as a way of getting one additional story, and she would suggest that they revisit that, to allow for the maximum amount of housing while respecting the neighborhood. She also noted that the state is now requiring increased sustainability for affordable housing projects with public money, so allowances need to be made for the physical design of buildings in order to accommodate sustainability requirements. She thinks that having a clear path to approval of affordable housing is an important goal.

Mr. Benson said that this proposal is the result of two years of work, but the Board has only had a few days to read and consider it. He likes the concept of using zoning to make it easier to create affordable housing, but this proposal goes too far and takes too much out of the zoning bylaw. He thinks that it can be scaled back in many ways and still provide more opportunities for the development of affordable housing than currently exist. He thinks that the allowable heights should match what was allowed in the Multi-Family Housing Overlay Districts created by the MBTA Communities process; those heights were extensively discussed and agreed to by Town Meeting. He is also concerned about allowing this in the Industrial district. He likes the idea of allowing affordable housing in the business districts if it is part of mixed-use development; the current proposal only requires mixed-use development in the B3 and B5 districts, but he would prefer to expand that requirement to B2, B2A, and B4 as well. He does not think it is a good idea to say that a community service facility makes something a mixed-use building. A community service facility is largely for residents of the building; even if it is open to others in the public, it would not activate the streetscape in the way that a retail establishment or restaurant would. He disagrees with the elimination of the traffic visibility requirement that allows drivers and pedestrians to see around corners in residential neighborhoods. He is also concerned about getting rid of lot frontage requirements. He noted that this proposal states that the current section of the bylaw regarding affordable housing will not apply to the overlay district, but he thinks some of it should be retained, in order to give the Town a little more oversight. He thinks that it makes sense to reduce parking requirements on Mass Ave and Broadway, but not in other areas with limited access to transit. He also noted that the proposal does not say anything about shadows and what would happen if one of these buildings blocks sunlight to a neighboring building's solar array. He also thinks that a development under this proposal should require a Special Permit and not be as of right. Without requiring a Special Permit, the Board loses the ability to review and consider questions about shadows, signs, parking, neighborhood context, etc. MBTA Communities had to be as of right per state law; that requirement was because many communities use Special Permit requirements to keep projects out, but this Board has not done so.

Mr. Revilak said that he followed Cambridge's affordable housing overlay process closely. In the first year, it permitted about 350 units. He asked the petitioners how funding works in mixed-use buildings. Ms. Schwarz said it is tricky to do mixed-use, because the types of funding available would not support non-residential space. Affordable housing developers typically do not do commercial space, and a project would face difficulties if it had a commercial space that could not be filled.

The Chair said that the Board will not take on this article. The articles proposed by other groups that the Board took on were proposed and discussed well ahead of the closing of the warrant. Mr. Newton said that the bylaw would require the citizen working group to notify every property owner in town, which they cannot afford to do. The Chair said that she understood that, but the Board has not had sufficient time to consider this proposal and come to agreement and be willing to move it forward as a Board article.

The Chair moved to **Agenda Item 3 – Discussion of Board Articles for 2025 Town Meeting Warrant Articles.**

The Chair clarified that this is not a hearing, but a working session to discuss warrant articles submitted by the Board.

Ms. Ricker explained that DPCD staff has put together main motion language for each of the articles that the Board is advancing. Main motion language must be available to the public on Thursday, February 13, 2025.

The Chair asked if the Town Moderator has confirmed the date by which the Board Report is due. If it is earlier than expected, the Board may need to add in an additional meeting to approve the Report.

Mr. Benson expressed concern about hearing all nine citizen petitions on March 17. Ms. Ricker clarified that a few of the citizen petitions are scheduled for March 24, which is the meeting set aside for maps that affect the Zoning Map, leaving only seven articles for March 17.

The Chair opened discussion on the main motion language for each of the Board's articles.

#### **ARTICLE A, Accessory Dwelling Units (ADUs)**

Mr. Benson said he does not think that it is necessary to modify Section 2 Definitions at all. The only proposed change was to add the phrase "as defined in M.G.L. ch. 40A," which is unnecessary and adds confusion. The other Board members agreed.

Mr. Benson also recommended removing the exception proposed to Section 5.10.2.B.(1).e, because it is contradicted elsewhere in the bylaw. The exception refers to the state law about when a second ADU can be allowed on a property, but Arlington's bylaw only allows one ADU per property. The exception is unnecessary unless the Board wants to amend the bylaw to allow for a second ADU. Mr. Revilak noted that a community could allow multiple ADUs per dwelling, and he would like to see a second ADU allowed by special permit. The Chair noted that the Board's intent was to bring the Zoning Bylaw into compliance with state law, not to change what was previously allowed. Mr. Lau said that if the Board wants to allow a second ADU per dwelling, it should be a separate warrant article, not included in this one. Ms. Korman-Houston would like to consider allowing multiple ADUs at some point in the future, but she agreed that it is not what the Board is doing at this time, with this warrant article.

Mr. Revilak has been in communication with Ms. Suarez about other changes proposed to come into compliance with state law. One of those was the question of whether the bylaw can continue to require a special permit for an ADU conversion with 6 feet of a lot line, given that state law does not allow a special permit requirement for ADUs in general. Town Counsel's interpretation was that requiring a special permit only for ADUs within six feet of a lot line is a reasonable restriction and is allowable under state law.

#### **ARTICLE B, Transportation Demand Management Plan**

Ms. Ricker said that she looked at Section 6.1.5, regarding the Transportation Demand Management (TDM) parking requirement reduction in certain zones, which currently requires all projects to use at least three of the listed TDM methods in order to reduce parking requirements. Research indicated that smaller projects, with less than ten residential units or less than 3,000 square feet, might have difficulty using three TDM methods, so the bylaw could be modified to more appropriately require only two TDM methods for such projects. She also proposed adding an additional TDM method – being located with 600 feet of a transit stop.

Mr. Benson suggested a number of changes for clarity. Research indicates that the largest percentage of people are willing to walk up to a quarter mile to get to a transit stop, so he proposed including a provision that any project requesting a parking reduction with a TDM must be located within a quarter mile of a transit stop. He also proposed changing TDM method 9 to be within 660 feet (which is one-eighth of a mile) of a transit stop with service at least every 30 minutes, 7 days a week, between 6:00 am and 10:00 pm. As of now, the 77 bus is the only bus serving Arlington that would meet those requirements. That method would not be required, but it would be one of the TDM options available.

Mr. Revilak noted that the Board has approved TDM plans with methods not included in the bylaw, and he would be interested in including offering charging facilities for electric bicycles. He has mixed feelings about predicated TDM

on the availability of a bus line. The goal is to reduce reliance on single-occupancy vehicles, but transit is not the only way to do that. Stating that only projects within a quarter-mile of a bus stop can use a TDM to reduce parking means that projects further from a bus line that offer options that increase the likelihood of bicycle use could not even submit a TDM. He would like to change TDM method 9 to be within a quarter-mile of a bus stop, and he would like to eliminate the frequency requirement. Making that change would allow projects on Broadway to use method 9, as the 87 bus is not quite frequent enough to meet the requirement proposed by Mr. Benson.

Ms. Korman-Houston agreed with Mr. Revilak about dropping the quarter-mile requirement to submit a TDM. She proposed changing method 9 by eliminating the frequency requirement but keeping the 7 days per week requirement proposed by Mr. Benson.

Mr. Lau asked what exactly method 4, "Provide a guaranteed emergency ride home," and method 5 "Provide transit pass subsidies," mean. Ms. Korman-Houston replied that transit pass subsidies are any amount of money that an employer provides towards the cost of an employee's transit pass. She agreed that method 4 needs clarification.

The Chair agreed with Mr. Revilak and Ms. Korman-Houston's proposed changes.

Mr. Benson agreed to drop the requirement that only projects within a quarter-mile of a bus stop can use a TDM to reduce parking requirements. He noted that his proposal for TDM method 9 specifies that it is for projects with residential units, because bus lines that have limited frequency on evenings or weekends are not useful for residents. He also noted that projects do not have to use TDM method 9; they only need to choose two or three methods from the nine listed.

The Chair noted that she would like to think more about the issue of bus frequency, and they can discuss it further at the hearing.

Mr. Revilak reiterated that he would like to include charging stations for bicycles and scooters as another TDM method option. He noted that charging stations or charging lockers that are outside the building or in a basement or other non-residential area are a safer option than charging large lithium batteries inside apartments. Mr. Benson agreed, noting that it is just one among several options, and a project is not required to include such charging facilities. The other Board members agreed.

The Board agreed to discuss method 9 further at the hearing.

#### **ARTICLE C, Delete Inland Wetland Overlay District**

Ms. Ricker explained that this article was proposed by the Conservation Commission, the Zoning Board of Appeals (ZBA), the Inspectional Services Department (ISD), and the Environmental Planner. The areas protected by the overlay district are under the jurisdiction of the Conservation Commission, and the inclusion of the overlay district in the bylaw creates confusion and redundancy. Many municipalities established this type of overlay district before the passage of the state Wetlands Protection Act, and other municipalities have eliminated their Inland Wetland Overlay Districts.

The Chair asked if members of the Conservation Commission will be at Town Meeting to speak to the issue. Ms. Ricker confirmed that they would. Mr. Benson said that it would be helpful to have the Chair of the ZBA as well as one or more of the Conservation Commissioners speak at Town Meeting to explain that this overlay district is actually counterproductive.

#### **ARTICLE D, Definition of Lot Coverage**

Ms. Ricker explained that the ZBA requested that Lot Coverage be defined in the Zoning Bylaw, as it is referred to in numerous places. The proposal defines Lot Coverage is being the same as Building Coverage, which is defined. The Board members all agreed with the main motion language as proposed.

## **ARTICLE E, Parking in Residential Districts**

Ms. Ricker explained that the ZBA also requested this warrant article to clarify under what circumstances a property could add a second driveway. Mr. Benson proposed a number of changes to the proposed main motion language for clarity. He shared his proposed changes with the Christian Klein, the Chair of the ZBA, who agreed with them.

Mr. Revilak suggested that parking in the longer of the two front yards be allowed on all corner lots, not only on corner lots of less than 6,000 square feet. Mr. Benson noted that the ZBA only requested that such parking be allowed on lots of less than 6,000 square feet, so he thinks only that change should be made at this time. They can discuss the issue with the ZBA and perhaps revisit it next year.

The Board members all agreed with Mr. Benson's changes.

## **ARTICLE F, Screening and Buffer Requirements**

Ms. Ricker said that this amendment is largely an administrative correction of sections that are duplicative. Mr. Benson explained that the purpose of this article is to get rid of duplication and add clarity, not to make any substantive changes.

Mr. Revilak said that this language makes sense for correction and clarification, but he suggested that in the future, the Board reconsider the allowed buffer distances as compared to uses that are currently allowed in the business and industrial districts. The screening requirements typically far exceed required setbacks.

The other Board members felt that the language adequately clarified the section.

## **ARTICLE G, Redevelopment Board Jurisdiction**

Ms. Ricker explained that several recent residential projects that would ordinarily be heard by the ZBA have recently come before the Board solely because they abut the Minuteman Bikeway. The Board feels that Redevelopment Board jurisdiction of such projects is not necessary. The proposed amendment makes single-family, two-family, or duplex dwellings in the R1 or R2 districts abutting the Bikeway subject to the ZBA, not the Redevelopment Board.

Mr. Benson proposed several minor changes for clarity. The other Board members agreed with Mr. Benson's changes.

## **ARTICLE H, Rezone B1 Parcels / ARTICLE I, Zoning Map Adoption for B1 Rezoning**

Ms. Ricker explained that these two articles propose rezoning B1 parcels larger than 5,000 square feet to B2A to allow for greater flexibility for potential redevelopment. Article H rezones the parcels, and Article I adopts the revised zoning map once the parcels are rezoned. Town Counsel recommended using two separate articles. She also explained that 5,000 square feet is the minimum parcel size in the B2A district. The Board never discussed the specific size of parcels to be rezoned, but DPCD staff suggested using 5,000 square feet as a cutoff so that the rezoned parcels would be conforming and more likely to be redeveloped.

Mr. Benson said that the 5,000 square foot limit makes sense. He also noted the B2A district is defined as only including parcels on Mass Ave, Mill St, Summer St, and Broadway, but a handful of B1 parcels are on other streets. He thinks that those parcels should remain zoned B1 because they are on smaller streets, so the scale of development allowed in B1 is more appropriate for those streets. He also noted that community centers are the only use allowed in B1 but not in B2A. He would like to amend the use table to allow community centers in the B2A district by special permit.

The Chair said that she is not sure that the size of the lot is the right metric to use to decide which parcels to rezone. She would like to create more continuity within districts by rezoning smaller parcels that are adjacent to larger parcels. That would enable developers to combine small parcels to create better projects.

Mr. Revilak said that he thinks that it makes sense to rezone B1 parcels that are adjacent to a parcel already zoned B2A, B4, B5, or I, even if they are not on one of the four main corridors. The definition of B2A could be changed to say that it is predominantly located on those four streets.

Ms. Korman-Houston agreed with the Chair and Mr. Revilak about which parcels to rezone. She agreed with Mr. Benson about amending the B2A use table to include community centers.

Mr. Lau said that he thinks the six parcels on Chestnut Street should be rezoned. Mr. Benson said that makes sense, and the Board should then take up Mr. Revilak's suggestion to add the word "predominantly" to the definition of the B2A district. He agreed with the Chair about including smaller parcels that are next to larger parcels. He would prefer to leave the parcels on side streets other than Chestnut Street as B1.

The Board members looked at the maps showing all B1 parcels and discussed which ones should be included. They decided for the time being to include all the B1 parcels, and they agreed that before the hearing on March 24, they would need to visit all the sites to determine if any should not be included.

#### The Chair moved to **Agenda Item 4 – Open Forum.**

The Chair opened the floor:

- John Worden, 27 Jason St, Town Meeting Member – He said that he has also filed an article for an Affordable Housing Overlay, which would allow affordable housing in any district except industrial and open space, but the structure must follow the regulations for that particular district in terms of dimensions and setbacks. It also says that buildings could not overshadow other buildings. Anything built under this amendment would not change the appearance of the neighborhood, but it would allow more units than are currently allowed.
- Wynelle Evans, 20 Orchard Pl – She asked if the proposed modification to Section 6.1.10.A.(1) in Article E means that driveways leading to garages underneath houses could no longer be built.
- Marina Popova, 255 Ridge St – She likes that the distance to a bus stop and the frequency of the bus are included in TDM method 9, because the frequency is critical to the ability for people to actually rely on a bus.

Seeing no one else who wished to speak, the Chair closed open forum.

Mr. Benson explained that Article E moves many of the parking requirements within the Zoning Bylaw section to make them read more easily, but they have not changed what is allowed, and people can still park in their driveways.

#### The Chair moved to **Agenda Item 5 – New Business.**

Ms. Ricker said that a home rule petition will be part of the warrant at Town Meeting. It proposes to modify the Town Manager Act to more clearly establish that the Redevelopment Board serves as the Board of Survey. DPCD is still looking into what exactly that will mean procedurally for the Board. Mr. Benson said his understanding is that the Redevelopment Board is the Board of Survey, and it is a good idea to end any inconsistency about that.

Mr. Revilak asked Ms. Ricker if the Town is in the process of forming a Beautification Committee. Ms. Ricker replied that such a committee has already been established, and staff are looking at how to make that committee more functional and impactful.

The Chair asked for a motion to adjourn. Mr. Lau so moved, and Mr. Benson seconded. The Board voted and approved unanimously.

Meeting **Adjourned** at 9:20.



**Documents used:**

- Agenda Item 1     Draft Meeting Minutes – January 27, 2025
- Agenda Item 2     Arlington Affordable Housing Overlay - Draft Bylaw
- Agenda Item 3     DRAFT ATM 2025 ARB ZONING BYLAW AMENDMENTS 02-06-2025  
Proposed Revised Warrant Article Hearing Schedule 2025  
760 CMR 71 - FINAL 1-17-25  
Arlington11496A Article 27 - AG Comments on ADU  
DRAFT ATM 2025 ARB ARTICLES A, B, E, G - EBB edits
- Correspondence   02042025 Heigham, C  
                             02092025 Englisher, L

DRAFT



## Town of Arlington, Massachusetts

### Public Hearing: Docket #3838, 15 Ryder St

#### Summary:

7:35 pm

Notice is herewith given that an application has been filed on January 27, 2025, by Thomas Allen, ABC Beer Inc, dba Arlington Brewing Company, 251 Lowell St, Arlington, MA, 02474, to open Special Permit Docket #3838 in accordance with the provisions of MGL Chapter 40A § 11, and the Town of Arlington Zoning Bylaw Sections 3.3, Special Permits, and 3.4, Environmental Design Review. The applicant proposes a change of use to a brewery and restaurant including a taproom, as well as renovation of a portion of the property located at 15 Ryder St, Arlington, MA, in the I Industrial District. The opening of the Docket is to allow the Board to review and approve the application under Section 3.4, Environmental Design Review.

- Applicant will be provided 10 minutes for an introductory presentation.
- DPCD staff will be provided 5 minutes for an overview of their Public Hearing Memorandum.
- Members of the public will be provided time to comment.
- Board members will discuss Docket and may vote.

#### ATTACHMENTS:

Type	File Name	Description
Application for Special Permit	ABC_Beer_Inc_-_15_Ryder_St_EDR_Application.pdf	ABC Beer Inc - 15 Ryder St EDR Application
Application for Special Permit	ABC_Beer_Inc_-_15_Ryder_St_Photos_of_Existing_Conditions.pdf	ABC Beer Inc - 15 Ryder St Photos of Existing Conditions
Application for Special Permit	ABC_Beer_Inc_-_15_Ryder_St_Plans_and_Drawings_of_Existing_Conditions.pdf	ABC Beer Inc - 15 Ryder St Plans and Drawings of Existing Conditions
Application for Special Permit	ABC_Beer_Inc_-_15_Ryder_St_Plans_and_Drawings.pdf	ABC Beer Inc - 15 Ryder St Plans and Drawings
Application for Special Permit	ABC_Beer_Inc_-_15_Ryder_St_LEED_Checklist.pdf	ABC Beer Inc - 15 Ryder St LEED Checklist
Application for Special Permit	ABC_Beer_Inc_-_15_Ryder_St_EDR_Erosion___Sediment_Plan.pdf	ABC Beer Inc - 15 Ryder St EDR Erosion & Sediment Plan
Application for Special Permit	ABC_Beer_Inc_-_15_Ryder_St_EDR_Solar.pdf	ABC Beer Inc - 15 Ryder St EDR Solar
Application for Special Permit	02202025_EDR_memo_-_15_Ryder_St.pdf	02202025 EDR memo - 15 Ryder St
Application for Special Permit	Arlington_Brewing_Company_-_presentation_to_ARB_2025-02-	Arlington Brewing Company - presentation to



PLANNING & COMMUNITY  
DEVELOPMENT

Docket 3838

## REQUIRED SUBMITTALS CHECKLIST

2025 JAN 29 A 9:49

One electronic copy of your application is required; print materials may be requested. Review the ARB's Rules and Regulations, which can be found at [www.arlingtonma.gov/arb](http://www.arlingtonma.gov/arb), for the full list of required submittals.



### Application Cover Sheet (project and property information, applicant information)



### Dimensional and Parking Information Form (see attached)



### Impact statement

Statement should respond to Environmental Design Review (Section 3.4) and Special Permit (Section 3.3) criteria on pages 6-8 of this packet; include:

- LEED checklist and sustainable building narrative as described in criteria 12.
- Summary of neighborhood outreach, if held or planned.



### Drawing and photographs of existing conditions

- Identify boundaries of the development parcel and illustrate the existing conditions on that parcel, adjacent streets, and lots abutting or directly facing the development parcel across streets.
- Photographs showing conditions on the development parcel at the time of application and showing structures on abutting lots.



### Site plan of proposal. Must include:

- Zoning boundaries, if any, and parcel boundaries;
- Setbacks from property lines;
- Site access/egress points;
- Circulation routes for pedestrians, bicyclists, passenger vehicles, and service/delivery vehicles;
- New buildings and existing buildings to remain on the development parcel, clearly showing points of entry/exit;
- Other major site features within the parcel or along its perimeter, including but not limited to trees, fences, retaining walls, landscaped screens, utility boxes, and light fixtures;
- Spot grades or site topography and finish floor level;
- Open space provided on the site;
- Any existing or proposed easements or rights of way.



### Drawings of proposed structure

- Schematic drawings of each interior floor of each proposed building, including basements.
- Schematic drawings of the roof surface(s), identifying roof materials, mechanical equipment, screening devices, green roofs, solar arrays, usable outdoor terraces, and parapets.
- Elevations of each exterior façade of each building, identifying floor levels, materials, colors, and appurtenances such as mechanical vents and light fixtures.
- Drawings from one or more prominent public vantage point illustrating how the proposed project will appear within the context of its surroundings.
- Graphic information showing façade materials and color samples.
- Include lighting plan and fixtures if not provided on site or landscaping plan.

TOWN CLERK  
ARLINGTON, MA. 02476  
2025 JAN 29 AM 9:52

**Vehicle, Bicycle, and Service Vehicle Plans**

- Parking and loading plans, including all vehicle and bicycle parking facilities located on the parcel or within a structure, showing dimensions of spaces, driveways, access aisles, and access/egress points. Include line-of-sight and turning radius along with length and type of delivery truck.
- If you are requesting a reduction in the amount of required parking, include a Transportation Demand Management Plan per Section 6.1.5.
- Plans of all bicycle parking facilities located on the lot and within any structure, including dimensions of spaces and access routes and types of bicycle racks.

**Sustainable Building and Site Design Elements**

- A solar energy systems assessment per Section 6.4, which must include:
  - An analysis for solar energy system(s) for the site detailing layout and annual production;
  - The maximum feasible solar zone area of all structures; and,
  - Drawings showing the solar energy system you propose, with a narrative describing the system, the reasons the system was chosen, and how the system meets the requirements of Section 6.4; or
  - A detailed explanation of why the project meets an exemption of Section 6.4.2.
- LEED checklist and narrative per EDR criterion 13.

**Proposed landscaping (*may be incorporated into site plan*)**

Schematic drawing(s) illustrating and clearly labels all landscape features, including hardscape materials, permeable areas, plant species, and light fixtures.

**Plans for sign permits, if signage is an element of development proposal****Stormwater management plan**

*(for stormwater management during construction for projects with new construction)*

**SketchUp Compatible Model, if required****Application fee**

(See [Rule 12 of the ARB Rules and Regulations](#) for how to calculate the fee)

**FOR OFFICE USE ONLY**

Docket #: 3838

\_\_\_\_\_ Special Permit Granted

Date: \_\_\_\_\_

\_\_\_\_\_ Received evidence of filing with Registry of Deeds

Date: \_\_\_\_\_

\_\_\_\_\_ Notified Building Inspector of Special Permit filing

Date: \_\_\_\_\_

PLANNING & COMMUNITY  
DEVELOPMENT

2025 JAN 29 AM 9:49

## COVER SHEET

Application for Special Permit in Accordance with Environmental Design Review

### PROPERTY AND PROJECT INFORMATION

- Property Address 15 Ryder St Arlington MA 02476  
Assessors Block Plan, Block, Lot No. 57-2-1 Zoning District Industrial
- Deed recorded in the Registry of deeds, Book 78808, Page 3  
or- registered in Land Registration Office, Cert. No. \_\_\_\_\_, in Book \_\_\_\_\_, Page \_\_\_\_\_
- Present Use of Property (include # of dwelling units, if any)  
Contractor Office, Shop and Warehouse
- Proposed Use of Property (include # of dwelling units, if any)  
Restaurant and Brewery

TOWN CLERK  
ARLINGTON, MA 02476  
2025 JAN 29 AM 9:52

### APPLICANT INFORMATION

- Applicant:** Identify the person or organization requesting the Special Permit:  
Name of Applicant(s) Thomas Allen  
Organization ABC Beer Inc dba Arlington Brewing Company  
Address 251 Lowell St, Arlington MA 02474  
Street City, State, Zip  
Phone (617) 299-9095 Email tom@drinkarlingtonbeer.com
- Applicant Interest:** the applicant must have a legal interest in the subject property:  
☐ Property owner ☐ Purchaser by land contract  
☐ Purchaser by option or purchase agreement ☒ Lessee/tenant
- Property Owner** ☐ Check here if applicant is also property owner  
Identify the person or organization that owns the subject property:  
Name Julia Mirak Kew Title Vice President  
Organization ABCJ Land LLC Phone (781) 641-2495  
Address 438 Massachusetts Ave Suite 127, Arlington MA 02474  
Street City, State, Zip  
Phone (781) 641-2495 Email Julia@mirakproperties.com

4. **Representative:** Identify any person representing the property owner or applicant in this matter:

Name Mary Winstanley O'Connor Title Attorney

Organization Krattenmaker O'Connor & Ingber P.C. Phone (617) 523-1010 Ext 223

Address One McKinley Square, Fifth Floor, Boston, MA 02109  
Street City, State, Zip

Phone (617) 523-1010 Ext. 223 Email moconnor@koilaw.com

5. Permit applied for in accordance with the following Zoning Bylaw section(s)

3.3 Special Permits

3.4 Environmental Design Review

section(s)

title(s)

6. List any waivers being requested and the Zoning Bylaw section(s) which refer to the minimum or maximum requirements from which you are seeking relief.

6.1.10(F)2 Location of Parking Spaces - location of loading area

section(s)

title(s)

7. Please attach a statement that describes your project and provide any additional information that may aid the ARB in understanding the permits you request. Include any reasons that you feel you should be granted the requested permission.

(In the statement below, check the options that apply)

The applicant states that ABC Beer Inc is the owner ☐ or occupant ☒ or purchaser under agreement ☐  
of the property in Arlington located at 15 Ryder St. Arlington MA 02476  
which is the subject of this application; and that unfavorable action ☐ or no unfavorable ☒ action has been taken by  
the Zoning Board of Appeals on a similar application regarding this property within the last two years. The applicant  
expressly agrees to comply with any and all conditions and qualifications imposed upon this permission, either by the  
Zoning Bylaw or by the Redevelopment Board, should the permit be granted.

Signature of Applicant(s):



251 Lowell St Arlington MA 02474  
Address

(617) 299-9095  
Phone

## DIMENSIONAL AND PARKING INFORMATION

Property Location: 15 Ryder Street, Arlington MA Zoning District: I - Industrial

Applicant: ABC Beer Inc. d/b/a Arlington Brewing Company Address: 251 Lowell St. Arlington MA

Present Use/Occupancy: No. of Dwelling Units: \_\_\_\_\_ Uses and their gross square feet:  
Office and Storage Office = 4,158 GFA  
Storage = 1,650 GFA

Proposed Use/Occupancy: No. of Dwelling Units: \_\_\_\_\_ Uses and their gross square feet:  
Manufacturing (Brewery) and Restaurant Manufacturing = 2,133 GFA  
Restaurant = 3,872 GFA

	Present Conditions	Proposed Conditions	Min. or Max. Req'd by Zoning for Proposed Use
Lot Size	45,687	45,687	min. (.....)
Frontage	193	193	min. (.....)
Floor Area Ratio <sup>1</sup>	.12	.13	max. 3.0
Lot Coverage (%), where applicable	8.75%	10.85%	max. (.....)
Lot Area per Dwelling Unit (sf)	N/A	N/A	min.
Front Yard Depth (feet)	62'	61.5'	min. 10'
Side Yard Width (feet) right side	23.5'	23'	min. 10'
left side	69.5'	68.9'	min. 10'
Rear Yard Depth (feet)	102.9'	77.4'	min. 10'
Height stories	2	1	stories <sup>2</sup> 3
feet	24'-5"	25'-3"	Feet 39'
Open Space (% of G.F.A.) <sup>3</sup>	390	6,690	min. (.....)
Landscaped (sf)	315	3,964	(sf) (.....)
Usable (sf)	0	2,630	(sf) (.....)
Parking Spaces (#) <sup>4</sup>	+/- 30 @ Leased Premises	20	min. 17
Parking Area Setbacks (feet) (where applicable)	0' @ sideyard w/out wall buffer 40' @ frontyard	2'-3" @ sideyard w/ wall buffer 44'-0" @ frontyard	min. 5'-0" @ sideyard w/ wall buffer min. 10'-0" @ frontyard
Loading Spaces (#)	0	0	min. 1
Bicycle Parking <sup>5</sup> short term	0	22	min. 4+2=6
long term	0	4	min. 1+3=4

<sup>1</sup> FAR is based on Gross Floor Area. See Section 5.3.22 for how to calculate Gross Floor Area. On a separate page, provide the calculations you used to determine FAR, including the calculations for Gross Floor Area.

<sup>2</sup> Where two heights are noted in the dimensional tables, refer to Section 5.3.19, Reduced Height Buffer Area to determine the applicable height or the conditions under which the Board may provide relief.

<sup>3</sup> Per Section 5.3.22(C), district dimensional requirements are calculated based on GFA. On a separate page, show how you determined the open space area amounts.

<sup>4</sup> See Section 6.1, Off-Street Parking. If requesting a parking reduction, refer to Section 6.1.5.

<sup>5</sup> See Section 6.1.12, Bicycle Parking, or refer to the [Bicycle Parking Guidelines](#).



# Support Narrative for Dimensional and Parking Form

Prepared by: Reisen Design Associates

Date: 1.24.2025

## Floor Area Ratio Existing

1<sup>st</sup> floor (4,000GFA) + Mezzanine (1,755GFA) = 5,755GFA

FAR = 5,755/45,687 = .12

## Floor Area Ratio Proposed

1<sup>st</sup> (4,000GFA) + Mezz (1,045GFA) + Addition (960GFA) = 6,005GFA

FAR = 6,005/45,687 = .13

## GFA by use

Manufacturing = 960 (walkin) + 1,070 (brewfloor) + 103 (boiler rm) = 2,133GFA

Restaurant = 4,000 – 1,070 - 103 + 1045 (mezz) = 3,872GFA

## Parking Spaces

Section 6.1.10(C) exempts the first 3,000 square feet of non-residential space in mixed-use developments from the parking requirements of Section 6.1. As per discussion with Director Ricker on January 21, 2025 the following is how the mixed-use parking reduction is applied: 3,000 GFA reduction = 2,133sq.ft. GFA from Manufacturing and 867sq.ft. GFA from Restaurant

Uses: Manufacturing = 2,133GFA, Restaurant = 3,872GFA

- Manufacturing = 2,133 sq.ft. GFA – 2,133 sq.ft. = 0 spaces required
- Restaurant = 1 space per 4 seats total seating capacity excluding seasonal , including above reduction: 106 seats total – 38 seat reduction (seating at Mezzanine = 867 sq.ft.) = 68 seats  
**68 / 4 = 17 parking spaces**

## Bicycle Parking Spaces

Restaurant (3,872GFA)

Short Term : 1 space per 1000SF (1 X 4) = 4 spaces

Long Term : 0.20 spaces per 1000SF (.2 X 4) = .8 = 1 space

Brewery - Light Manufacturing (2,133GFA)

Short Term : 0.60 spaces per 1000SF (.6 X 3) = 1.8 = 2 spaces

Long Term : 1 space per 1000SF = 3 spaces

*or if greater - .75 per employee .75 X 2 = 1.5 = 2 spaces (less than SF based requirement)*

## Memorandum

To: Arlington Redevelopment Board  
From: ABC Beer Inc. d/b/a Arlington Brewing Company  
Re: Special Permit Application via EDR for 15 Ryder St.

We are ABC Beer Inc., doing business as Arlington Brewing Company. Since our launch in the spring of 2023, we have proudly operated in Arlington, with the goal of becoming a vibrant and valued part of the community. We are now applying to establish a brick-and-mortar brewery, taproom, kitchen, and beer garden at 15 Ryder Street.

Our mission at Arlington Brewing Company is to spread joy within our community and beyond by creating memorable experiences centered around incredible craft beer, delicious food, and authentic human connections. Since June 2023, we have been selling our craft beer at pop-up events throughout Arlington and neighboring communities. Most notably, we have hosted the popular Arlington Res Beer Garden for the past two years. This beloved event brings community members together to enjoy beer, food, live music, and camaraderie in a welcoming and inclusive setting.

The parcel located at 15 Ryder Street encompasses 45,687 square feet of land, of which we are leasing approximately 17,860 square feet. The leased area includes the existing building and the immediately surrounding land. The landlord will retain the remainder of the lot for their own use. Our proposal entails converting the existing contractor workshop on the leased premises into a microbrewery featuring a taproom, a full-service kitchen, and an outdoor beer garden. In addition to our handcrafted beers, we will offer a full menu of elevated American pub fare with global influences. Our vision is to establish a warm, community-oriented destination that fosters connection and celebration. We are excited to bring this unique destination to Arlington and look forward to contributing to the town's thriving community and economy.

### **Summary of Neighborhood Outreach**

On October 1, before we made a public announcement about signing the lease, We knocked on doors Ryder, Forest and Beck Streets, and talked to neighbors that were home. Additionally, we left a short flyer introducing ourselves and inviting neighbors to come join us for an informal meet and greet at Workbar. We asked them to send us their name and email addresses so we could communicate with them in the future. Alex Tee is the only individual who provided us with contact information, and subsequent communication to the neighborhood has all passed through him.

On October 8 we hosted a neighborhood meeting at workbar. There were approximately 12 people in attendance, in addition to ABC Beer staff. During this meeting, we introduced ourselves and engaged in a good conversation. The neighbors presented us with a letter welcoming us to the neighborhood and expressing their comments & concerns. We passed the

letter along to our architect and have been using their ideas and keeping their feedback in mind as we develop the plan for the site.

On December 12 we met with neighbors to review a draft of our plans. In attendance were Alex and Peter. Mariah was on the phone. We showed them the plot-plan of the site and explained the steps we have taken to address the concerns they highlighted in their letter:

- Traffic and pedestrian safety on Ryder St
  - Removed 10 nose-in parking spots along the frontage of the building to prevent backing up into the street
  - Created a pedestrian corridor against the building where parking was previously located. This allows patrons to enter/exit the building without having to walk in the middle of the street. It also allows pedestrians walking between the bike path and Forest st to have at least a portion of their path out of the street.
  - Created a buffer between corridor and street.
  - Implemented more bike parking than is required.
- Noise
  - Beer garden located in rear of building
  - Beer garden enclosed by a privacy fence
  - No equipment along front or sides of building
  - HVAC equipment on roof
  - External brewing equipment behind building, buffered by fence
  - Taproom operating hours restricted to a maximum of 9am-11pm
  - No music in the beer garden after 9pm
  - Primary delivery hours restricted to 9am-11am weekdays and 9am-11am on Saturday
- Odors
  - All exhaust vents out top of building
  - Kitchen exhaust located in back of building
  - Trash and recycling at the very back of the lot, as far from abutters as possible

Their response was to reiterate their high level concerns, particularly about vehicles driving too fast on the street and the need for traffic calming measures. We explained that changes to Ryder St are beyond the scope of this project, but suggested additional improvements on our premises to further mitigate existing traffic and parking issues. We listened as additional concerns were raised and worked together to identify potential solutions.

As a result of this second meeting, we made additional design changes to mitigate the impact of traffic and noise in the neighborhood. These changes are:

- Removed the parallel parking spots in the buffer between the street and pedestrian corridor and replaced them with trees and bike parking.
- Added an interior vestibule to prevent noise from leaking into the street when the door is opened.

We have gone to great lengths to be good neighbors and to address the concerns that have been brought to our attention and believe that our design will dramatically improve the overall safety, attractiveness and quality of life in the neighborhood. We look forward to building a strong relationship with our neighbors and becoming a valued member of the community.

Respectfully Submitted,  
Tom Allen  
Founder & President  
Arlington Brewing Company

## IMPACT STATEMENT REQUIREMENTS

Projects subject to Environmental Design Review must address and meet the following Special Permit Criteria (see Section 3.3.3 of the Zoning Bylaw) and Environmental Design Review Criteria (see Section 3.4) of the Zoning Bylaw. Please submit an impact statement that describes your proposal, and addresses each of the following criteria.

### SPECIAL PERMIT CRITERIA

**1. The use requested is listed as a special permit in the use regulations for the applicable district or is so designated elsewhere in this Bylaw.**

15 Ryder Street is located in the Industrial District. We propose a mixed use building to include a brewery, which is allowed by special permit, pursuant to Section 5.6.4 (D) and a restaurant and beer garden. Pursuant to Section 5.6.1 (B), mixed use development is allowed with all uses in the Industrial District. Section 5.6.3 permits a restaurant use by special permit for a restaurant more than 2,00 square feet in size. In fact, Section 5.6.4 (D) contemplates a restaurant serving product manufactured at a brewery or distillery.

**2. The requested use is essential or desirable to the public convenience or welfare.**

The proposed brewery and taproom at 15 Ryder St will serve as a significant asset to the community, enhancing public convenience and welfare in the following ways:

Community Gathering Space: The brewery and taproom will provide a welcoming and inclusive space where residents can gather, socialize, and connect. Such spaces are vital for fostering a sense of community and belonging.

Local Economic Growth: As a locally owned and operated business, the brewery will contribute to the local economy by creating jobs, supporting regional suppliers, and attracting patrons who will also visit neighboring businesses. This multiplier effect will enhance the vibrancy of Arlington heights and the surrounding areas.

Enhancing Public Convenience: The taproom will serve as a convenient and family-friendly destination for socializing and dining, catering to the needs of both residents and visitors. By offering a high-quality craft beer experience locally, the brewery reduces the need for residents to travel out of town, supporting walkability and minimizing vehicle trips. Its proximity to key local attractions, such as the Ed Burns Arena, further enhances Arlington's appeal, providing visitors with additional amenities and enriching their overall experience in town.

Public Support: Feedback from Arlington residents has consistently demonstrated a strong desire for a local brewery with a brick and mortar presence. This project fulfills a clear demand for community-oriented businesses that enhance the town's character, foster connections, and contribute positively to Arlington's overall quality of life.

Operating Experience: Over the past several years we have operated beer gardens in various locations in Arlington, including the Res, Whittemore Park and the Schwamb mill, among others. We have also operated indoor events at Town Hall and many other private locations. The positive public response to, and attendance at these events clearly shows there is strong demand for this type of community building activity. Continued and repeated support for these events indicates that we are delivering a product that satisfies the demand. We intend to deliver the same positive atmosphere, welcoming hospitality and high quality product to our taproom.

**3. The requested use will not create undue traffic congestion or unduly impair pedestrian safety.**

The current use of the site generates significant large-vehicle traffic, including construction and landscaping vehicles accessing on-site storage, which contributes to congestion and potential safety concerns. Our proposed site plan addresses these issues by reducing the number of on-site parking spaces and creating a dedicated pedestrian corridor along the Ryder Street side of the building.

The existing parking configuration, with vehicles parked facing the building and backing out into the street, forces pedestrians to walk in the street or behind parked cars, posing safety risks. Our new layout prioritizes pedestrian safety by providing a clear and secure walkway for customers and passersby. This improvement will benefit not only our patrons but also the broader community, offering a safer route for those traveling between Massachusetts Avenue and the Minuteman Bike Path without needing to walk in the street along our frontage.

**4. The requested use will not overload any public water, drainage or sewer system or any other municipal**

**system to such an extent that the requested use or any developed use in the immediate area or in any other area of the Town will be unduly subjected to hazards affecting health, safety or the general welfare.**

Water Usage: While breweries require water for production, the scale of our operations is consistent with other businesses of similar size and does not represent an extraordinary burden on the municipal water supply. We have implemented efficient water usage practices, including water re-use where feasible, to minimize consumption.

Wastewater Management: Wastewater generated on-site will be managed responsibly to ensure it meets all municipal discharge standards. Pretreatment processes will be implemented if necessary, preventing undue strain on the town's sewer system. Best practices to avoid unnecessary creation of waste water will be employed. These best practices include recapture of cooling water to be used as ingredient water in subsequent batches, employing mechanical means of cleaning (ie sweeping or squeegeing) instead of spraying, and re-use of cleaning/sanitizing solutions.

Drainage: We plan to convert approximately 2,700 square feet of impervious parking surface into a permeable, landscaped beer garden. This transformation will significantly reduce stormwater runoff from the site, improving the overall drainage capacity and mitigating potential impacts on the town's stormwater infrastructure. By enhancing permeability, this change aligns with modern stormwater management practices, supporting sustainability and reducing the burden on municipal drainage systems. Additionally, no other modifications to the existing drainage systems are anticipated, as the current infrastructure is adequate for our needs.

#### **5. Any special regulations for the use as may be provided in this Bylaw are fulfilled.**

*5.6.4.D Brewery, distillery, and winery, including functional equivalents. Tap room hours of operation open to the public shall not represent disturbance to adjacent residential uses and such hours must follow the Commonwealth of Massachusetts requirements for licensing and operations.*

- Proposed taproom opening hours are between 9am-11pm.
- No live music in the beer garden is permitted after 9pm.
- Deliveries are restricted to the hours between 9am and 3pm on weekdays and 9am and 11am on Saturdays

*5.6.4.F Food production facility. Food and beverage facilities shall: (1) Properly store equipment and remove litter and waste within the immediate vicinity of the plant buildings or structures as to avoid becoming a breeding place, or harborage for pests. (2) Constantly check for pests and pest infestation (3) Locate and operate fans and other air-blowing equipment in a manner that minimizes noise levels and the potential for contaminating the building and its surroundings to avoid health hazards to the public. (4) Not locate vents on the façade adjacent to sidewalks or the Minuteman Bikeway to avoid exposure to the public.*

Per our design, we have complied with these requirements.

#### **6. The requested use will not impair the integrity or character of the district or adjoining districts, nor be detrimental to the health, morals, or welfare.**

The proposed brewery and taproom will complement and enhance the integrity and character of the district by contributing to its vibrancy and appeal. The project has been thoughtfully designed with a commitment to fostering a vibrant, safe, and sustainable community asset.

District Character: The brewery will repurpose an existing building that is currently an eyesore. We will improve the aesthetics, circulation and landscaping, adding much needed outdoor green space in our beer garden and removing unsightly temporary storage containers from the lot. The taproom will serve as a gathering place for residents and visitors, fostering a sense of connection and contributing to Arlington Hights' identity as a welcoming and lively area.

Community Health and Welfare: By offering a family-friendly space, the brewery will encourage responsible socialization and provide a safe, inclusive environment for community engagement. The brewery's commitment to sustainability, including water conservation and reducing impervious surfaces, demonstrates care for the environment and public health.

#### **7. The requested use will not, by its addition to a neighborhood, cause an excess of the particular use that could**

**be detrimental to the character of said neighborhood.**

There are no other comparable type uses in the Town.

## **ENVIRONMENTAL DESIGN REVIEW CRITERIA**

- 1. Preservation of Landscape. The landscape shall be preserved in its natural state, insofar as practicable, by minimizing tree and soil removal, and any grade changes shall be in keeping with the general appearance of neighboring developed areas.**

The existing site condition has almost no natural landscaping. Our beer garden will change that by converting approximately 2700 square feet of parking lot into a landscaped outdoor dining and drinking area. Additionally we are adding landscaping in the front of the building along Ryder St. All outdoor changes to the site are intended to improve the natural state, not detract from it.

- 2. Relation of Buildings to Environment. Proposed development shall be related harmoniously to the terrain and to the use, scale, and architecture of existing buildings in the vicinity that have functional or visual relationship to the proposed buildings. The Arlington Redevelopment Board may require a modification in massing to reduce the effect of shadows on abutting property in an R0, R1 or R2 district or on public open space.**

This project adds 960 sq ft on the back of the building, and we will be updating the exterior to improve both building energy efficiency and aesthetics. The addition will be used for cold storage of both raw materials and finished product. It sits behind the building and will not significantly change the visual relationship between this site and adjacent buildings.

- 3. Open Space. All open space (landscaped and usable) shall be so designed as to add to the visual amenities of the vicinity by maximizing its visibility for persons passing the site or overlooking it from nearby properties. The location and configuration of usable open space shall be so designed as to encourage social interaction, maximize its utility, and facilitate maintenance.**

Due to site constraints, the beer garden will be in the rear of the building, and not easily visible from Ryder St. The presence of the beer garden is, however, a significant improvement to the visual amenities of the area as viewed from the Artemis driveway, Artemis residence, and nearby businesses such as Workbar. The purpose of the beer garden is to create an open outdoor space for socializing and enjoying our products (beer, food, and non-alcoholic beverages).

- 4. Circulation. With respect to vehicular, pedestrian and bicycle circulation, including entrances, ramps, walkways, drives, and parking, special attention shall be given to location and number of access points to the public streets (especially in relation to existing traffic controls and mass transit facilities), width of interior drives and access points, general interior circulation, separation of pedestrian and vehicular traffic, access to community facilities, and arrangement of vehicle parking and bicycle parking areas, including bicycle parking spaces required by Section 8.13 that are safe and convenient and, insofar as practicable, do not detract from the use and enjoyment of proposed buildings and structures and the neighboring properties.**

We are enhancing pedestrian safety and accessibility by creating a dedicated pedestrian corridor along our Ryder Street frontage. This corridor ensures pedestrians can travel safely from the bike path to Forest Street without needing to walk in the street. Our site plan incorporates trees planted along Ryder Street to act as a natural barrier between the roadway and pedestrian areas.

To further promote sustainable transportation, we are installing substantial bike parking, strategically located between the trees and the building. This placement shields the bike parking from vehicle traffic while maintaining ample space for pedestrians.

To accommodate these improvements, we have removed all parking in front of the building. This change

significantly enhances both safety and the aesthetic appeal of the site. Cars will no longer back out into the street where pedestrians currently walk, reducing the risk of accidents. Neighbors will benefit from improved views of trees and bike parking, replacing the current view of parked vehicles.

At present, parking extends up to Ryder Street, enclosed by a chain-link gate. This gate will be removed as part of our redesign. All parking will be relocated to the lot on the side of the building, which has been designed to include adequate turning radii and lane clearances. This layout improves sight lines for vehicles entering and exiting the lot, eliminates the need for cars to turn around on Ryder Street, and reduces congestion. In the event the lot is full, vehicles will turn around within the lot itself, rather than in the street.

- 5. Surface Water Drainage. Special attention shall be given to proper site surface drainage so that removal of surface waters will not adversely affect neighboring properties or the public storm drainage system. Available Best Management Practices for the site should be employed, and include site planning to minimize impervious surface and reduce clearing and re-grading. Best Management Practices may include erosion control and storm water treatment by means of swales, filters, plantings, roof gardens, native vegetation, and leaching catch basins. Storm water should be treated at least minimally on the development site; that which cannot be handled on site shall be removed from all roofs, canopies, paved and pooling areas and carried away in an underground drainage system. Surface water in all paved areas shall be collected at intervals so that it will not obstruct the flow of vehicular or pedestrian traffic, and will not create puddles in the paved areas.**

The most significant site change we propose is the conversion of approximately 2,500 square feet of impervious asphalt parking lot into a 2,500-square-foot beer garden with permeable surfaces and landscaping. This transformation directly aligns with Best Management Practices for surface water drainage by significantly reducing impervious surface area, minimizing runoff, and enhancing on-site stormwater management.

- 6. In accordance with Section 3.3.4, the Board may require from any applicant, after consultation with the Director of Public Works, security satisfactory to the Board to insure the maintenance of all storm water facilities such as catch basins, leaching catch basins, detention basins, swales, etc. within the site. The Board may use funds provided by such security to conduct maintenance that the applicant fails to do. The Board may adjust in its sole discretion the amount and type of financial security such that it is satisfied that the amount is sufficient to provide for the future maintenance needs.**

No response required

- 7. Utility Service. Electric, telephone, cable TV and other such lines and equipment shall be underground. The proposed method of sanitary sewage disposal and solid waste disposal from all buildings shall be indicated.**

Electrical service will be upgraded and a pad-mount transformer will be installed on site in the location indicated on our site plan. New utility service will be run underground.

Sanitary sewage disposal will use existing sewer lines and is not inconsistent with the sewage loading of other buildings in the immediate vicinity.

Solid waste is generated from 2 primary sources - food operations and brewing operations. Food operation waste is consistent with typical restaurant waste stream, and we plan to divert food scraps and compostable serviceware into the compost to minimize the amount of trash headed to a landfill. Solid waste from the brewing process primarily takes the form of spent grains that are left over after all the sugars have been extracted. These grains will be collected at the end of each brewing cycle and placed in bins that will then be removed to be composted offsite.

We will have dedicated dumpsters for trash and recycling, the locations are indicated on the site plan.

- 8. Advertising Features. The size, location, design, color, texture, lighting and materials of all permanent signs and outdoor advertising structures or features shall not detract from the use and enjoyment of proposed buildings and structures and the surrounding properties. Advertising features are subject to the provisions**



**of Section 6.2 of the Zoning Bylaw.**

See elevation drawings for proposed signage.

- 9. Special Features. Exposed storage areas, exposed machinery installations, service areas, truck loading areas, utility buildings and structures, and similar accessory areas and structures shall be subject to such setbacks, screen plantings or other screening methods as shall reasonably be required to prevent their being incongruous with the existing or contemplated environment and the surrounding properties.**

Our Cold room, located behind the building, and other special features such as outdoor brewery equipment, trash and recycling are all located behind a screening fence. The compressor for the cold room is located on the roof of the cold room and not visible from the street.

We request relief from the requirements in section 6.1.10(F)2. The layout of the site, coupled with the requirements for access to the building for production activities, prevents loading areas from being located in the rear of the building. Instead, we propose the loading area be located in the existing parking area on the side of the building.

Truck loading/unloading will occur in our parking lot primarily during the hours of 9am-11am M-F (outside of peak taproom hours) and will have no special impact.

- 10. Safety. With respect to personal safety, all open and enclosed spaces shall be designed to facilitate building evacuation and maximize accessibility by fire, police, and other emergency personnel and equipment. Insofar as practicable, all exterior spaces and interior public and semi-public spaces shall be so designed as to minimize the fear and probability of personal harm or injury by increasing the potential surveillance by neighboring residents and passersby of any accident or attempted criminal act.**

See plans, which are compliant with all building, fire and safety codes and regulations.

- 11. Heritage. With respect to Arlington's heritage, removal or disruption of historic, traditional or significant uses, structures, or architectural elements shall be minimized insofar as practicable, whether these exist on the site or on adjacent properties.**

Not Applicable

- 12. Microclimate. With respect to the localized climatic characteristics of a given area, any development which proposes new structures, new hard-surface ground coverage, or the installation of machinery which emits heat, vapor, or fumes, shall endeavor to minimize, insofar as practicable, any adverse impact on light, air, and water resources, or on noise and temperature levels of the immediate environment.**

The equipment proposed for the brewery process generates a maximum noise level of 65 dbas at 10 feet. 65 dbas is equivalent to the sound of a normal conversation. Moreover, any sound will be further minimized since the equipment will be screened by exterior fencing. The equipment is to be located behind the building and approximately 120 feet from the nearest residence. The heat stream for all equipment is vented out of the top of the building.

- 13. Sustainable Building and Site Design. Projects are encouraged to incorporate best practices related to sustainable sites, water efficiency, energy and atmosphere, materials and resources, and indoor environmental quality. Applicants must submit a current Green Building Council Leadership in Energy and Environmental Design (LEED) checklist, appropriate to the type of development, annotated with narrative description that indicates how the LEED performance objectives will be incorporated into the project. (LEED checklists can be found at <http://www.usgbc.org/DisplayPage.aspx?CMSPageID=220b> )**

See attached checklist and associated comments.

# Photos of Existing Conditions

# Rear of building looking north





# View from Artemis driveway



# Side of building looking North





# View from corner of Ryder St and Artemis driveway



32 of 232



# View from Ryder St, looking north





# View from Ryder St looking south



34 of 232



# View from Ed Burns Arena parking lot



35 of 232

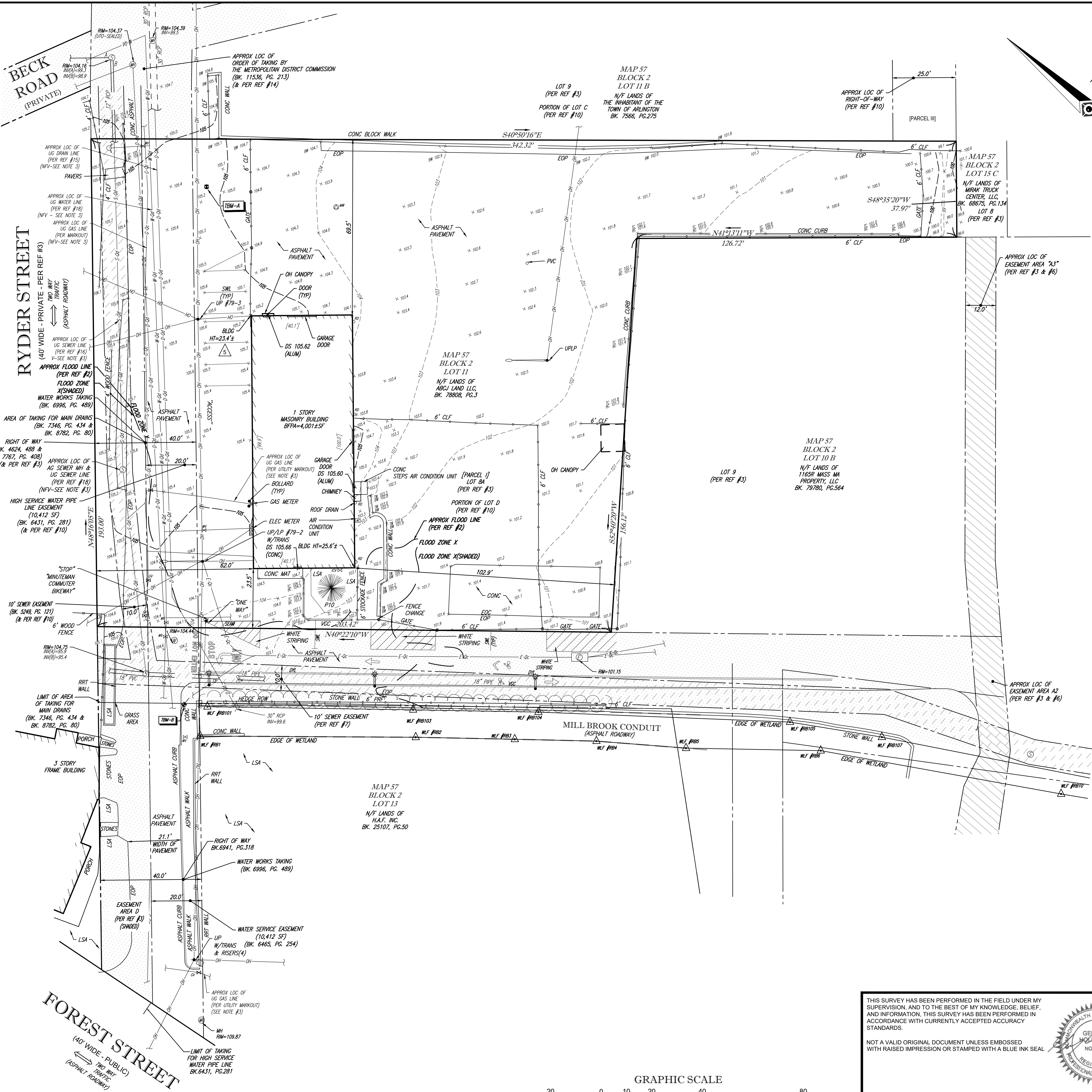


REFERENCES:

1. THE TAX ASSESSOR'S MAP OF ARLINGTON, MIDDLESEX COUNTY, MAP 57.
2. MAP ENTITLED "NATIONAL FLOOD INSURANCE PROGRAM, FIRM, FLOOD INSURANCE RATE MAP, MIDDLESEX COUNTY, MASSACHUSETTS (ALL JURISDICTIONS) PANEL 17 OF 176," COMMUNITY-PANEL NUMBER 2502SC0017K, EFFECTIVE DATE JULY 3, 2024.
3. MAP ENTITLED "PLAN OF LOTS AND EASEMENTS IN ARLINGTON, MA," PREPARED BY: RM ENGINEERING CO., INC., DATED: OCTOBER 23, 2006, AND RECORDED IN THE SOUTH MIDDLESEX REGISTRY OF DEEDS AS PLAN NO. 1072 OF 2007.
4. MAP ENTITLED "PLAN OF LAND IN ARLINGTON, MA," PREPARED BY: RM ENGINEERING CO., INC., DATED: DECEMBER 28, 2018, AND RECORDED WITH THE SOUTH MIDDLESEX REGISTRY OF DEEDS AS PLAN NO. 110 OF 2019.
5. WATER MAPPING PROVIDED BY THE TOWN ENGINEERING DEPARTMENT.
6. MAP ENTITLED "PLAN OF LOTS AND EASEMENTS IN ARLINGTON, MA," PREPARED BY: RM ENGINEERING CO., INC., DATED: JANUARY 21, 2003, AND RECORDED IN THE SOUTH MIDDLESEX REGISTRY OF DEEDS AS PLAN NO. 83 OF 2003 IN BOOK 37850 PAGE 612.
7. MAP ENTITLED "PLAN OF LAND IN ARLINGTON MASS." DATED SEPTEMBER 13, 1945, RECORDED IN THE SOUTH MIDDLESEX REGISTRY OF DEEDS AS PLAN NO. 172 OF 1946.
8. MAP ENTITLED "AS-BUILT GEOTHERMAL SITE PLAN 1167R MASSACHUSETTS AVE. ARLINGTON, MA PREPARED FOR ACHIEVE RENEWABLE" DATES JANUARY 4, 2016 PREPARED BY PAUL LINDHOLM, P.E.
9. MAP ENTITLED "ALTANSPPS LAND TITLE SURVEY, 15 RYDER STREET, TOWN OF ARLINGTON, MIDDLESEX COUNTY COMMONWEALTH OF MASSACHUSETTS," PREPARED BY CONTROL POINT ASSOCIATES, INC., DATED: SEPTEMBER 28, 2021.
10. MAP ENTITLED "SUB-DIVISION OF LAND IN ARLINGTON MASS.," PREPARED BY: FRANK K. NICKSAY, CIVIL ENGINEER, DATED: AUGUST 2, 1947, AND RECORDED WITH THE MIDDLESEX COUNTY REGISTRY OF DEEDS IN BOOK 7178, PAGE 331 AS PLAN 1232 OF 1947.
11. MAP ENTITLED "PLAN OF LAND IN ARLINGTON MASS. TAKEN FOR STORM DRAIN PURPOSES", PREPARED BY: JAMES M. KEANE, TOWN ENGINEER, DATED: OCTOBER 16, 1937, AND RECORDED WITH THE MIDDLESEX COUNTY REGISTRY OF DEEDS AS PLAN 992 OF 1937.
12. MAP ENTITLED "PLAN OF LAND IN ARLINGTON MASS. TAKEN FOR STORM DRAIN PURPOSES", PREPARED BY: JAMES M. KEANE, TOWN ENGINEER, DATED: DECEMBER 15, 1939, AND RECORDED WITH THE MIDDLESEX COUNTY REGISTRY OF DEEDS AS PLAN 1086 OF 1939.
13. MAP ENTITLED "PLAN OF LAND IN ARLINGTON MASS. TAKEN FOR SEWER PURPOSES", PREPARED BY: JAMES M. KEANE, TOWN ENGINEER, DATED: DECEMBER 14, 1939, AND RECORDED WITH THE MIDDLESEX COUNTY REGISTRY OF DEEDS AS PLAN 1086 OF 1939.
14. MAP ENTITLED "THE COMMONWEALTH OF MASSACHUSETTS METROPOLITAN DISTRICT COMMISSION SEWERAGE DIVISION PLAN OF LAND IN ARLINGTON", DATED: FEBRUARY 1968, AND RECORDED WITH THE MIDDLESEX COUNTY REGISTRY OF DEEDS AS PLAN 729 OF 1968.
15. MAP ENTITLED "PLAN AND PROFILE OF STORM DRAIN IN RYDER STREET," PREPARED BY JAMES M. KEANE TOWN ENGINEER, DATED: AUGUST 8, 1949, FILED IN THE TOWN OF ARLINGTON ENGINEERS OFFICE AS PLAN 4822.
16. MAP ENTITLED "SEWER ASSESSMENT PLAN NO. 477," PREPARED BY J. M. KEANE TOWN ENGINEER, DATED: FEBRUARY 8, 1928, FILED IN THE TOWN OF ARLINGTON ENGINEERS OFFICE AS PLAN 2676.
17. UNDERGROUND GAS MAPPING PROVIDED BY NATIONAL GRID GAS, MAP ARLL1250.
18. UNDERGROUND WATER MAPPING PROVIDED BY MWRA.

LEGEND

- 124 --- EXISTING CONTOUR
- 125 --- EXISTING SPOT ELEVATION
- X 102.45 EXISTING TOP OF CURB ELEVATION
- X BC 122.95 EXIST. BOTTOM OF CURB ELEVATION
- HYDRANT
- WATER VALVE
- GAS VALVE
- GAS METER
- ELECTRIC METER
- OVERHEAD WIRES
- APPROX. LOC. UNDERGROUND GAS LINE
- APPROX. LOC. UNDERGROUND DRAINAGE LINE
- APPROX. LOC. UNDERGROUND SANITARY / SEWER LINE
- APPROX. LOC. UNDERGROUND WATER LINE
- SUBSURFACE UTILITY QUALITY LEVEL B
- UTILITY POLE
- UTILITY POLE/LIGHT POLE
- MONITORING WELL
- BOLLARD
- METAL GUIDE RAIL
- TYPICAL
- DMH DRAINAGE/STORM MANHOLE
- SMH SANITARY/SEWER MANHOLE
- UNKNOWN MANHOLE
- DELINEATION FLAG
- CONIFEROUS TREE & TRUNK SIZE
- SHRUBS
- PARKING SPACE COUNT
- DEPRESSED CURB
- [PARCEL I] TITLE REPORT PARCEL
- (TRACT I) DEED DESIGNATION
- [10.5] BUILDING DIMENSION
- SWL SOLID WHITE LINE
- HT HEIGHT
- BLDG BUILDING
- BFFPA BUILDING FOOTPRINT AREA
- EDC EDGE OF CONC.
- EDP EDGE OF PAVEMENT
- LSA LANDSCAPED AREA
- CLF CHAIN LINK FENCE
- MW MONITORING WELL

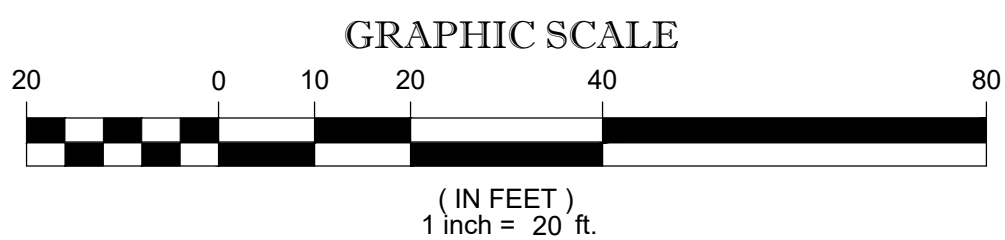


NOTES:

1. PROPERTY KNOWN AS LOT 11 AS SHOWN ON THE TOWN OF ARLINGTON, MIDDLESEX COUNTY, COMMONWEALTH OF MASSACHUSETTS, MAP NO. 57.
  2. AREA = 45,687 SQUARE FEET OR 1.049 ACRES.
  3. LOCATION OF UNDERGROUND UTILITIES ARE APPROXIMATE. LOCATIONS AND SIZES ARE BASED ON UTILITY MARK-OUTS, ABOVE GROUND STRUCTURES THAT WERE VISIBLE & ACCESSIBLE IN THE FIELD, AND THE MAPS AS LISTED IN THE REFERENCES AVAILABLE AT THE TIME OF THE SURVEY. AVAILABLE ASBUILT PLANS AND UTILITY MARKOUT DOES NOT ENSURE MAPPING OF ALL UNDERGROUND UTILITIES AND STRUCTURES. BEFORE ANY EXCAVATION IS TO BEGIN, ALL UNDERGROUND UTILITIES SHOULD BE VERIFIED AS TO THEIR LOCATION, SIZE AND TYPE BY THE PROPER UTILITY COMPANIES. CONTROL POINT ASSOCIATES, INC. DOES NOT GUARANTEE THE UTILITIES SHOWN COMPRISE ALL SUCH UTILITIES IN THE AREA EITHER IN SERVICE OR ABANDONED.
- THE SOURCE OF UNDERGROUND UTILITIES ARE SHOWN UTILIZING A QUALITY LEVEL SYSTEM:
- QUALITY LEVEL D - UTILITIES SHOWN BASED UPON REFERENCE MAPPING OR ORAL HISTORY. NOT FIELD VERIFIED.
- QUALITY LEVEL C - LOCATION OF UTILITY SURFACE FEATURES SUPPLEMENTS REFERENCE MAPPING. INCLUDES MARKOUT BY OTHERS.
- QUALITY LEVEL B - UTILITY LOCATION DATA IS COLLECTED THROUGH GEOPHYSICAL SENSING TECHNOLOGY TO SUPPLEMENT SURFACE FEATURES AND OR REFERENCE MAPPING. INCLUDES MARKOUT BY CONTROL POINT ASSOCIATES, INC.
- QUALITY LEVEL A - HORIZONTAL AND VERTICAL LOCATION OF UTILITIES ARE OBTAINED USING VACUUM EQUIPMENT EXCAVATION OR OTHER METHODS TO EXPOSE THE UTILITY. LOCATION SHOWN AT SINGLE POINT WHERE EXCAVATION OCCURRED UNLESS UTILITY WAS LOCATED PRIOR TO FILLING.
- ALL FOUR TYPES MAY NOT BE PRESENT ON THIS SURVEY.
4. THIS PLAN IS BASED ON INFORMATION PROVIDED BY CLIENT, A SURVEY PREPARED IN THE FIELD BY CONTROL POINT ASSOCIATES, INC., AND OTHER REFERENCE MATERIAL AS LISTED HEREON.
  5. THIS SURVEY WAS PREPARED WITHOUT THE BENEFIT OF A TITLE REPORT AND IS SUBJECT TO THE RESTRICTIONS, COVENANTS AND/OR EASEMENTS THAT MAY BE CONTAINED THEREIN. IT IS STRONGLY RECOMMENDED THAT A COMPLETE TITLE SEARCH BE PROVIDED TO THE SURVEYOR FOR REVIEW PRIOR TO THE PLACEMENT OF OR ALTERATION TO IMPROVEMENTS ON THE PROPERTY.
  6. EXISTING FIRM: BY GRAPHIC PLOTTING ONLY PROPERTY IS LOCATED IN FLOOD HAZARD ZONE X-UNSHADED (AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN) PER REF. #2.
  7. THE EXISTENCE OF UNDERGROUND STORAGE TANKS, IF ANY, WAS NOT KNOWN AT THE TIME OF THE FIELD SURVEY.
  8. ELEVATIONS REFER TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAV88), BASED ON GPS OBSERVATIONS UTILIZING THE KEYSTONE VRS NETWORK (KEYNETGPS). TEMPORARY BENCH MARKS SET:
    - TBM-A: MAG NAIL SET IN ASPHALT PAVEMENT, ELEVATION= 105.23'
    - TBM-B: MAG NAIL SET IN ASPHALT WALK, ELEVATION= 105.52'
  9. THE OFFSETS SHOWN ARE NOT TO BE USED FOR THE CONSTRUCTION OF ANY STRUCTURE, FENCE, PERMANENT ADDITION, ETC.
  10. ANY DISCREPANCIES FOUND ON THE SURVEY SHOULD BE REPORTED TO THE SURVEYOR IMMEDIATELY.
  11. THIS SURVEY WAS PREPARED FOR THE CLIENT, ABC BEER INC., AND INTENDED ONLY FOR THE USE BY THE CLIENT AS CONTRACTED FOR THE PROJECT AND THE PURPOSE ORIGINALLY INTENDED. NO FURTHER PARTIES OTHER THAN THOSE CERTIFIED HEREON SHALL RELY ON IT FOR ANY PURPOSE.



LOCUS MAP  
©2013 ESRI WORLD STREET MAPS  
NOT TO SCALE



THIS SURVEY HAS BEEN PERFORMED IN THE FIELD UNDER MY SUPERVISION, AND TO THE BEST OF MY KNOWLEDGE, BELIEF, AND INFORMATION, THIS SURVEY HAS BEEN PERFORMED IN ACCORDANCE WITH CURRENTLY ACCEPTED ACCURACY STANDARDS.

NOT A VALID ORIGINAL DOCUMENT UNLESS EMBOSSED WITH RAISED IMPRESSION OR STAMPED WITH A BLUE INK SEAL



01-21-2025  
DATE

GERRY L. HOLDRIGHT, PLS  
MASSACHUSETTS PROFESSIONAL LAND SURVEYOR #49211

FIELD DATE  
12-26-2024  
FIELD BOOK NO.  
24-14- MA  
FIELD BOOK PG.  
13

FIELD CREW  
J.D.O.  
DRAWN:  
A.B.

REVIEWED:  
R.J.K.

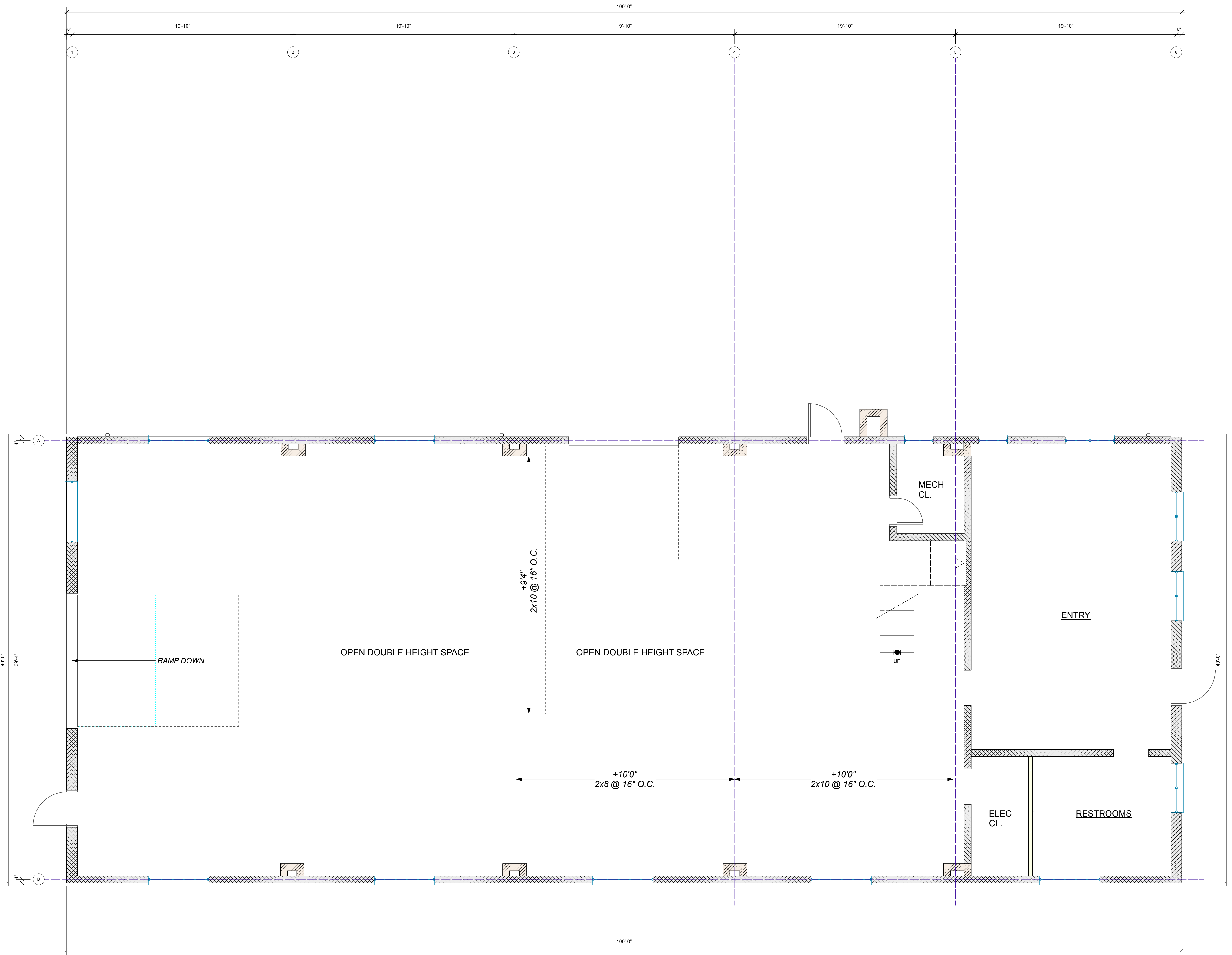
BOUNDARY, TOPOGRAPHIC & UTILITY SURVEY  
ABC BEER INC.

15 RYDER STREET  
LOT 11, BLOCK 2, MAP 57  
TOWN OF ARLINGTON, MIDDLESEX COUNTY  
COMMONWEALTH OF MASSACHUSETTS

CONTROL POINT ASSOCIATES, INC.  
ALBANY, NY 518-217-5010  
CHALFONT, PA 215-712-9800  
HAUPPAUGE, NY 631-880-2645  
MANHATTAN, NY 646-780-0411  
MT LAUREL, NJ 609-857-3999  
WARREN, NJ 908-668-0999

FILE NO.  
03-210320-01  
DWS. NO.  
1 OF 1





NOTES

Issue			Revision		
No.	Date	Issue Notes	No.	Date	Revision Notes

**Reisen Design Associates**  
ARCHITECTURE  
32R Essex Street, Studio 6  
Cambridge MA 02139  
(617) 876-7440

Consultant

Project Title

**Arlington Brewing Co.**  
15 Ryder St.  
Arlington MA

Sheet Title

Existing 1st Floor Plan

**NOT FOR CONSTRUCTION**

Project ID

Drawn By

Reviewed By

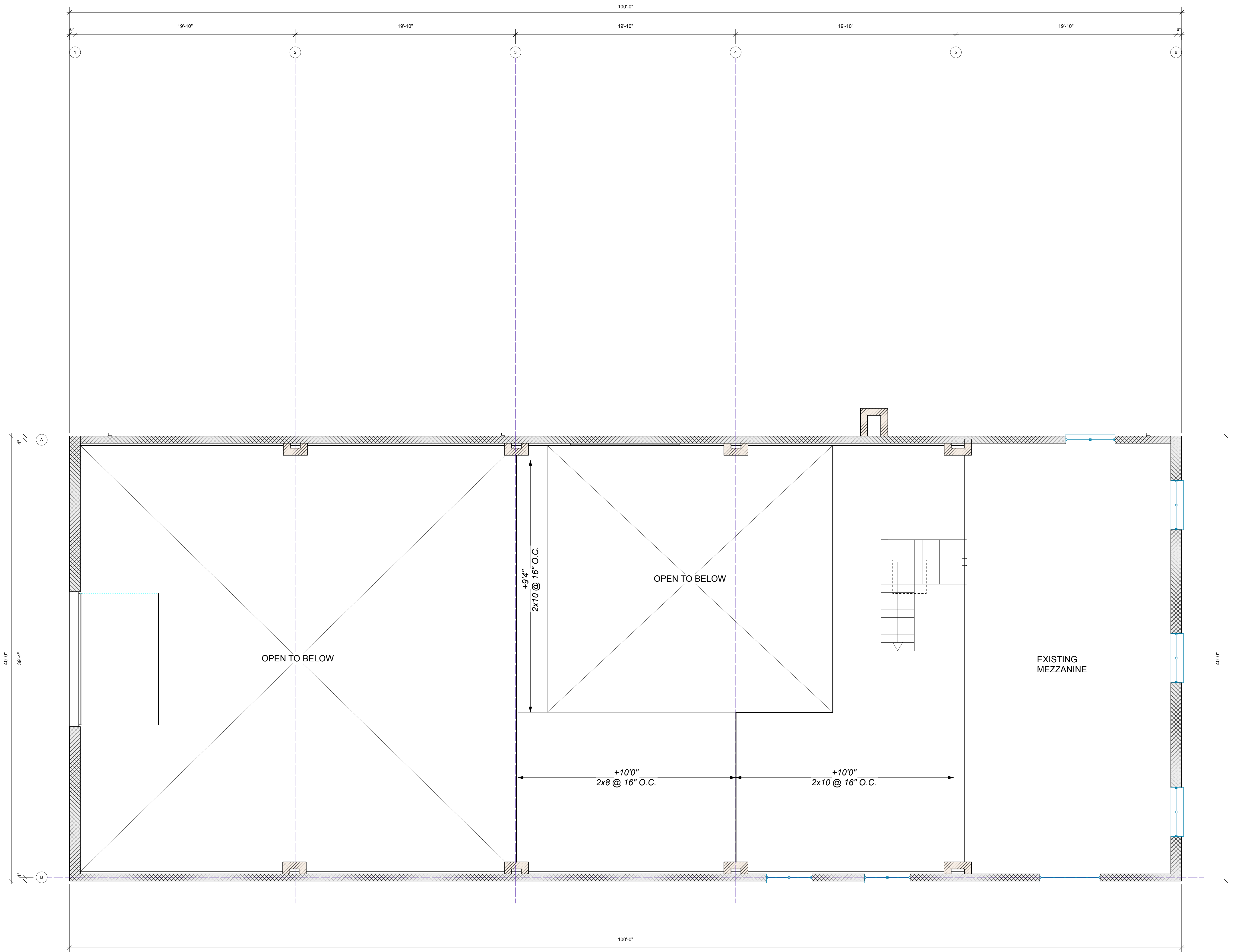
Scale

Date

Drawing No.

**EC110**

01.27.2025



1 Mezzanine Floor Plan  
Scale: 1/4" = 1'-0"

NOTES

Issue			Revision		

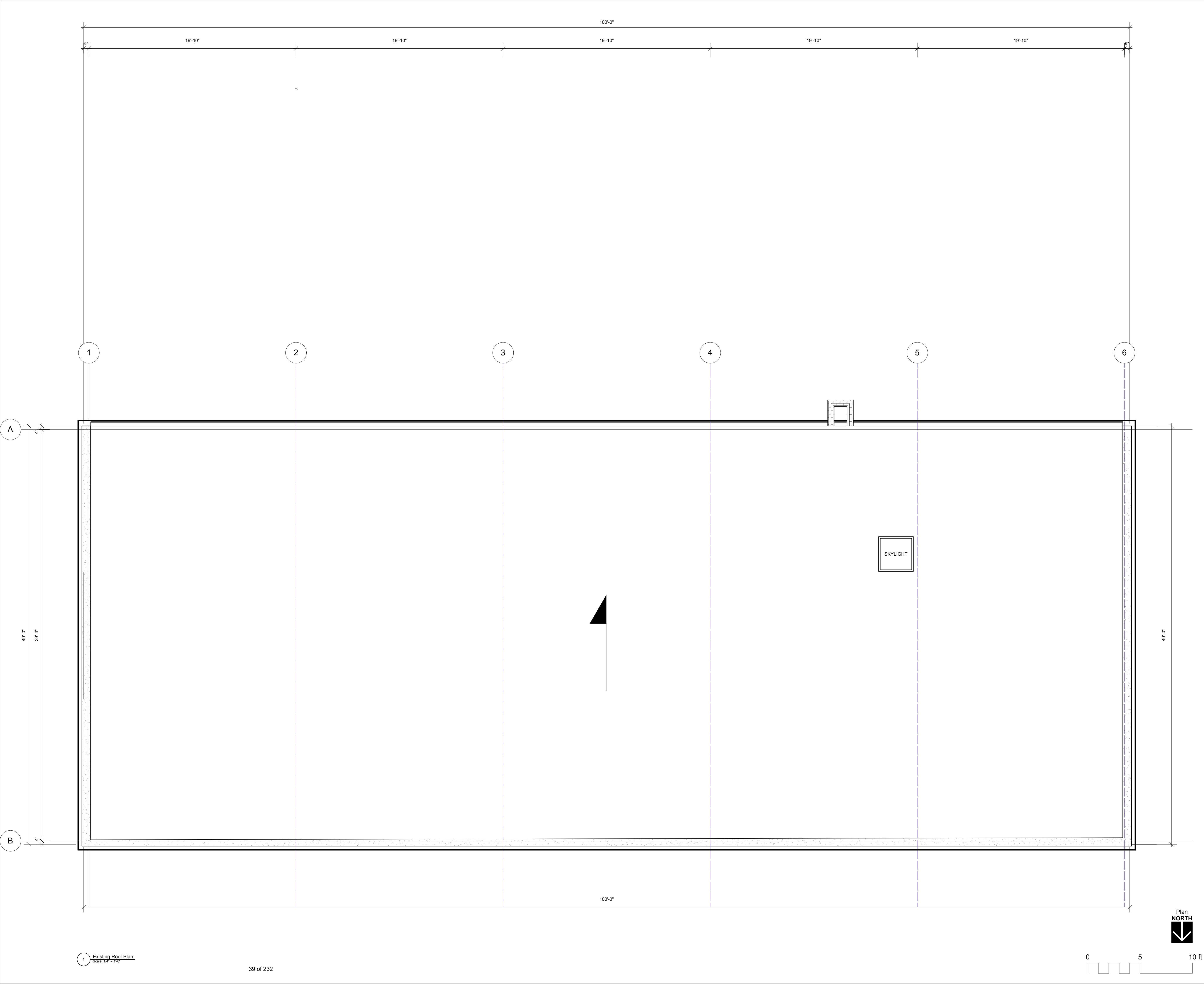
No.	Date	Issue Notes	No.	Date	Revision Notes
-----	------	-------------	-----	------	----------------

<b>Reisen Design Associates</b> ARCHITECTURE  32R Essex Street, Studio 6 Cambridge MA 02139 (617) 876-7440		Consultant
---	--	------------

Project Title <b>Arlington Brewing Co.</b> 15 Ryder St. Arlington MA	
---	--

Sheet Title Existing Mezzanine Floor Plan	
--	--

<b>NOT FOR CONSTRUCTION</b>	Project ID	<b>Drawing No.</b>  <b>EC120</b>
	Drawn By	
	Reviewed By	
	Scale As Noted	
	Date 01.27.2025	



NOTES

Issue			Revision		

No.	Date	Issue Notes	No.	Date	Revision Notes

Reisen Design Associates  
ARCHITECTURE  
32R Essex Street, Studio 6  
Cambridge MA 02139  
(617) 876-7440

Consultant

Project Title

Arlington Brewing Co.  
15 Ryder St.  
Arlington MA

Sheet Title

Existing Roof Plan

NOT FOR CONSTRUCTION

Project ID

Drawn By

Reviewed By

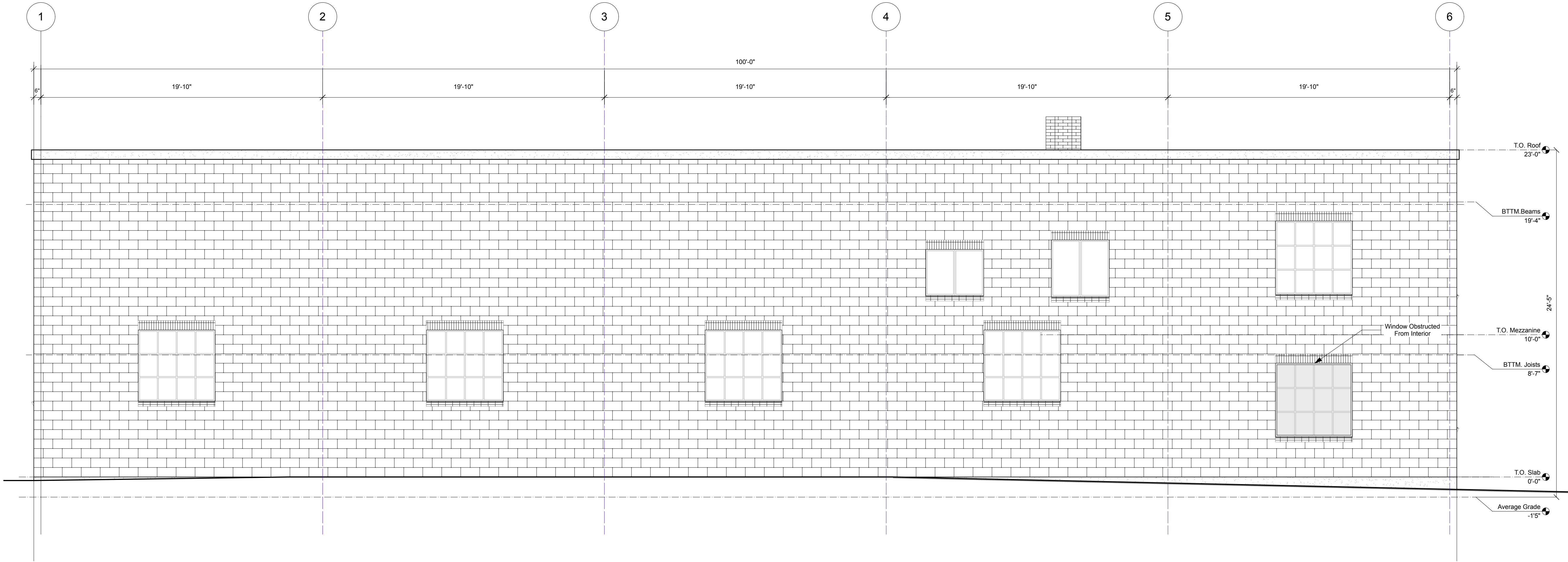
Scale

Date

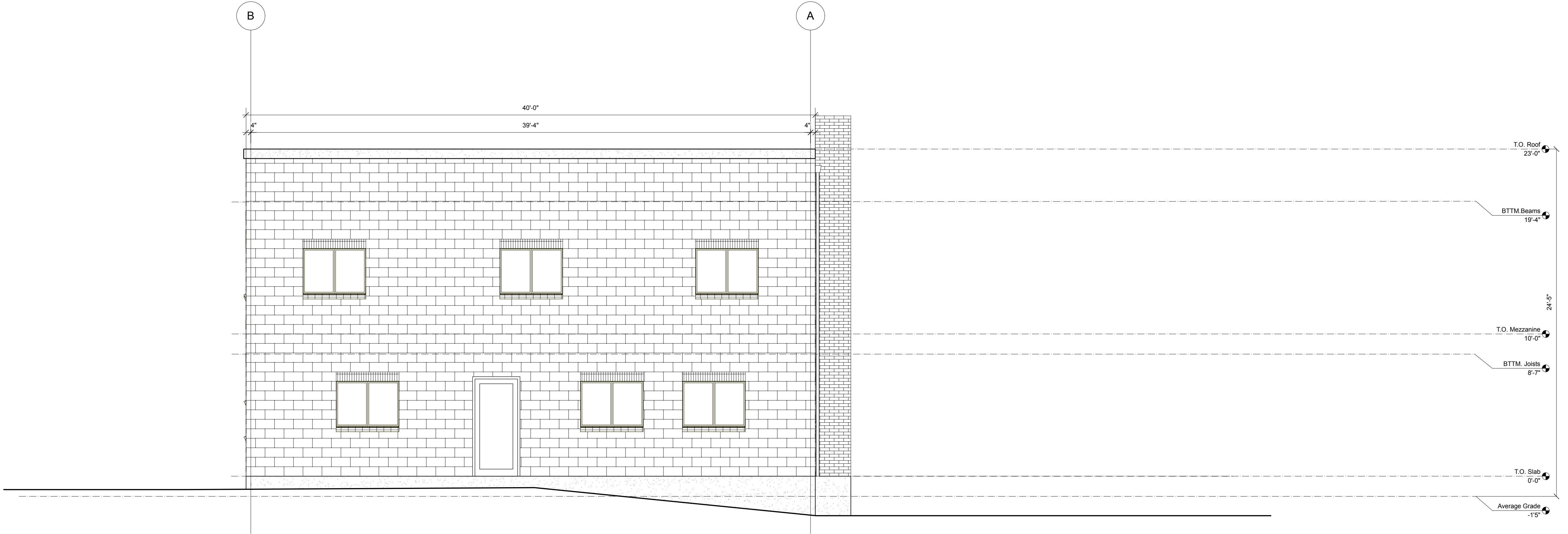
Drawing No.

EC130

01.27.2025



1 North Elevation  
Scale: 1/4" = 1'-0"



2 West Elevation  
Scale: 1/4" = 1'-0"

NOTES

Issue		Revision	
No.	Date	Issue Notes	No. Date Revision Notes

Reisen Design Associates  
ARCHITECTURE  
32R Essex Street, Studio 6  
Cambridge MA 02139  
(617) 876-7440

Consultant

Project Title

Arlington Brewing Co.  
15 Ryder St.  
Arlington MA

Sheet Title

Existing Building Elevations

NOT FOR CONSTRUCTION

Project ID

Drawn By

Reviewed By

Scale

Date

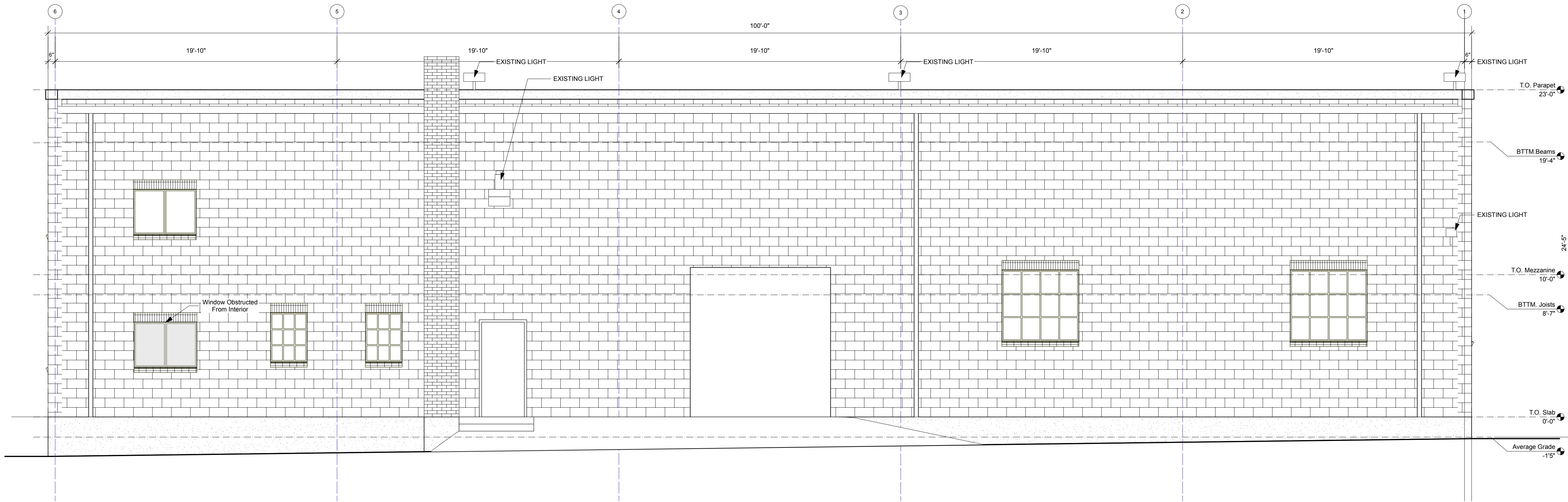
As Noted

01.27.2025

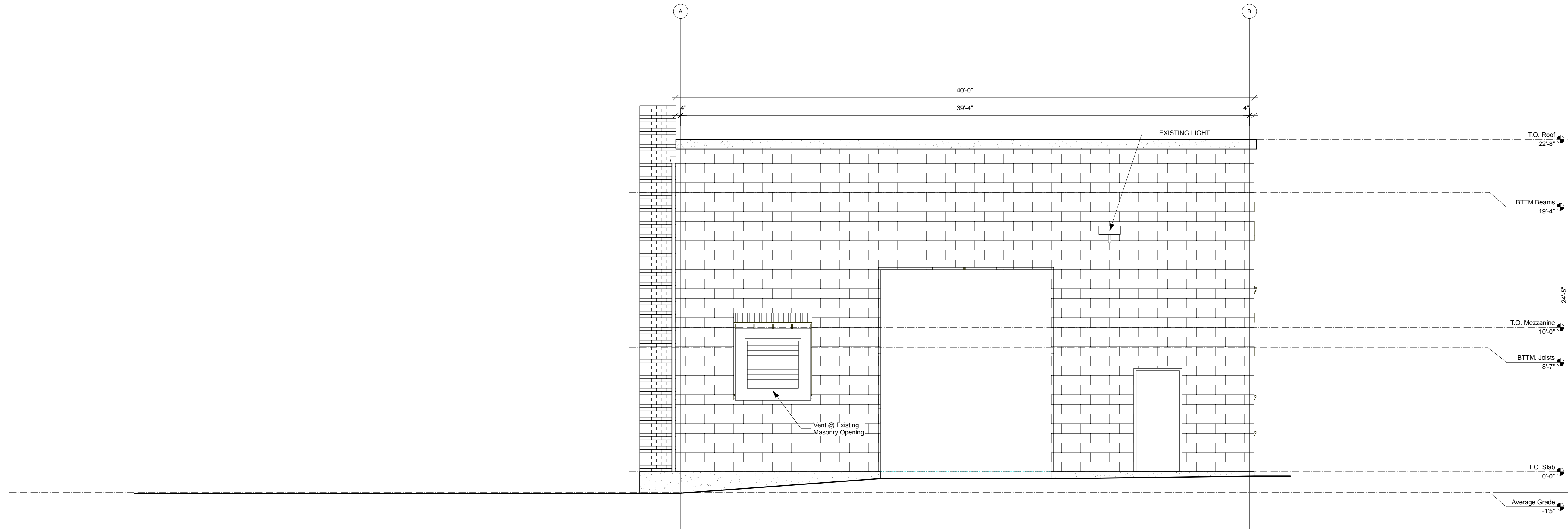
Drawing No.

EC210

40 of 232



1 South Elevation  
Scale: 1/4" = 1'-0"



2 East Elevation  
Scale: 1/4" = 1'-0"

#### NOTES

Issue			Revision		
No.	Date	Issue Notes	No.	Date	Revision Notes

Reisen Design Associates ARCHITECTURE 32R Essex Street, Studio 6 Cambridge MA 02139 (617) 876-7440			Consultant		
--	--	--	------------	--	--

Project Title Arlington Brewing Co. 15 Ryder St. Arlington MA		
--	--	--

Sheet Title Existing Building Elevations		
---	--	--

NOT FOR CONSTRUCTION	Project ID	Drawing No.  EC220
	Drawn By	
	Reviewed By	
	Scale As Noted	
	Date 01.27.2025	





THE COMMONWEALTH OF MASSACHUSETTS REQUIRES NOTIFICATION BY EXCAVATORS, DESIGNERS, OR ANY PERSON PREPARING TO DISTURB THE EARTH'S SURFACE ANYWHERE IN THE COMMONWEALTH.

RYDER STREET  
(40' WIDE - PRIVATE - PER REF #3)  
710 HWY TRAFFIC (ASPHALT ROADWAY)

FOREST STREET  
(40' WIDE - PUBLIC)  
710 HWY TRAFFIC (ASPHALT ROADWAY)

LEGEND

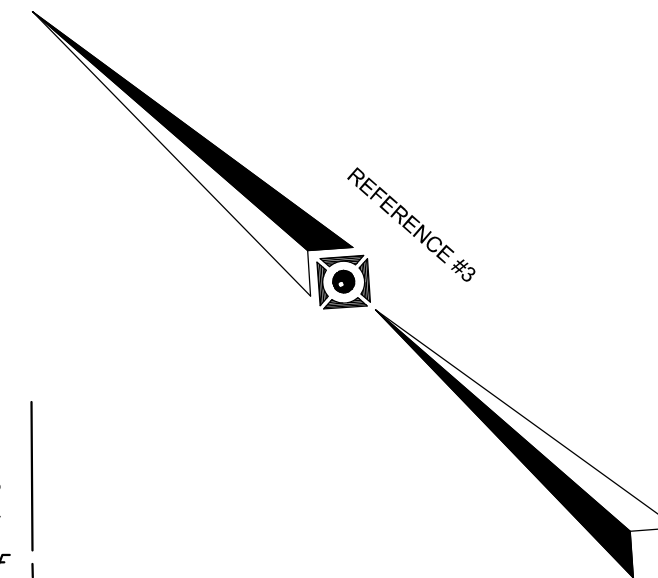
- 124 --- EXISTING CONTOUR
- 125 --- EXISTING SPOT ELEVATION
- X 123.45 EXISTING TOP OF CURB ELEVATION
- X 10 123.45 EXIST. BOTTOM OF CURB ELEVATION
- X BC 122.95
- HYDRANT
- GAS METER
- ELECTRIC METER
- UTILITY POLE
- UTILITY POLE/LIGHT POLE
- SIGN
- BOLLARD
- METAL GUIDE RAIL
- (TYP) TYPICAL
- DELINERATION FLAG
- CONFEROUS TREE & TRUNK SIZE
- SHRUBS
- PARKING SPACE COUNT
- DEPRESSED CURB
- [PARCEL I] TITLE REPORT PARCEL
- (TRACT I) DEED DESIGNATION
- SWL SOLID WHITE LINE
- HT HEIGHT
- BLDG BUILDING
- BFPA BUILDING FOOTPRINT AREA
- EDGE OF CONC.
- EDGE OF PAVEMENT
- LANDSCAPED AREA
- CHAIN LINK FENCE
- FEET
- [40.0'] PROPOSED BUILDING DIMENSIONS
- 1.0' OFFSET OF STRUCTURE AT GROUND LEVEL RELATIVE TO PROPERTY LINE
- SF SQUARE FEET

APPROX LOC OF ORDER OF TAKING BY THE METROPOLITAN DISTRICT COMMISSION (BK. 11536, PG. 213) (& PER REF #14)

MAP 57 BLOCK 2 LOT 11 B N/T LANDS OF THE INHABITANT OF THE TOWN OF ARLINGTON BK. 7566, PG.275

APPROX LOC OF RIGHT-OF-WAY (PER REF #10)

MAP 57 BLOCK 2 LOT 15 C N/T LANDS OF WIRMA TRUCK CENTER, LLC, BK. 68675, PG.134 LOT 8 (PER REF #3)



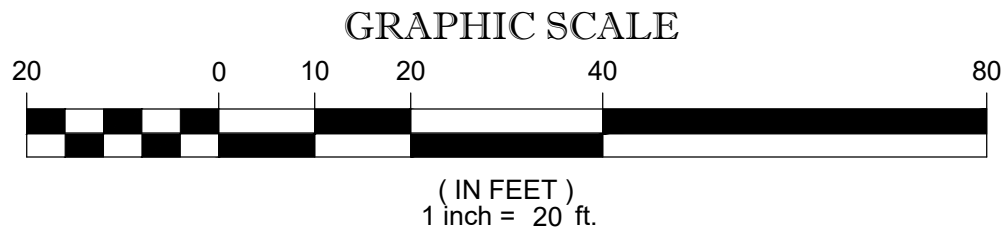
- NOTES:
- PROPERTY KNOWN AS LOT 11 AS SHOWN ON THE TOWN OF ARLINGTON, MIDDLESEX COUNTY, COMMONWEALTH OF MASSACHUSETTS, MAP NO. 57.
  - AREA = 45,887 SQUARE FEET OR 1.049 ACRES.
  - THIS PLAN IS BASED ON INFORMATION PROVIDED BY CLIENT, A SURVEY PREPARED IN THE FIELD BY CONTROL POINT ASSOCIATES, INC., AND OTHER REFERENCE MATERIAL AS LISTED HEREON.
  - EXISTING FIRM: BY GRAPHIC PLOTTING ONLY PROPERTY IS LOCATED IN FLOOD HAZARD ZONE X-UNSHADED (AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN) PER REF. #2.
  - ELEVATIONS REFER TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88), BASED ON GPS OBSERVATIONS UTILIZING THE KEYSTONE VRS NETWORK (KEYNETGPS). TEMPORARY BENCH MARKS SET:
    - TBM-A: MAG NAIL SET IN ASPHALT PAVEMENT, ELEVATION= 105.23'
    - TBM-B: MAG NAIL SET IN ASPHALT WALK, ELEVATION= 105.52'
  - THE OFFSETS SHOWN ARE NOT TO BE USED FOR THE CONSTRUCTION OF ANY STRUCTURE, FENCE, PERMANENT ADDITION, ETC.
  - THE PURPOSE OF THIS PLAN IS TO SHOW THE PROPOSED ADDITION IN RELATION TO THE PROPERTY LINES, ALL IMPROVEMENTS SHOWN HEREON SHOWN BY OTHERS.
  - THE EXISTING BUILDING SHOWN HEREON WILL BE COVERED IN 0.5' OF INSULATION DURING CONSTRUCTION.

- REFERENCES:
- THE TAX ASSESSOR'S MAP OF ARLINGTON, MIDDLESEX COUNTY, MAP 57.
  - MAP ENTITLED "NATIONAL FLOOD INSURANCE PROGRAM, FIRM, FLOOD INSURANCE RATE MAP, MIDDLESEX COUNTY, MASSACHUSETTS (ALL JURISDICTIONS) PANEL 17 OF 176," COMMUNITY-PANEL NUMBER 250250017K, EFFECTIVE DATE JULY 3, 2024.
  - MAP ENTITLED "PLAN OF LOTS AND EASEMENTS IN ARLINGTON, MA," PREPARED BY: RIM ENGINEERING CO., INC., DATED: OCTOBER 23, 2006, AND RECORDED IN THE SOUTH MIDDLESEX REGISTRY OF DEEDS AS PLAN NO. 1072 OF 2007.
  - MAP ENTITLED "PLAN OF LAND IN ARLINGTON, MA," PREPARED BY: RIM ENGINEERING CO., INC., DATED: DECEMBER 28, 2018, AND RECORDED WITH THE SOUTH MIDDLESEX REGISTRY OF DEEDS AS PLAN NO. 110 OF 2019.
  - WATER MAPPING PROVIDED BY THE TOWN ENGINEERING DEPARTMENT.
  - MAP ENTITLED "PLAN OF LOTS AND EASEMENTS IN ARLINGTON, MA," PREPARED BY: RIM ENGINEERING CO., INC., DATED: JANUARY 21, 2003, AND RECORDED IN THE SOUTH MIDDLESEX REGISTRY OF DEEDS AS PLAN NO. 83 OF 2003 IN BOOK 37850 PAGE 612.
  - MAP ENTITLED "PLAN OF LAND IN ARLINGTON MASS." DATED SEPTEMBER 13, 1945, RECORDED IN THE SOUTH MIDDLESEX REGISTRY OF DEEDS AS PLAN NO. 172 OF 1946.
  - MAP ENTITLED "AS-BUILT GEOTHERMAL SITE PLAN 1167R MASSACHUSETTS AVE. ARLINGTON, MA PREPARED FOR ACHIEVE RENEWABLE" DATES JANUARY 4, 2016 PREPARED BY PAUL LINDHOLM, P.E.
  - MAP ENTITLED "ALTANSPS LAND TITLE SURVEY, 15 RYDER STREET, TOWN OF ARLINGTON, MIDDLESEX COUNTY COMMONWEALTH OF MASSACHUSETTS," PREPARED BY CONTROL POINT ASSOCIATES, INC., DATED SEPTEMBER 28, 2021.
  - MAP ENTITLED "SUB-DIVISION OF LAND IN ARLINGTON MASS.," PREPARED BY: FRANK K. NICKSAY, CIVIL ENGINEER, DATED: AUGUST 2, 1947, AND RECORDED WITH THE MIDDLESEX COUNTY REGISTRY OF DEEDS IN BOOK 7178, PAGE 331 AS PLAN 1232 OF 1947.
  - MAP ENTITLED "PLAN OF LAND IN ARLINGTON MASS. TAKEN FOR STORM DRAIN PURPOSES," PREPARED BY: JAMES M. KEANE, TOWN ENGINEER, DATED: OCTOBER 16, 1937, AND RECORD WITH THE MIDDLESEX COUNTY REGISTRY OF DEEDS AS PLAN 992 OF 1937.
  - MAP ENTITLED "PLAN OF LAND IN ARLINGTON MASS. TAKEN FOR STORM DRAIN PURPOSES," PREPARED BY: JAMES M. KEANE, TOWN ENGINEER, DATED: DECEMBER 15, 1939, AND RECORD WITH THE MIDDLESEX COUNTY REGISTRY OF DEEDS AS PLAN 1086 OF 1939.
  - MAP ENTITLED "PLAN OF LAND IN ARLINGTON MASS. TAKEN FOR SEWER PURPOSES," PREPARED BY: JAMES M. KEANE, TOWN ENGINEER, DATED: DECEMBER 14, 1939, AND RECORD WITH THE MIDDLESEX COUNTY REGISTRY OF DEEDS AS PLAN 1086 OF 1939.
  - MAP ENTITLED "THE COMMONWEALTH OF MASSACHUSETTS METROPOLITAN DISTRICT COMMISSION SEWERAGE DIVISION PLAN OF LAND IN ARLINGTON," DATED: FEBRUARY 1968, AND RECORD WITH THE MIDDLESEX COUNTY REGISTRY OF DEEDS AS PLAN 729 OF 1968.
  - MAP ENTITLED "PLAN AND PROFILE OF STORM DRAIN IN RYDER STREET," PREPARED BY JAMES M. KEANE TOWN ENGINEER, DATED: AUGUST 8, 1949, FILED IN THE TOWN OF ARLINGTON ENGINEERS OFFICE AS PLAN 4822.
  - MAP ENTITLED "SEWER ASSESSMENT PLAN NO. 477," PREPARED BY J. M. KEANE TOWN ENGINEER, DATED FEBRUARY 8, 1929, FILED IN THE TOWN OF ARLINGTON ENGINEERS OFFICE AS PLAN 2676.
  - MAP ENTITLED "BOUNDARY, TOPOGRAPHIC & UTILITY SURVEY, ABC BEER INC., 15 RYDER STREET, LOT 11, BLOCK 2, MAP 57, TOWN OF ARLINGTON, MIDDLESEX COUNTY, COMMONWEALTH OF MASSACHUSETTS," PREPARED BY CONTROL POINT ASSOCIATES, INC., DATED JANUARY 21, 2025.
  - CAD FILE ENTITLED "25\_01\_17 RYDER ST. AWNING," PROVIDED BY REISEN DESIGN ASSOCIATES ON JANUARY 17, 2025.

ZONING INFORMATION  
DISTRICT: INDUSTRIAL (I)  
SOURCE: TOWN OF ARLINGTON ZONING MAP

ITEMS	REQUIRED
MIN LOT AREA	N/A
MIN FRONTAGE	N/A
MIN FRONT YARD	10 FT.
MIN SIDE YARD	10 FT.
MIN REAR YARD	10 FT.
MAX BUILDING HEIGHT	39 FT.

NOTE: ZONING CRITERIA IDENTIFIED HEREON ARE BASED UPON PRELIMINARY RESEARCH AND PRESENTED FOR REFERENCE ONLY. SAME MUST BE CONFIRMED WITH LOCAL ZONING OFFICIAL AND LEGAL COUNSEL TO CONFIRM VALIDITY.



THIS SURVEY HAS BEEN PERFORMED IN THE FIELD UNDER MY SUPERVISION, AND TO THE BEST OF MY KNOWLEDGE, BELIEF, AND INFORMATION, THIS SURVEY HAS BEEN PERFORMED IN ACCORDANCE WITH CURRENTLY ACCEPTED ACCURACY STANDARDS.

NOT A VALID ORIGINAL DOCUMENT UNLESS EMBOSSED WITH RAISED IMPRESSION OR STAMPED WITH A BLUE INK SEAL



GERRY L. HOLDRIGHT, PLS  
MASSACHUSETTS PROFESSIONAL LAND SURVEYOR #49211

01-24-2025  
DATE

1	ADD PROPOSED DECK	N/A	G.R.D.	G.L.H.	01-24-2025
No.	DESCRIPTION OF REVISION	FIELD CREW	DRAWN	APPROVED	DATE
FIELD DATE	12-26-2024				
FIELD BOOK NO.	24-14- MA				
FIELD BOOK PG.	13				
FIELD CREW	J.D.O.				
DRAWN:	G.R.D.				
REVIEWED:	G.L.H.				
APPROVED:	G.L.H.				
DATE	01-23-2025				
SCALE	1" = 20'				
FILE NO.	03-210320-01				
DWS. NO.	1 OF 1				

**PROPOSED PLOT PLAN OF LAND**  
**ABC BEER INC.**  
15 RYDER STREET  
LOT 11, BLOCK 2, MAP 57  
TOWN OF ARLINGTON, MIDDLESEX COUNTY  
COMMONWEALTH OF MASSACHUSETTS

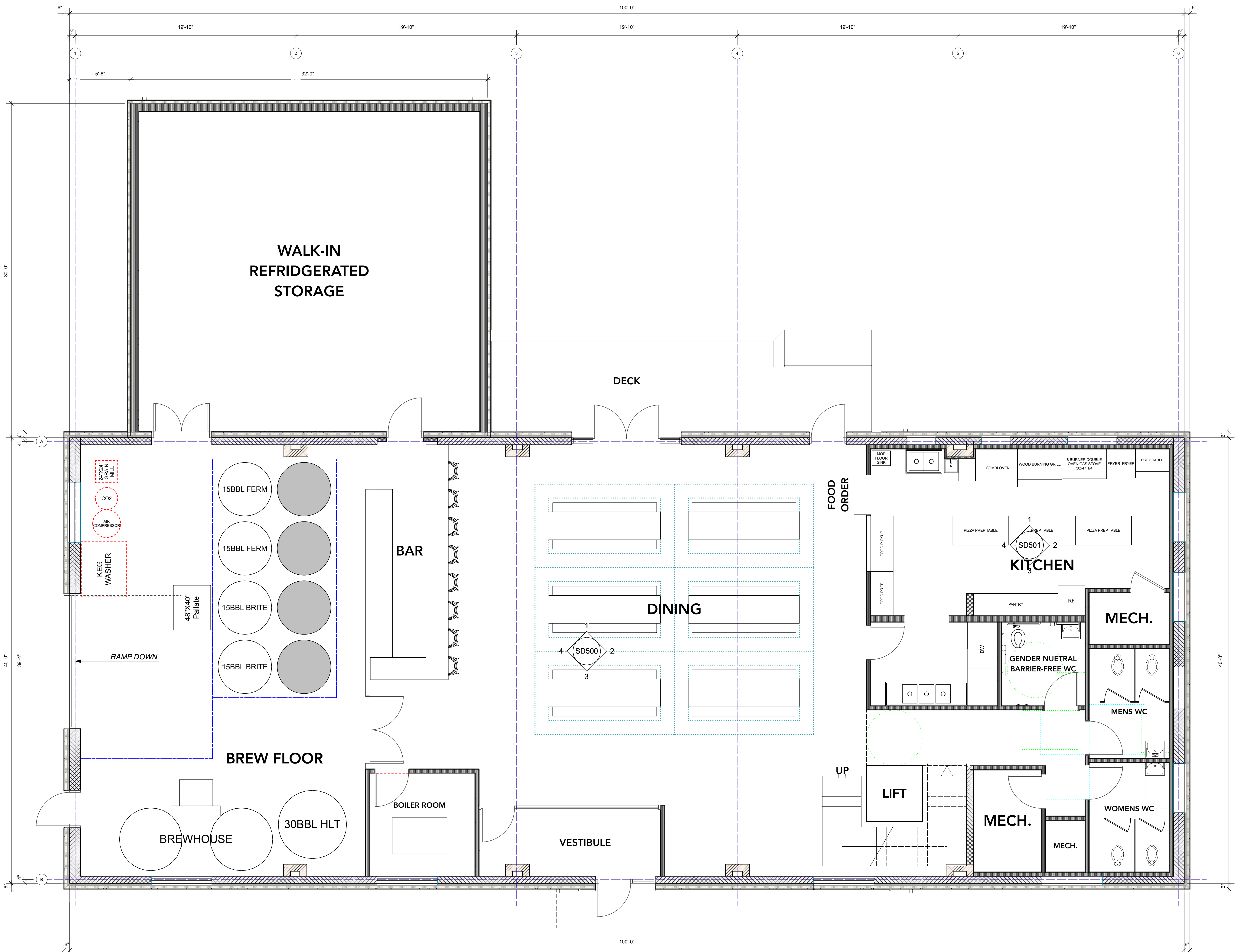
**CONTROL POINT ASSOCIATES, INC.**  
352 TURNPIKE ROAD  
SOUTH BROOKFIELD, MA 01772  
508.948.3000 - 508.948.3003 FAX  
WWW.CPASURVEY.COM

ALBANY, NY 518-217-5010  
CHALFONT, PA 215-712-9800  
HAUPPAUGE, NY 631-880-2645  
MANHATTAN, NY 646-780-0411  
MT LAUREL, NJ 609-357-3999  
WARREN, NJ 908-668-0999









NOTES

Issue			Revision		
No.	Date	Issue Notes	No.	Date	Revision Notes

**Reisen Design Associates**  
ARCHITECTURE  
32R Essex Street, Studio 6  
Cambridge MA 02139  
(617) 876-7440

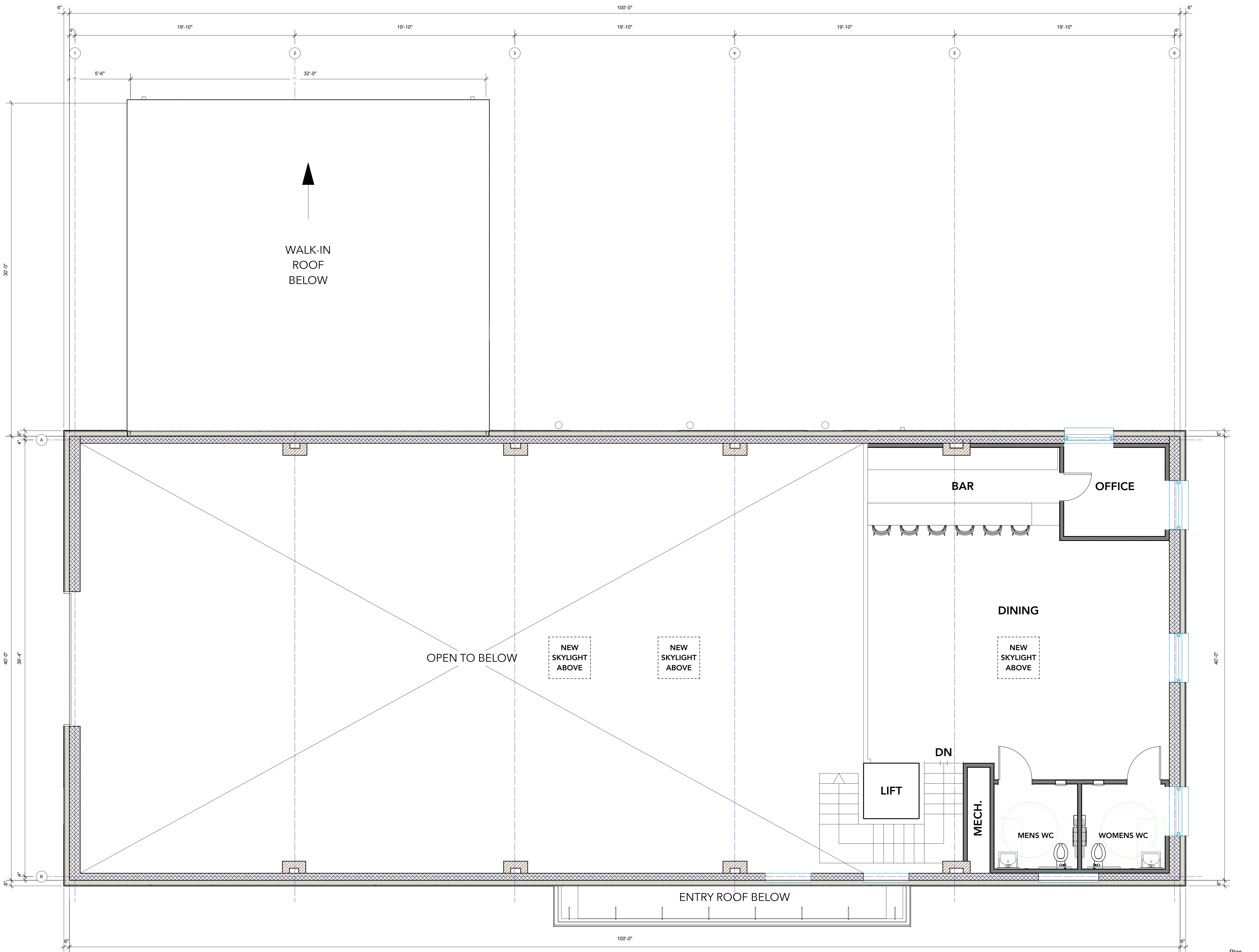
Project Title  
**Arlington Brewing Co.**  
15 Ryder St.  
Arlington MA

Sheet Title  
Proposed 1st Floor Plan

<b>NOT FOR CONSTRUCTION</b>	Project ID	Drawing No.
	Drawn By	<b>A110</b>
	Reviewed By	
	Scale	
	Date	

As Noted  
01.27.2025





NOTES

Issue			Revision		

No.	Date	Issue Notes	No.	Date	Revision Notes
-----	------	-------------	-----	------	----------------

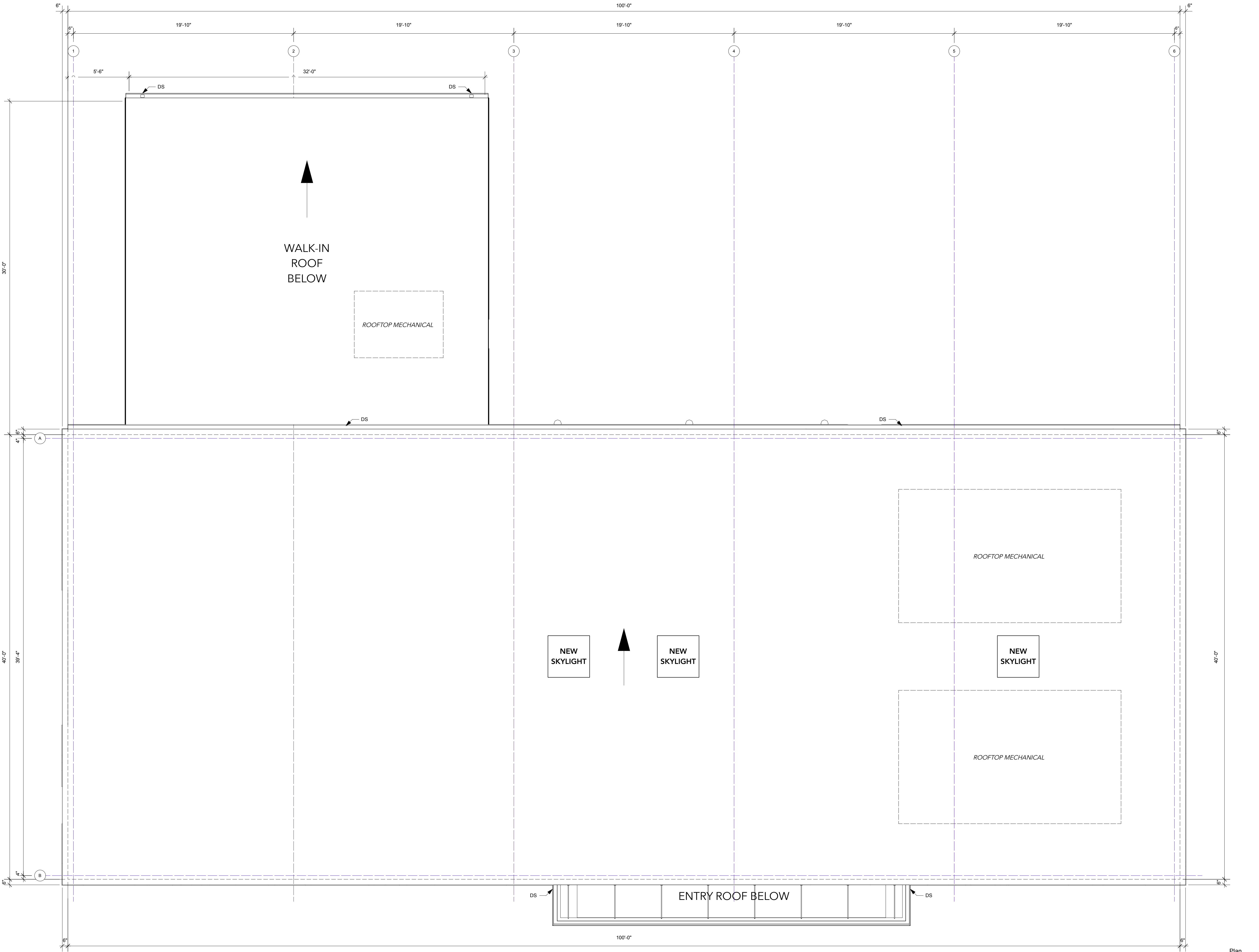
**Reisen Design Associates**  
ARCHITECTURE  
32R Essex Street, Studio 6  
Cambridge MA 02139  
(617) 876-7440

Consultant

Project Title  
**Arlington Brewing Co.**  
15 Ryder St.  
Arlington MA

Sheet Title  
Proposed Mezzanine Floor Plan

<b>NOT FOR CONSTRUCTION</b>	Project ID	Drawing No.
	Drawn By	<b>A120</b>
	Reviewed By	
	Scale	
	Date	
As Noted		01.27.2025



NOTES

Issue			Revision		
No.	Date	Issue Notes	No.	Date	Revision Notes

**Reisen Design Associates**  
ARCHITECTURE  
  
32R Essex Street, Studio 6  
Cambridge MA 02139  
  
(617) 876-7440

Consultant

Project Title

**Arlington Brewing Co.**  
15 Ryder St.  
Arlington MA

Sheet Title

Proposed Roof Plan

**NOT FOR CONSTRUCTION**

Project ID

Drawn By

Reviewed By

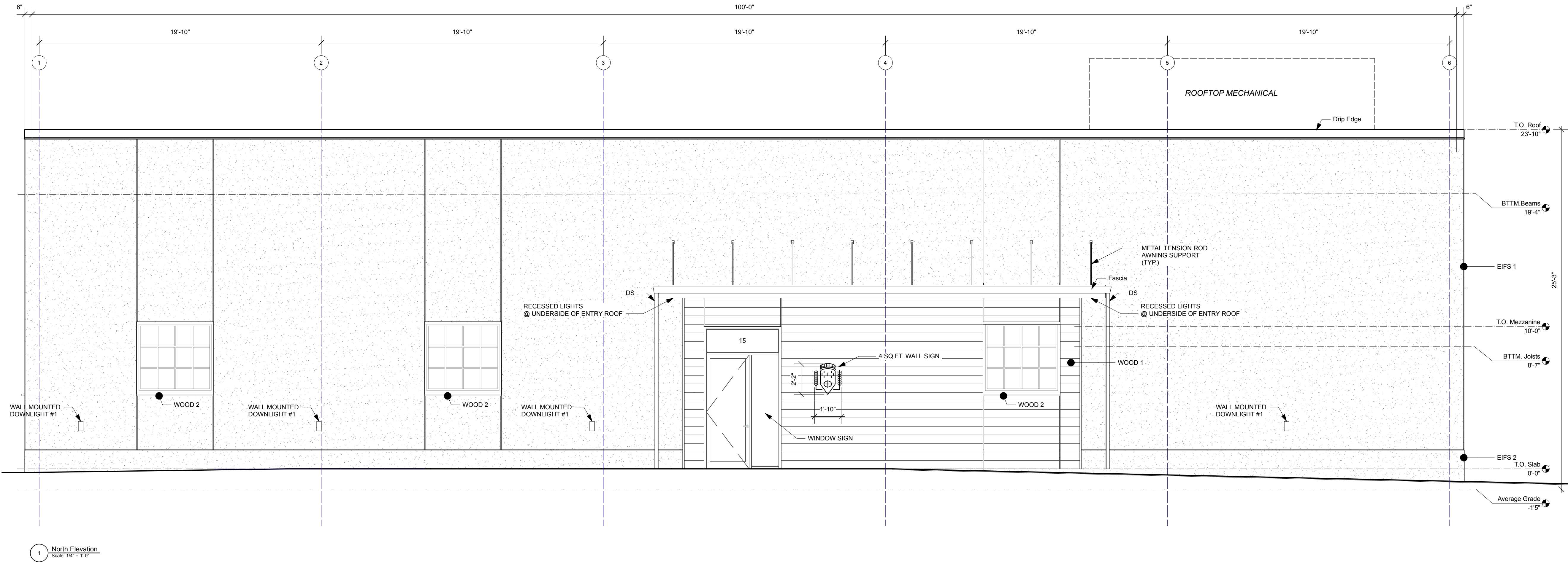
Scale

Date

Drawing No.

**A130**





NOTES

EIFS COLOR 1

EIFS COLOR 2

WOOD 1

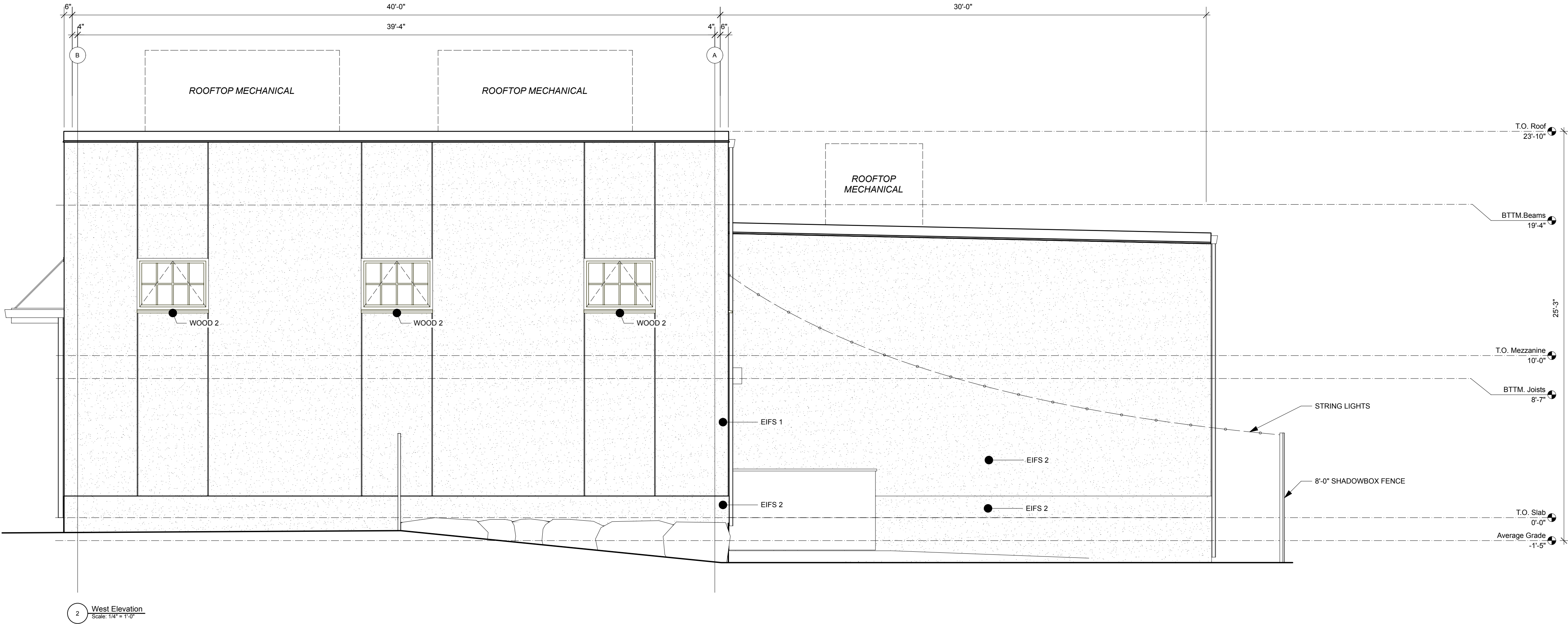
WOOD 2

WINDOW FRAMES

STOREFRONT

FASCIA & GUTTER  
@ AWNING

DRIP EDGE & GUTTER  
@ BUILDING



Issue			Revision		

No.	Date	Issue Notes	No.	Date	Revision Notes

Reisen Design Associates  
ARCHITECTURE  
32R Essex Street, Studio 6  
Cambridge MA 02139  
(617) 876-7440

Consultant

Project Title

Arlington Brewing Co.  
15 Ryder St.  
Arlington MA

Sheet Title

Proposed Building Elevations

NOT  
FOR  
CONSTRUCTION

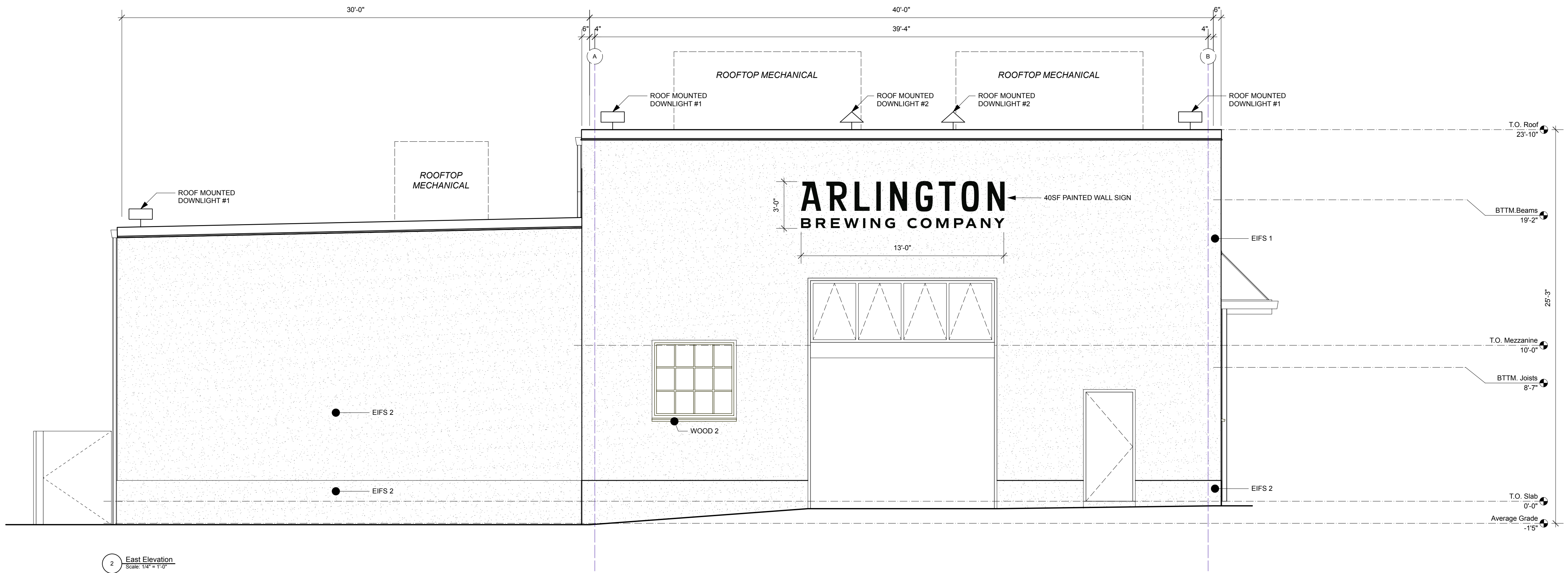
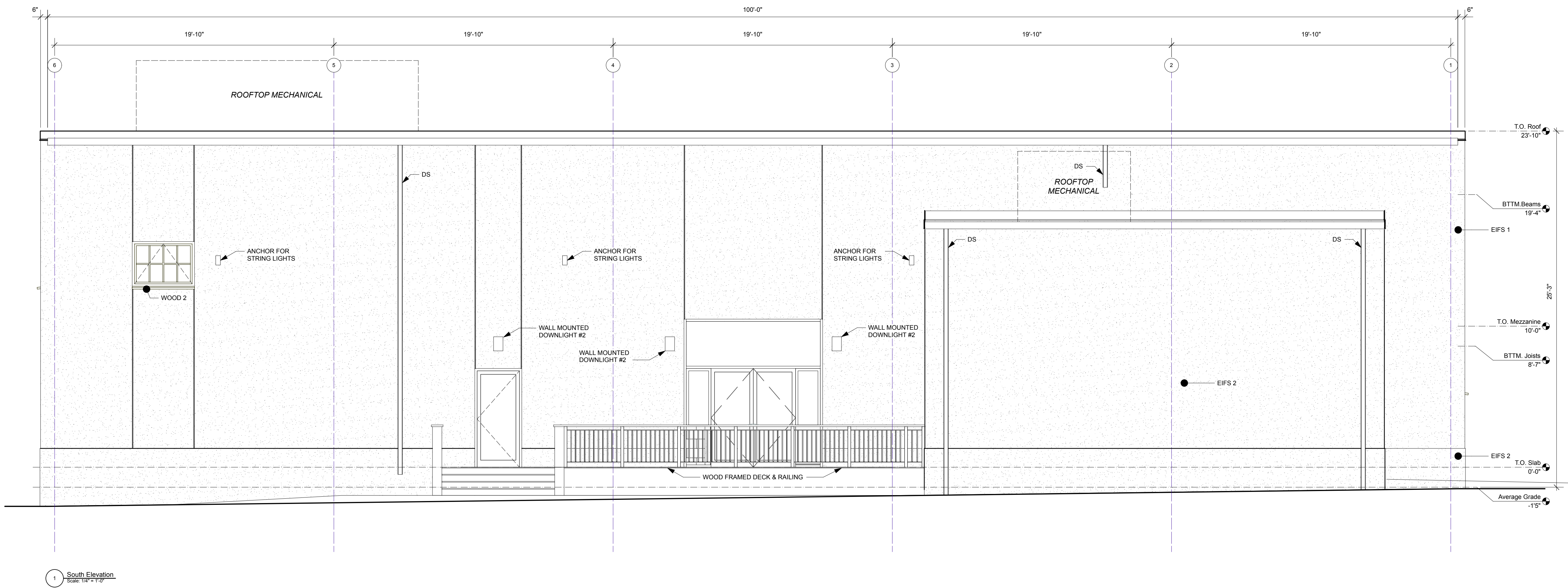
Project ID	Drawing No.
Drawn By	
Reviewed By	
Scale	
Date	

As Noted

01.27.2025

A210





NOTES

EIFS COLOR 1

EIFS COLOR 2

WOOD 1

WOOD 2

WINDOW FRAMES

STOREFRONT

FASCIA & GUTTER  
@ AWNING

DRIP EDGE & GUTTER  
@ BUILDING

Issue		Revision	
No.	Date	Issue Notes	Revision Notes

Reisen Design Associates

ARCHITECTURE

32R Essex Street, Studio 6  
Cambridge MA  
02139  
(617) 876-7440

Consultant

Project Title

Arlington Brewing Co.  
15 Ryder St.  
Arlington MA

Sheet Title

Proposed Building Elevations

NOT  
FOR  
CONSTRUCTION

Project ID

Drawn By

Reviewed By

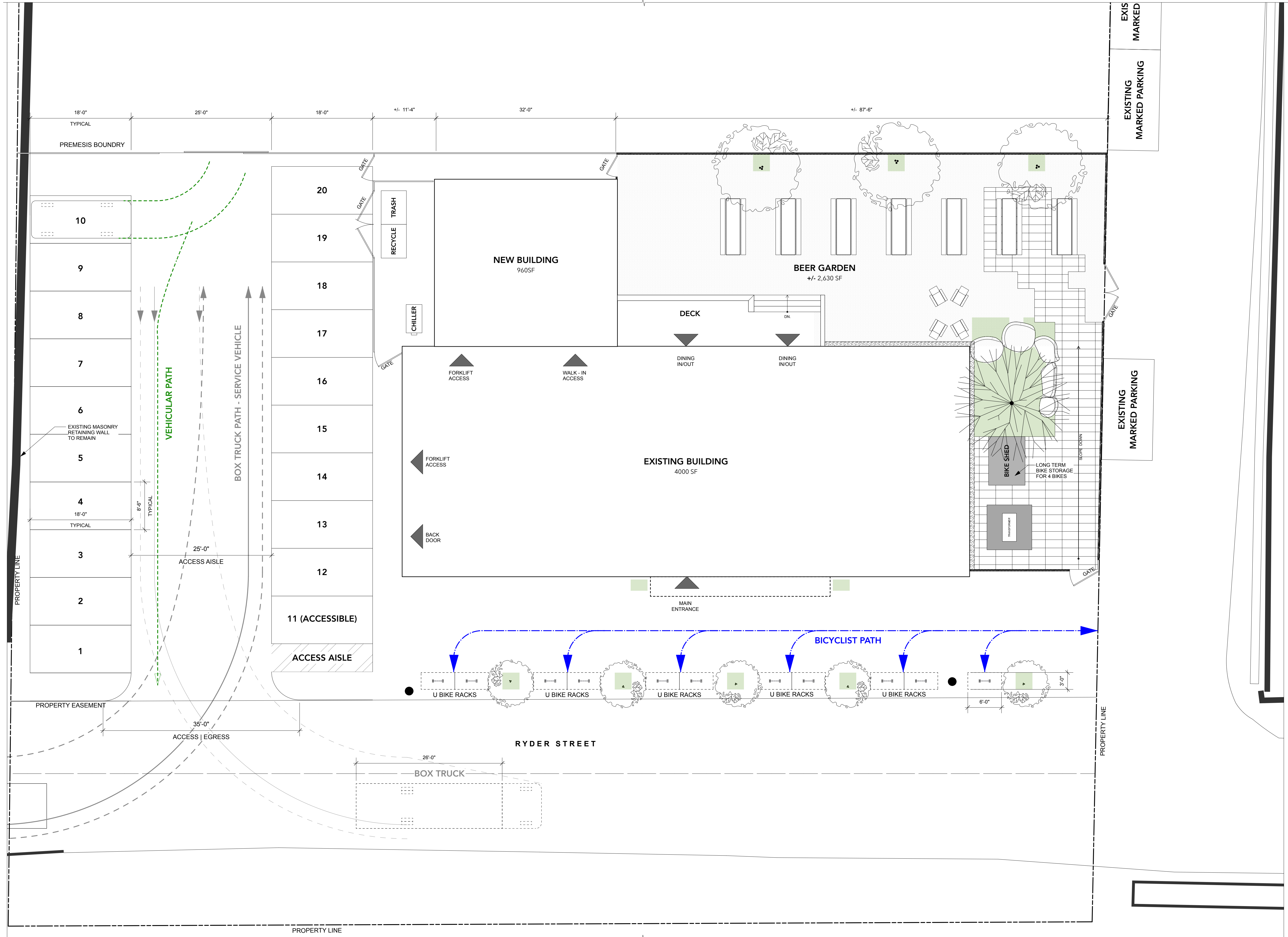
Scale

Date

Drawing No.

A220

01.27.2025



NOTES

Issue			Revision		
No.	Date	Issue Notes	No.	Date	Revision Notes
<div>Reisen Design Associates</div> <div>ARCHITECTURE</div> <div>32R Essex Street, Studio 6</div> <div>Cambridge MA</div> <div>02139</div> <div>(617) 876-7440</div>			Consultant		
Project Title					
Arlington Brewing Co.					
15 Ryder St.					
Arlington MA					
Sheet Title					
Proposed Vehicular, Bicycle and Service Vehicle Plan					
NOT FOR CONSTRUCTION	Project ID		Drawing No.		
	Drawn By		A020		
	Reviewed By				
	Scale				
	As Noted				
	Date				
01.27.2025					





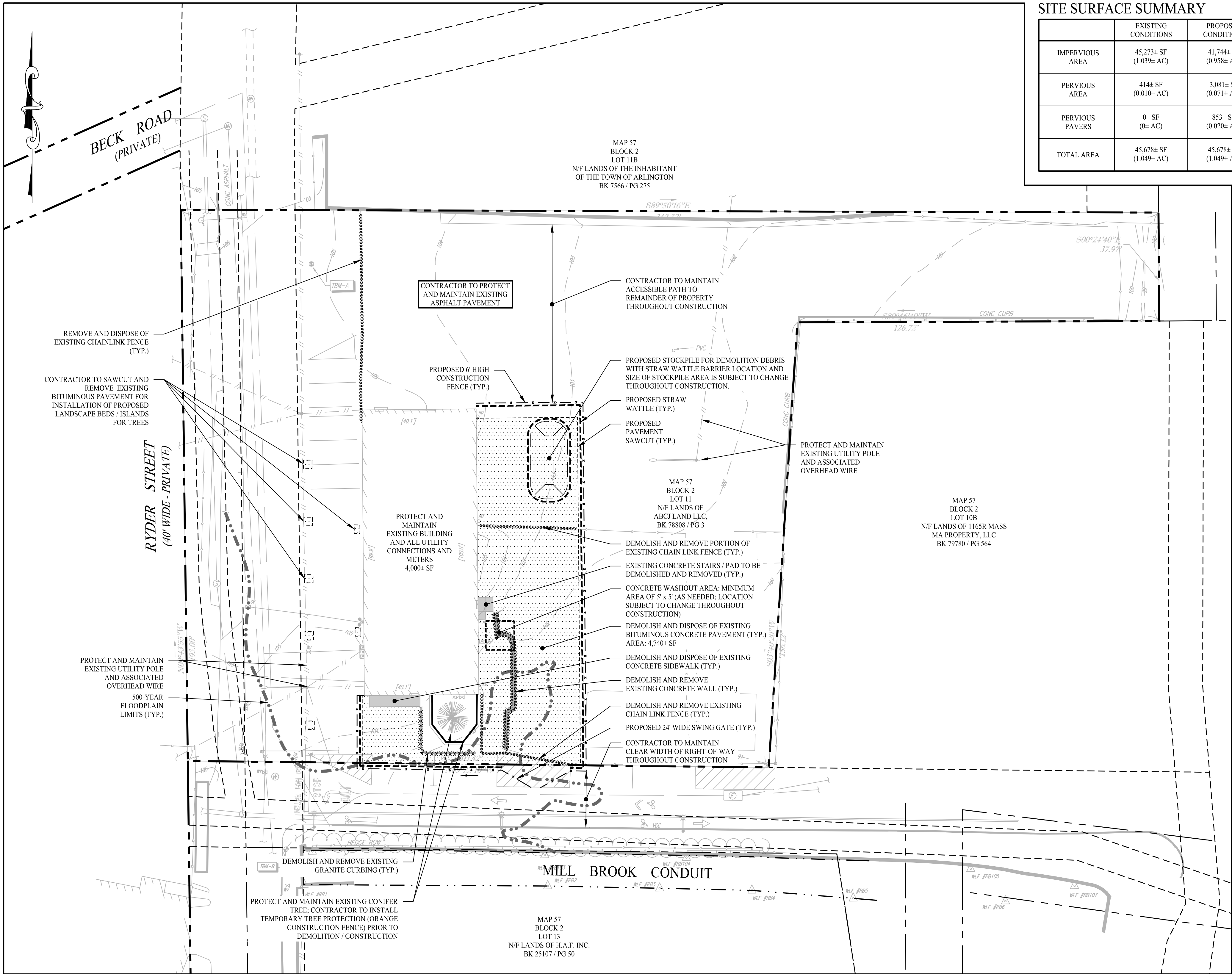
# LEED v4.1 BD+C Project Checklist

Project Name: Arlington Brewing Company - 15 Ryder St  
Date: 2025-01-22

Y	?	N				
			Credit	Integrative Process	1	Comment / Narrative
7	0	0	<b>Location and Transportation</b>			<b>16</b>
			Credit	LEED for Neighborhood Development Location	16	
1			Credit	Sensitive Land Protection	1	This project is building on previously developed land.
			Credit	High Priority Site and Equitable Development	2	
4			Credit	Surrounding Density and Diverse Uses	5	The walk score for 15 Ryder St is 86
			Credit	Access to Quality Transit	5	The location does not meet this requirement. Requirerent is 72/30 trips (weekday/weekend) . Existing total is 60/36. Route 77 = 36/36, Route 67 = 24/0
			Credit	Bicycle Facilities	1	The project meets this criteria except for showers.
1			Credit	Reduced Parking Footprint	1	Parking footprint is 24.6% of baseline requirement - Appendix 4 requirement is 81.3 TOTAL ==> 77.4 restaurant (3872sf*20spots/1000sf) + 3.9 manufacturing (2133sf*1.85spots/1,000sf)
1			Credit	Electric Vehicles	1	Stretch energy code requires rediness for 2 EV stations
3	0	0	<b>Sustainable Sites</b>			<b>10</b>
Y			Prereq	Construction Activity Pollution Prevention	Required	
1			Credit	Site Assessment	1	Project takes into account various site considerations.
1			Credit	Protect or Restore Habitat	2	Project inclues 22% restored area, with calculation based on leased premises.
			Credit	Open Space	1	
			Credit	Rainwater Management	3	
			Credit	Heat Island Reduction	2	
1			Credit	Light Pollution Reduction	1	Light pollution considerations are required by town bylaws
8	0	0	<b>Water Efficiency</b>			<b>11</b>
Y			Prereq	Outdoor Water Use Reduction	Required	
Y			Prereq	Indoor Water Use Reduction	Required	
Y			Prereq	Building-Level Water Metering	Required	
2			Credit	Outdoor Water Use Reduction	2	No irrigation required for landscaping.
3			Credit	Indoor Water Use Reduction	6	36% reduction from current baseline.
2			Credit	Optimize Process Water Use	2	Reclaim 100% of brewing process water for reuse in product.
1			Credit	Water Metering	1	Install dedicated water meters for indoor plumbing and fixtures and brewing process
8	2	0	<b>Energy and Atmosphere</b>			<b>33</b>
Y			Prereq	Fundamental Commissioning and Verification	Required	
Y			Prereq	Minimum Energy Performance	Required	
Y			Prereq	Building-Level Energy Metering	Required	
Y			Prereq	Fundamental Refrigerant Management	Required	
	2		Credit	Enhanced Commissioning	6	Building Enclosure Commissioning
4			Credit	Optimize Energy Performance	18	Compliance with stretch energy code results in a minimum of 4 points
			Credit	Advanced Energy Metering	1	
			Credit	Grid Harmonization	2	
3			Credit	Renewable Energy	5	100% renewable energy from Arlington Community Energy
1			Credit	Enhanced Refrigerant Management	1	Use only refrigerants with ODP=0, GWP<50
5	2	0	<b>Materials and Resources</b>			<b>13</b>
Y			Prereq	Storage and Collection of Recyclables	Required	
5			Credit	Building Life-Cycle Impact Reduction	5	Build will reuse at least 60% of existing structural elements, with the goal to get to 75%
	1		Credit	Environmental Product Declarations	2	Strive to use products with EPD certifications.
	1		Credit	Sourcing of Raw Materials	2	Responsible sourcing of at least 15% building products
			Credit	Material Ingredients	2	
			Credit	Construction and Demolition Waste Management	2	



1	5	0	Indoor Environmental Quality			16	
Y			Prereq	Minimum Indoor Air Quality Performance	Required		
Y			Prereq	Environmental Tobacco Smoke Control	Required		
	1		Credit	Enhanced Indoor Air Quality Strategies	2	We plan to install entryway system, filter outdoor air, filter recirculated air, operable windows, and CO2 monitor.	
	2		Credit	Low-Emitting Materials	3		Low VOC material categories include paint, insulation, ceilings.
			Credit	Construction Indoor Air Quality Management Plan	1		
			Credit	Indoor Air Quality Assessment	2		
			Credit	Thermal Comfort	1		
1	1		Credit	Interior Lighting	2	We will install a lighting control system that provides dimmable lighting. Additionally, we will target glare reduction and color rendering strategies.	
			Credit	Daylight	3		
			Credit	Quality Views	1		
	1		Credit	Acoustic Performance	1	Sound transmission, reverbration time	
1	0	0	Innovation			6	
1			Credit	Innovation	5	Reduced parking footprint achieves 75% reduction	
			Credit	LEED Accredited Professional	1		
4	0	0	Regional Priority			4	
1			Credit	Regional Priority: Specific Credit - Indoor water use reduction	1		
1			Credit	Regional Priority: Specific Credit - Renewable energy	1		
1			Credit	Regional Priority: Specific Credit - Optimize energy performance	1		
1			Credit	Regional Priority: Specific Credit - Building lifecycle impact reduction	1		
37	9	0	TOTALS			Possible Points: 110	
Certified: 40 to 49 points, Silver: 50 to 59 points, Gold: 60 to 79 points, Platinum: 80 to 110							



#### SITE SURFACE SUMMARY

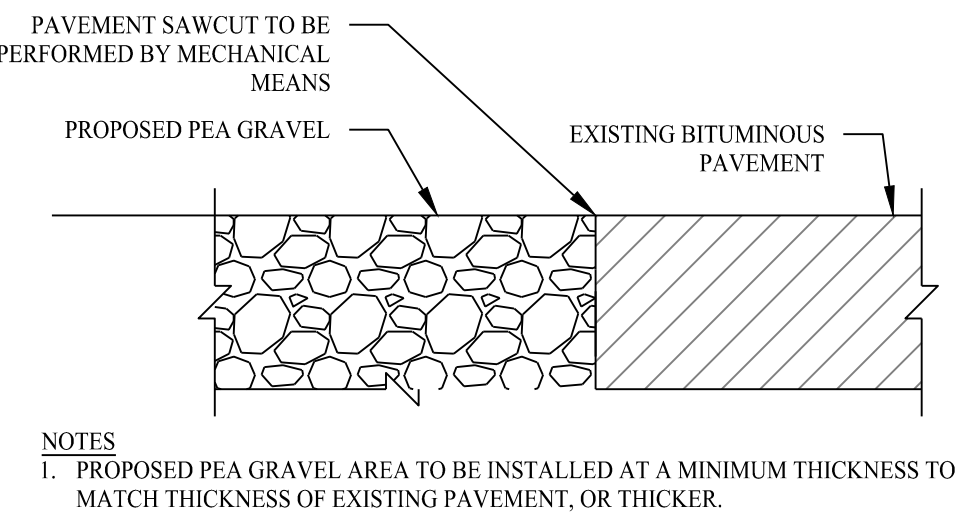
	EXISTING CONDITIONS	PROPOSED CONDITIONS
IMPERVIOUS AREA	45,273± SF (1.039± AC)	41,744± SF (0.958± AC)
PERVIOUS AREA	414± SF (0.010± AC)	3,081± SF (0.071± AC)
PERVIOUS PAVERS	0± SF (0± AC)	853± SF (0.020± AC)
TOTAL AREA	45,678± SF (1.049± AC)	45,678± SF (1.049± AC)

#### CONSTRUCTION SEQUENCE (PHASE 1)

1. **PHASE I**
  - 1.1. ENSURE ALL BONDING / FEES / ZONING PERMITS ARE PAID AND APPROVED BY TOWN OF A PRIOR TO CONSTRUCTION.
  - 1.2. INSTALL CONSTRUCTION FENCE AND ESTABLISH CONTRACTOR PARKING AREA AND STAGING / STORAGE AREAS.
  - 1.3. INSTALL STRAW WATTLE BARRIER ON INSIDE OF CONSTRUCTION FENCE.
  - 1.4. PREPARE TEMPORARY PARKING, STAGING AND STORAGE AREAS.
  - 1.5. INSTALL SILT SACK INLET PROTECTION ON ALL EXISTING DRAINAGE STRUCTURES WITHIN THE PROJECT SITE AND WITHIN PROXIMITY OF THE SITE.
  - 1.6. HALT ALL ACTIVITIES AND CONTACT THE ENGINEER OF RECORD / TOWN OF ARLINGTON LAND USE AGENT TO PERFORM INSPECTION AND CERTIFICATION OF BEST MANAGEMENT PRACTICES (BMPs). GENERAL CONTRACTOR SHALL SCHEDULE AND CONDUCT THE STORM WATER PRE-CONSTRUCTION MEETING WITH THE ENGINEER OF RECORD / TOWN AGENCIES, AND GROUND-DISTURBING CONTRACTOR BEFORE PROCEED WITH CONSTRUCTION.
  - 1.7. PERFORM PAVEMENT SAWCUTS.
  - 1.8. DEMOLISH AND REMOVE EXISTING STRUCTURES / FOUNDATIONS.
  - 1.9. REMOVE ALL EXISTING CURBING, BITUMINOUS CONCRETE PAVEMENT, CONCRETE PADS, AND FENCING.
  - 1.10. ESTABLISH MATERIAL STOCKPILE AREA AND INSTALL SEC BARRIER SURROUNDING PILE.
  - 1.11. BEGIN ROUGH GRADING OF THE SITE.

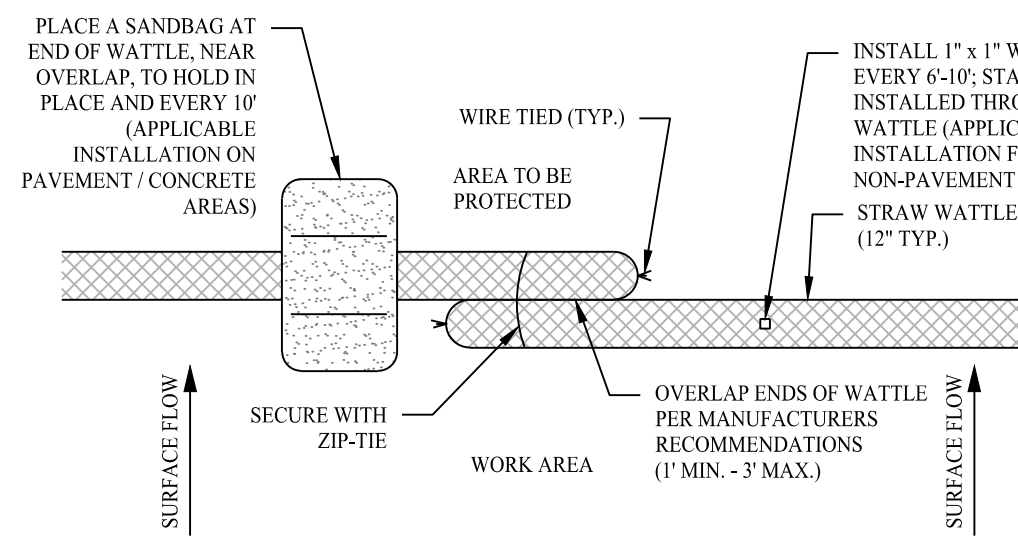
#### LEGEND

	PROPERTY LINE
	RIGHT-OF-WAY LINE
	ADJOINING LOT LINE
	LIMIT OF EASEMENT
	PAVEMENT SAWCUT
	STRAW WATTLE BARRIER
	CONCRETE WASHOUT AREA
	CONSTRUCTION FENCE
	CONCRETE SIDEWALK DEMOLITION
	BITUMINOUS CONCRETE PAVEMENT DEMOLITION
	CURB REMOVAL
	CONCRETE WALL / CHAIN LINK FENCE REMOVAL
	TREE PROTECTION / ORANGE CONSTRUCTION FENCE



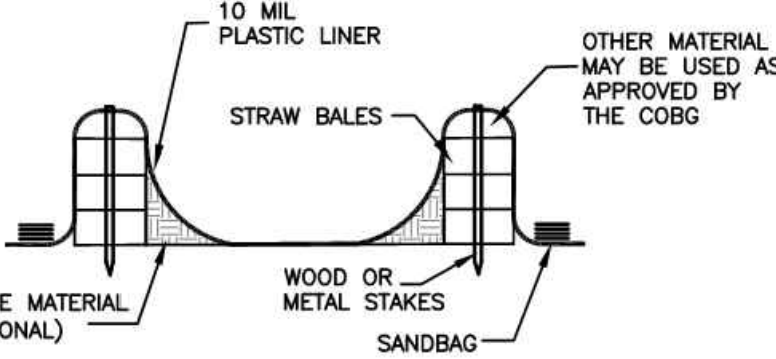
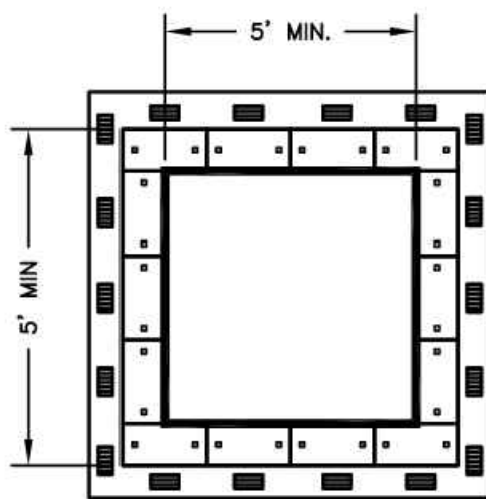
#### PAVEMENT SAWCUT DETAIL

SCALE: NTS



#### STAKED STRAW WATTLE

SCALE: NTS



- NOTES:
1. NO WASHING OUT OF CONCRETE TRUCKS OR WASHING OF SWEEPING FROM EXPOSED AGGREGATE CONCRETE INTO STORM DRAINS, OPEN DITCHES, STREETS OR STREAMS IS PERMITTED.
  2. EXCESS CONCRETE IS NOT PERMITTED TO BE DUMPED ON-SITE, EXCEPT IN DESIGNATED TEMPORARY WASHOUT AREAS (REFER TO SHEET 2.32).
  3. ONCE WASHOUT AREA IS 75% FULL, CONTRACTOR TO CLEAN OUT OR REPLACE AREA.
  4. PLASTIC LINING SHALL BE MINIMUM 10 MIL. POLYETHYLENE SHEETING AND WILL BE FREE OF HOLES, TEARS, OR OTHER DEFECTS.
  5. HARDENED CONCRETE WILL BE REMOVED AND DISPOSED OF OFF-SITE. MATERIALS USED TO CONSTRUCT TEMPORARY CONCRETE WASHOUT FACILITIES WILL BE REMOVED FROM THE SITE AND DISPOSED OF.

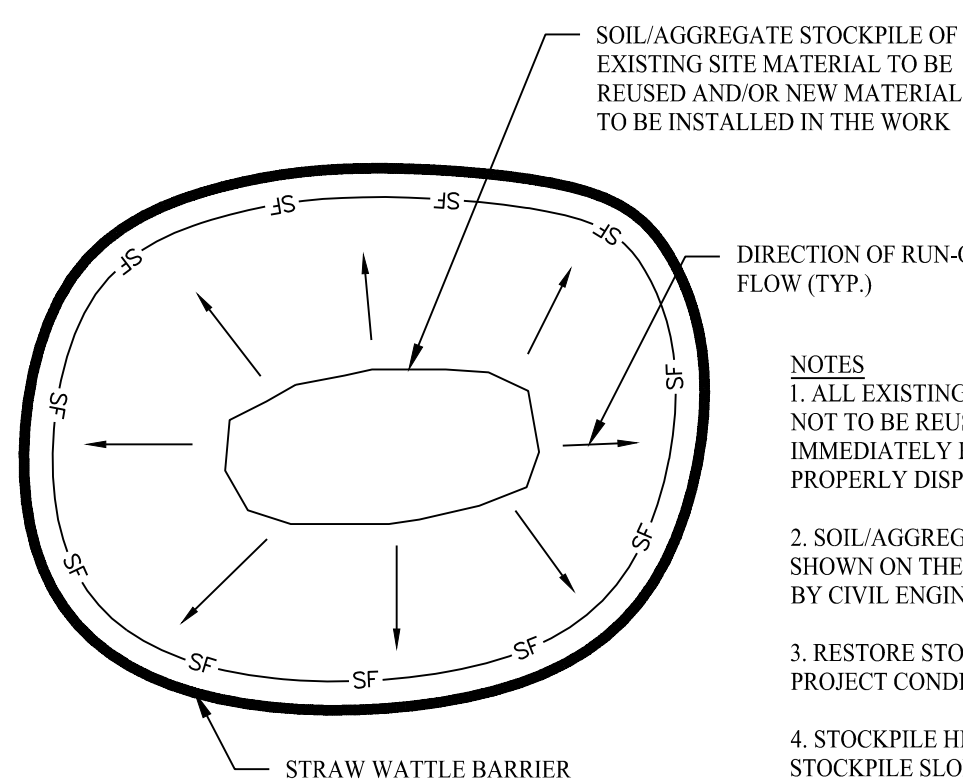
#### CONCRETE WASHOUT DETAIL

SCALE: NTS

REFER TO PROPOSED PLOT PLAN OF LAND FOR PROPOSED IMPROVEMENTS PREPARED BY CONTROL POINT ASSOCIATES, INC.

#### PHASE I - DEMOLITION & SOIL EROSION & SEDIMENT CONTROL PLAN

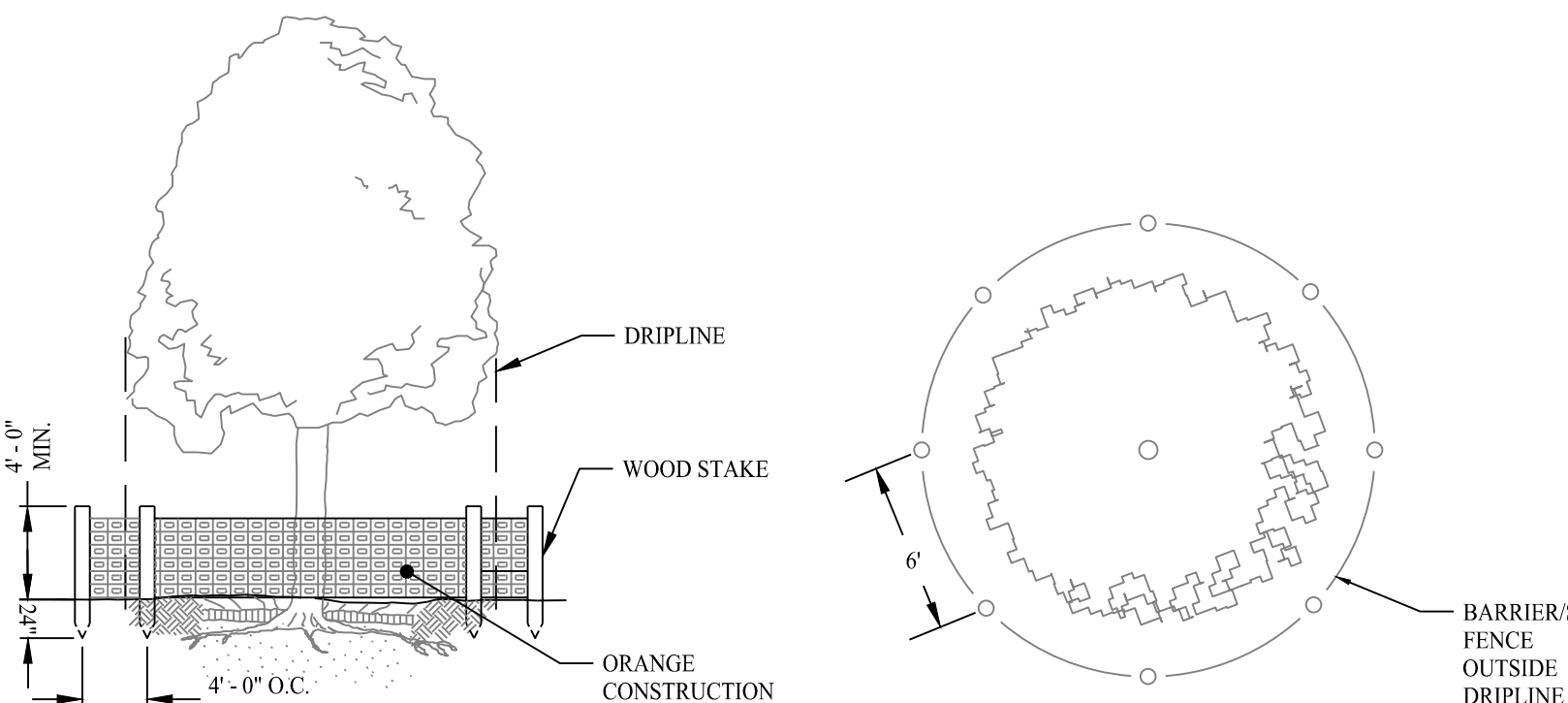
SCALE: 1" = 20'



#### MATERIALS STOCKPILE DETAIL BARRIER

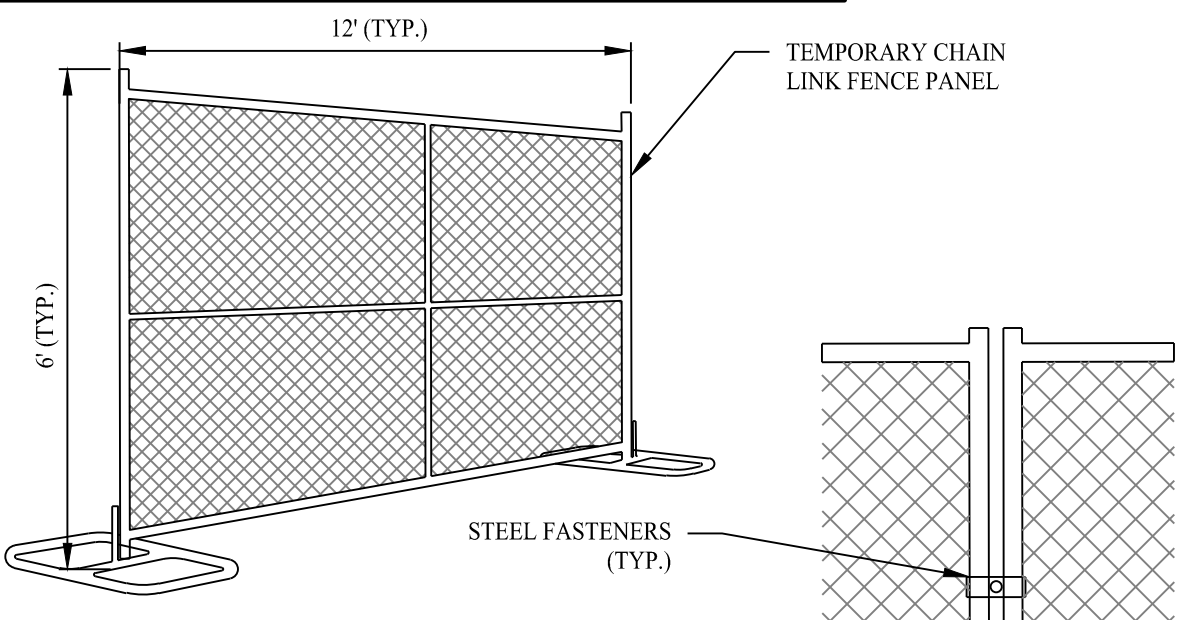
SCALE: NTS

52 of 232



#### TYPICAL TREE PROTECTION DETAIL

SCALE: NTS



#### TEMPORARY CONSTRUCTION FENCE DETAIL

SCALE: NTS

#### GENERAL NOTES

1. THESE PLANS ARE FOR PERMITTING PURPOSES ONLY AND ARE NOT FOR CONSTRUCTION. NO CONSTRUCTION OR DEMOLITION SHALL BEGIN UNTIL APPROVAL OF THE FINAL PLANS IS GRANTED BY ALL GOVERNING AND REGULATORY AGENCIES.
2. ALL SITE WORK TO BE COMPLETED IN ACCORDANCE WITH ALL PERMITS, APPROVALS, AND CONDITIONS OF APPROVALS ISSUED BY THE TOWN OF ARLINGTON, MASSACHUSETTS FOR THIS PROJECT.
3. EXISTING SITE CONDITIONS AND BOUNDARY INFORMATION TAKEN FROM A PLAN TITLED "BOUNDARY, TOPOGRAPHIC & UTILITY SURVEY; ABC BEER INC.", DATED JANUARY 21, 2025, SCALE: 1"=20'. PREPARED BY CONTROL POINT ASSOCIATES, INC.
- 3.1. FIELD SURVEY PERFORMED IN DECEMBER 2024.
- 3.2. THE VERTICAL DATUM IS NAVD83.
4. THE PROJECT SITE CONSISTS OF ONE (1) PARCEL WITH A TOTAL LAND AREA OF APPROXIMATELY 1.049± ACRES (45,687± SF) LOCATED WITHIN THE TOWN OF ARLINGTON INDUSTRIAL (I) ZONING DISTRICT.
- 4.1. MAP 57 BLOCK 2 LOT 11 AS SHOWN ON THE TOWN OF ARLINGTON, MIDDLESEX COUNTY, COMMONWEALTH OF MASSACHUSETTS ASSESSORS MAPS.
- 4.2. DEED REFERENCE: BK 78808 / PG 3
5. PRIOR TO DEMOLITION OR CONSTRUCTION, THE CONTRACTOR SHALL CONTACT "DIG SAFE" 72 HOURS BEFORE THE COMMENCEMENT OF WORK AT "811" AND VERIFY ALL UTILITIES AND STORM DRAINAGE SYSTEM LOCATIONS. INFORMATION ON EXISTING UTILITIES AND STORM DRAINAGE SYSTEMS HAS BEEN COMPILED FROM AVAILABLE INFORMATION INCLUDING UTILITY PROVIDER AND MUNICIPAL RECORD MAPS AND/OR FIELD SURVEY AND IS NOT GUARANTEED CORRECT OR COMPLETE. UTILITIES AND STORM DRAINAGE SYSTEMS ARE SHOWN TO ALERT THE CONTRACTOR TO THEIR PRESENCE AND THE CONTRACTOR IS SOLELY RESPONSIBLE FOR DETERMINING ACTUAL LOCATIONS AND ELEVATIONS OF ALL UTILITIES AND STORM DRAINAGE SYSTEMS INCLUDING SERVICES.
6. SHOULD ANY UNCHARTED OR INCORRECTLY CHARTED, EXISTING PIPING OR OTHER UTILITY BE UNCOVERED DURING EXCAVATION, CONSULT THE ENGINEER OF RECORD IMMEDIATELY FOR DIRECTIONS BEFORE PROCEEDING FURTHER WITH WORK IN THIS AREA.
7. CONTRACTOR SHALL BE RESPONSIBLE FOR SITE SAFETY AND SECURITY OF THE SITE DURING ALL PHASES OF CONSTRUCTION. THE ARCHITECT AND ENGINEER OF RECORD HAVE NO CONTRACTUAL DUTY TO CONTROL THE SAFEST METHODS OR MEANS OF THE WORK, JOB SITE RESPONSIBILITIES, SUPERVISION OR TO SUPERVISE SAFETY AND DOES NOT VOLUNTARILY ASSUME ANY SUCH DUTY OR RESPONSIBILITY.
8. THE OWNER IS RESPONSIBLE FOR OBTAINING ALL NECESSARY ZONING PERMITS REQUIRED BY GOVERNMENT AGENCIES PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL OBTAIN ALL LOCAL AND STATE PERMITS. THE CONTRACTOR SHALL POST ALL BONDS, PAY ALL FEES, PROVIDE PROOF OF INSURANCE, AND PROVIDE TRAFFIC CONTROLS NECESSARY FOR THIS PROJECT.
9. CONTRACTOR SHALL REFERENCE ARCHITECTURAL PLANS / PROPOSED PLOT PLAN FOR EXACT DIMENSIONS AND CONSTRUCTION DETAILS OF BUILDING, SIDEWALKS, PATIO AREAS AND ALL OTHER IMPROVEMENTS.
10. THE CONTRACTOR SHALL RESTORE ANY DRAINAGE STRUCTURE, PIPE, UTILITY, PAVEMENT, CURBS, SIDEWALKS, LANDSCAPED AREAS, OR SIGNAGE DISTURBED DURING CONSTRUCTION TO THEIR ORIGINAL CONDITION OR BETTER, AS APPROVED BY THE ENGINEER OF RECORD. DURING CONSTRUCTION CONTRACTOR IS TO HAVE THE SITE MAINTAINED FREE OF ALL TRASH, LITTER, DEBRIS, AND OVERGROWN VEGETATION.
11. THE OWNER SHALL BE RESPONSIBLE TO HAVE THE SITE MAINTAINED FREE OF ALL TRASH, LITTER, DEBRIS, AND OVERGROWN VEGETATION.
12. ACCORDING TO FLOOD MAP NUMBER 2502SC0017K, PANEL 17 OF 176, DATED 07/03/2024, PRODUCED BY FEMA, A PORTION OF THE SITE LIES WITHIN A ZONE "X" SHADED, AREAS OF 0.2% ANNUAL CHANCE OF FLOOD (500-YR FLOOD PLAIN).

Rev. #:      Date      Description

Graphic Scale:



**SOLLI ENGINEERING**

MONROE, CT | W. HARTFORD, CT | NORWOOD, MA  
SOLLIENGINEERING.COM  
T: (203) 880-5455 | F: (203) 880-9695

Drawn By: STM

Checked By: CJB

Approved By: KMS

Project #: 25200801

Plan Date: 01/25/25

Scale: 1" = 20'

Project:

**PROPOSED ARLINGTON BREWING COMPANY**

15 RYDER STREET  
ARLINGTON, MASSACHUSETTS  
OWNER: ABCJ LAND LLC  
MAP 57 BLOCK 2 LOT 11

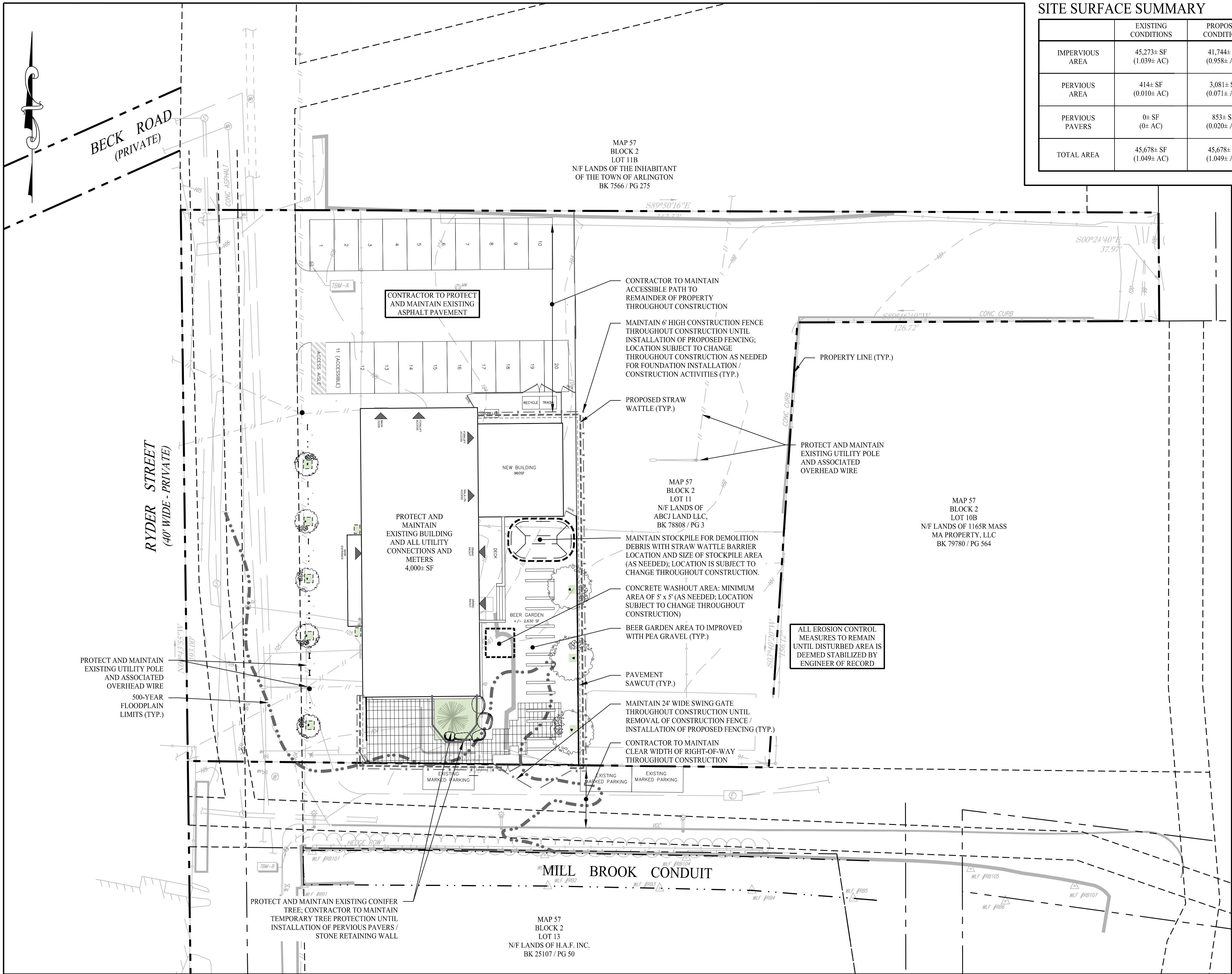
Sheet Title:

**PHASE I  
EROSION &  
SEDIMENT  
CONTROL PLAN**

Sheet #:

**2.01**





#### SITE SURFACE SUMMARY

	EXISTING CONDITIONS	PROPOSED CONDITIONS
IMPERVIOUS AREA	45,273± SF (1.039± AC)	41,744± SF (0.958± AC)
PERVIOUS AREA	414± SF (0.010± AC)	3,081± SF (0.071± AC)
PERVIOUS PAVERS	0± SF (0± AC)	853± SF (0.020± AC)
TOTAL AREA	45,678± SF (1.049± AC)	45,678± SF (1.049± AC)

#### CONSTRUCTION SEQUENCE (PHASE II)

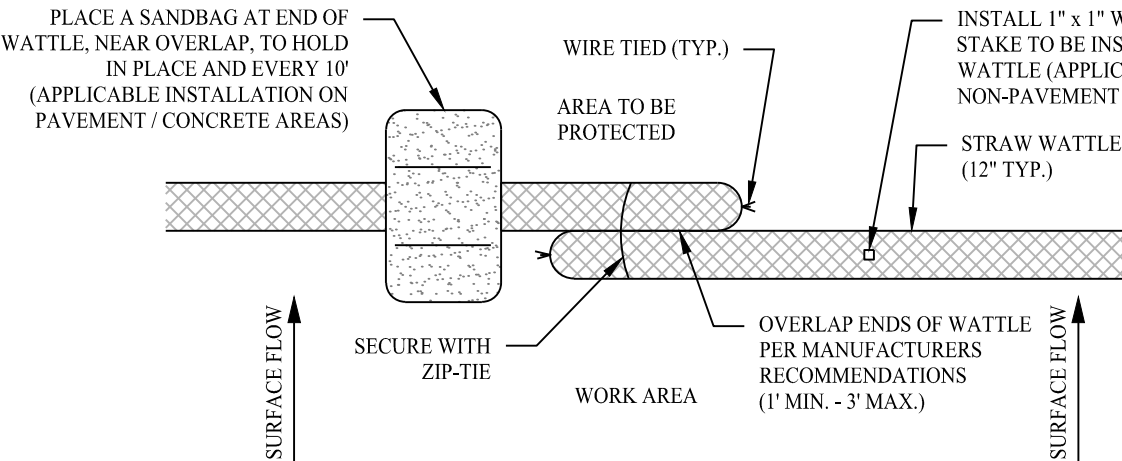
- PHASE II:**
  - INSTALL PROPOSED IMPROVEMENTS (SEE PROPOSED PLOT PLAN OF LAND FOR PROPOSED IMPROVEMENTS PREPARED BY CONTROL POINT ASSOCIATES, INC.).
  - INSTALL UTILITIES AS SHOWN ON PROPOSED PLOT PLAN OF LAND FOR PROPOSED IMPROVEMENTS PREPARED BY CONTROL POINT ASSOCIATES, INC.
  - START CONSTRUCTION OF BUILDING ADDITION, BEER GARDEN AREA, AND PAVER AREA.
  - INSTALL CURBS (AS NEEDED).
  - PERMANENTLY STABILIZE AREAS TO BE VEGETATED AS THEY ARE BROUGHT TO FINAL GRADE.
  - PREPARE SITE FOR FINAL GRADING.
  - CONSTRUCT CONCRETE PADS AND SIDEWALKS.
  - PAVE AREAS SHOWN ON PROPOSED PLOT PLAN OF LAND FOR PROPOSED IMPROVEMENTS PREPARED BY CONTROL POINT ASSOCIATES, INC.
  - CONTRACTOR / CONSTRUCTION MANAGER TO COORDINATE WITH ENGINEER OF RECORD TO OBTAIN STABILIZED SITE STATUS.
  - CONTINUE DAILY INSPECTION REPORTS UNTIL THE FINAL DAILY INSPECTION REPORT IS SIGNED BY THE CONSTRUCTION MANAGER AND SUBMITTED.

#### LEGEND

	PROPERTY LINE
	RIGHT-OF-WAY LINE
	ADJOINING LOT LINE
	LIMIT OF EASEMENT
	PAVEMENT SAWCUT
	STRAW WATTLE BARRIER
	CONCRETE WASHOUT AREA
	CONSTRUCTION FENCE
	TREE PROTECTION / ORANGE CONSTRUCTION FENCE

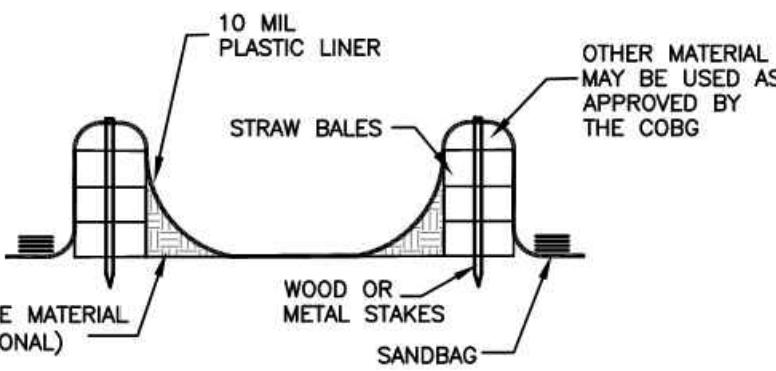
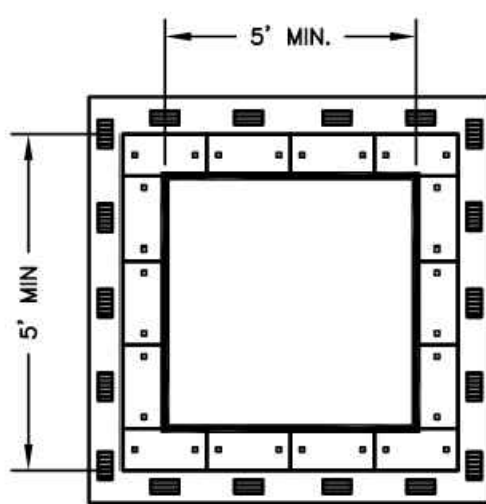
#### GENERAL NOTES

- THESE PLANS ARE FOR PERMITTING PURPOSES ONLY AND ARE NOT FOR CONSTRUCTION. NO CONSTRUCTION OR DEMOLITION SHALL BEGIN UNTIL APPROVAL OF THE FINAL PLANS IS GRANTED BY ALL GOVERNING AND REGULATORY AGENCIES.
- ALL SITE WORK TO BE COMPLETED IN ACCORDANCE WITH ALL PERMITS, APPROVALS, AND CONDITIONS OF APPROVALS ISSUED BY THE TOWN OF ARLINGTON, MASSACHUSETTS FOR THIS PROJECT.
- EXISTING SITE CONDITIONS AND BOUNDARY INFORMATION TAKEN FROM A PLAN TITLED "BOUNDARY, TOPOGRAPHIC & UTILITY SURVEY; ABC BEER INC.", DATED JANUARY 21, 2025. SCALE: 1"=20'. PREPARED BY CONTROL POINT ASSOCIATES, INC.
- 3.1. FIELD SURVEY PERFORMED IN DECEMBER 2024.
- 3.2. THE VERTICAL DATUM IS NAVD83.
4. THE PROJECT SITE CONSISTS OF ONE (1) PARCEL WITH A TOTAL LAND AREA OF APPROXIMATELY 1.049± ACRES (45,687± SF) LOCATED WITHIN THE TOWN OF ARLINGTON INDUSTRIAL (I) ZONING DISTRICT.
- 4.1. MAP 57 BLOCK 2 LOT 11 AS SHOWN ON THE TOWN OF ARLINGTON, MIDDLESEX COUNTY, COMMONWEALTH OF MASSACHUSETTS ASSESSORS MAPS.
- 4.2. DEED REFERENCE: BK 78808 / PG 3
5. PRIOR TO DEMOLITION OR CONSTRUCTION, THE CONTRACTOR SHALL CONTACT "DIG SAFE" 72 HOURS BEFORE THE COMMENCEMENT OF WORK AT "811" AND VERIFY ALL UTILITY AND STORM DRAINAGE SYSTEM LOCATIONS. INFORMATION ON EXISTING UTILITIES AND STORM DRAINAGE SYSTEMS HAS BEEN COMPILED FROM AVAILABLE INFORMATION INCLUDING UTILITY PROVIDER AND MUNICIPAL RECORD MAPS AND/OR FIELD SURVEY AND IS NOT GUARANTEED CORRECT OR COMPLETE. UTILITIES AND STORM DRAINAGE SYSTEMS ARE SHOWN TO ALERT THE CONTRACTOR TO THEIR PRESENCE AND THE CONTRACTOR IS SOLELY RESPONSIBLE FOR DETERMINING ACTUAL LOCATIONS AND ELEVATIONS OF ALL UTILITIES AND STORM DRAINAGE SYSTEMS INCLUDING SERVICES.
6. SHOULD ANY UNCHARTED OR INCORRECTLY CHARTED, EXISTING PIPING OR OTHER UTILITY BE UNCOVERED DURING EXCAVATION, CONSULT THE ENGINEER OF RECORD IMMEDIATELY FOR DIRECTIONS BEFORE PROCEEDING FURTHER WITH WORK IN THIS AREA.
7. CONTRACTOR SHALL BE RESPONSIBLE FOR SITE SAFETY AND SECURITY OF THE SITE DURING ALL PHASES OF CONSTRUCTION. THE ARCHITECT AND ENGINEER OF RECORD HAVE NO CONTRACTUAL DUTY TO CONTROL THE SAFEST METHODS OR MEANS OF THE WORK, JOB SITE RESPONSIBILITIES, SUPERVISION OR TO SUPERVISE SAFETY AND DOES NOT VOLUNTARILY ASSUME ANY SUCH DUTY OR RESPONSIBILITY.
8. THE OWNER IS RESPONSIBLE FOR OBTAINING ALL NECESSARY ZONING PERMITS REQUIRED BY GOVERNMENT AGENCIES PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL OBTAIN ALL LOCAL AND STATE PERMITS. THE CONTRACTOR SHALL POST ALL BONDS, PAY ALL FEES, PROVIDE PROOF OF INSURANCE, AND PROVIDE TRAFFIC CONTROLS NECESSARY FOR THIS PROJECT.
9. CONTRACTOR SHALL REFERENCE ARCHITECTURAL PLANS / PROPOSED PLOT PLAN FOR EXACT DIMENSIONS AND CONSTRUCTION DETAILS OF BUILDING, SIDEWALKS, PATIO AREAS AND ALL OTHER IMPROVEMENTS.
10. THE CONTRACTOR SHALL RESTORE ANY DRAINAGE STRUCTURE, PIPE, UTILITY, PAVEMENT, CURBS, SIDEWALKS, LANDSCAPED AREAS, OR SIGNAGE DISTURBED DURING CONSTRUCTION TO THEIR ORIGINAL CONDITION OR BETTER, AS APPROVED BY THE ENGINEER OF RECORD. DURING CONSTRUCTION CONTRACTOR IS TO HAVE THE SITE MAINTAINED FREE OF ALL TRASH, LITTER, DEBRIS, AND OVERGROWN VEGETATION.
11. THE OWNER SHALL BE RESPONSIBLE TO HAVE THE SITE MAINTAINED FREE OF ALL TRASH, LITTER, DEBRIS, AND OVERGROWN VEGETATION.
12. ACCORDING TO FLOOD MAP NUMBER 2502SC0017K, PANEL 17 OF 176, DATED 07/03/2024, PRODUCED BY FEMA, A PORTION OF THE SITE LIES WITHIN A ZONE "X" SHADED, AREAS OF 0.2% ANNUAL CHANCE OF FLOOD (500-YR FLOOD PLAIN).



#### STAKED STRAW WATTLE

SCALE: NTS

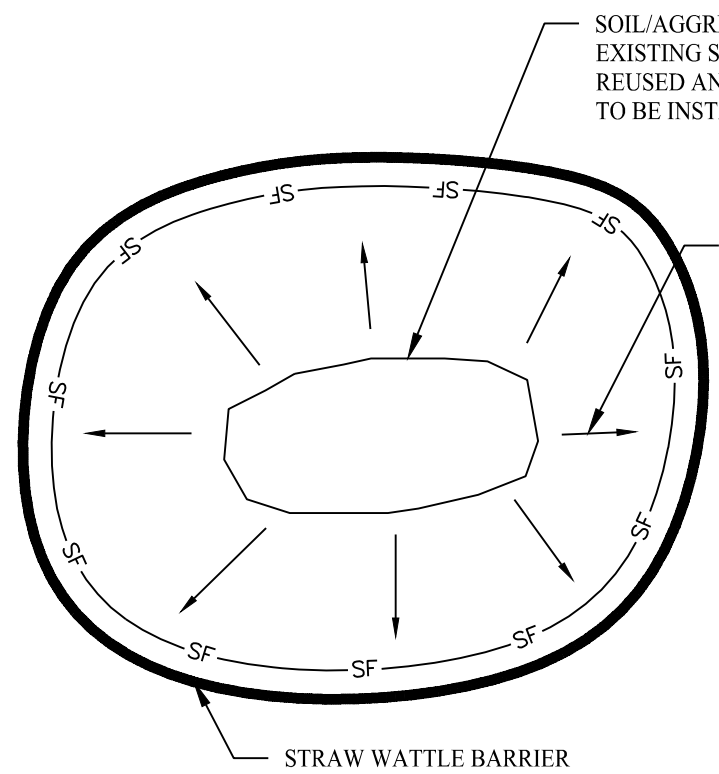


- NOTES:
- NO WASHING OUT OF CONCRETE TRUCKS OR WASHING OF SWEEPING FROM EXPOSED AGGREGATE CONCRETE INTO STORM DRAINS, OPEN DITCHES, STREETS OR STREAMS IS PERMITTED.
  - EXCESS CONCRETE IS NOT PERMITTED TO BE DUMPED ON-SITE, EXCEPT IN DESIGNATED TEMPORARY WASHOUT AREAS (REFER TO SHEET 2.32).
  - ONCE WASHOUT AREA IS 75% FULL, CONTRACTOR TO CLEAN OUT OR REPLACE AREA.
  - PLASTIC LINING SHALL BE MINIMUM 10 MIL. POLYETHYLENE SHEETING AND WILL BE FREE OF HOLES, TEARS, OR OTHER DEFECTS.
  - HARDENED CONCRETE WILL BE REMOVED AND DISPOSED OF OFF-SITE. MATERIALS USED TO CONSTRUCT TEMPORARY CONCRETE WASHOUT FACILITIES WILL BE REMOVED FROM THE SITE AND DISPOSED OF.

#### CONCRETE WASHOUT DETAIL

SCALE: NTS

REFER TO PROPOSED PLOT PLAN OF  
LAND FOR PROPOSED  
IMPROVEMENTS PREPARED BY  
CONTROL POINT ASSOCIATES, INC.



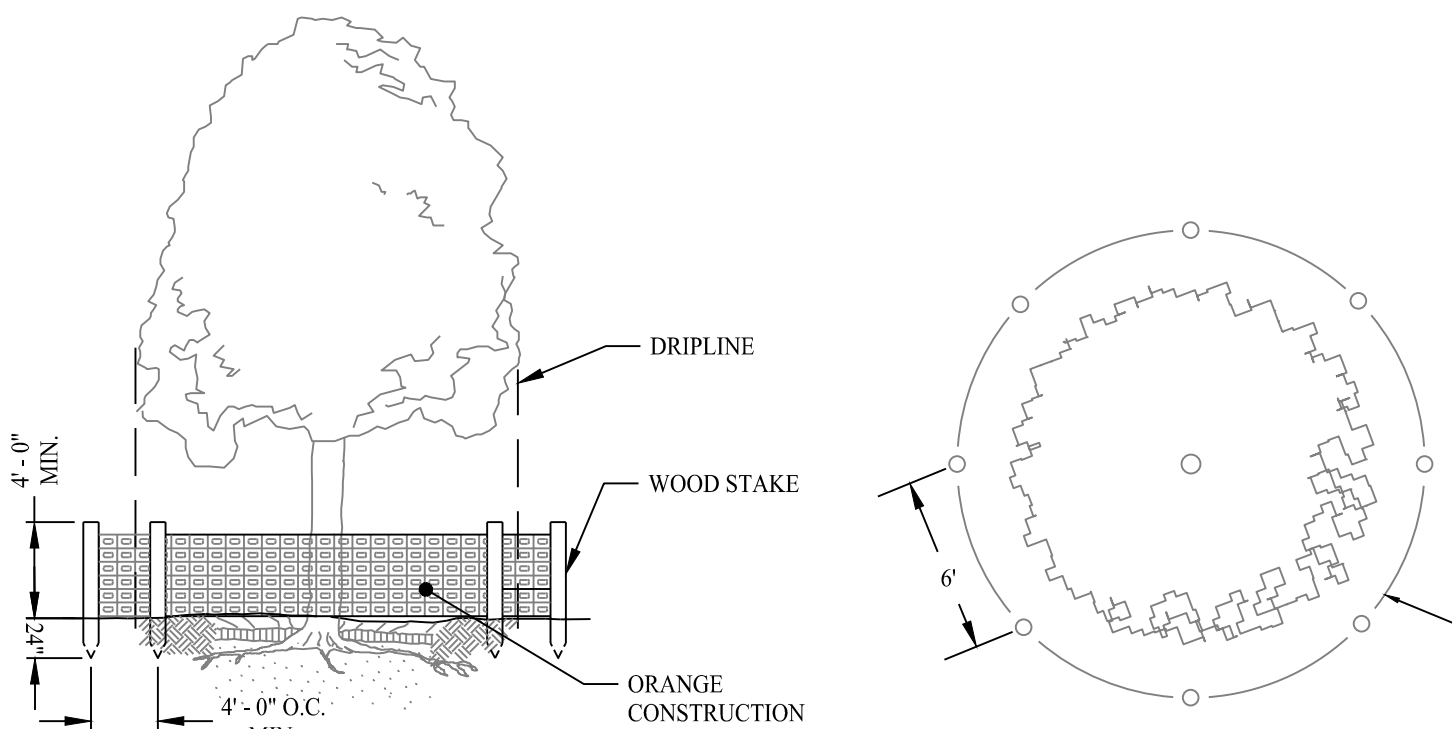
#### MATERIALS STOCKPILE DETAIL BARRIER

SCALE: NTS

- NOTES:
- ALL EXISTING EXCAVATED MATERIAL THAT IS NOT TO BE REUSED IN THE WORK IS TO BE IMMEDIATELY REMOVED FROM THE SITE AND PROPERLY DISPOSED OF.
  - SOIL/AGGREGATE STOCKPILE SITES TO BE WHERE SHOWN ON THE DRAWINGS OR WHERE APPROVED BY CIVIL ENGINEER.
  - RESTORE STOCKPILE SITES TO PRE-EXISTING PROJECT CONDITION AND RESEED AS REQUIRED.
  - STOCKPILE HEIGHTS MUST NOT EXCEED 35'. STOCKPILE SLOPES MUST BE 2:1 OR FLATTER.

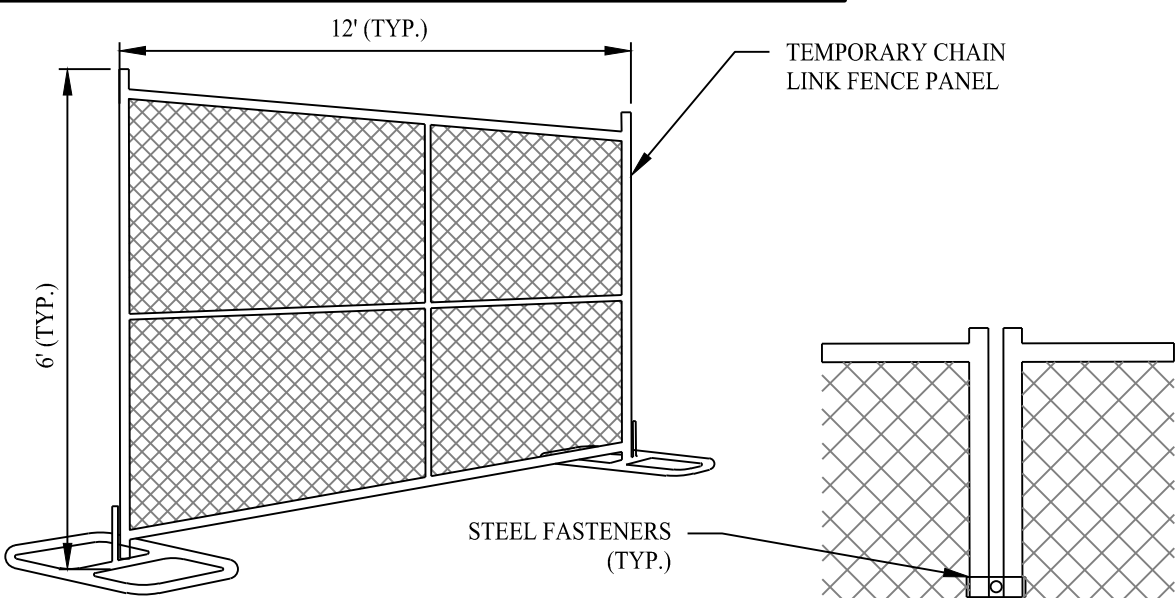
#### PHASE II - SOIL EROSION & SEDIMENT CONTROL PLAN

SCALE: 1" = 20'



#### TYPICAL TREE PROTECTION DETAIL

SCALE: NTS



- NOTES:
- FENCING PANELS COMPLY WITH ASTM - A392-06 STANDARDS.
  - GALVANIZED STEEL, CORROSION-RESISTANT ZINC COATING.
  - 36" BASE STANDS FOR STABILITY, NO DIGGING OR POST SETTING REQUIRED. SAND BAGS TO BE USED FOR ADDED STABILITY.
  - TYPICAL FENCE WIDTH: 12". TYPICAL FENCE HEIGHT: 6'.
  - SCREENING MADE OF RESILIENT HDPE POLYETHYLENE, WITH 88% BLOCKAGE.
  - REINFORCED SCREENING WITH HEMMED EDGES AND STEEL GROMMETS.
  - SCREEN HEIGHT: 5'-8" FOR 6' FENCE.
  - SCREEN COLORS: GREEN, TAN, BLUE, OR RED.

#### TEMPORARY CONSTRUCTION FENCE DETAIL

SCALE: NTS

Rev. #:      Date      Description

Graphic Scale:



**SOLLI**  
ENGINEERING

MONROE, CT | W. HARTFORD, CT | NORWOOD, MA  
SOLLIENGINEERING.COM  
T: (203) 880-5455 | F: (203) 880-9695

Drawn By: STM

Checked By: CJB

Approved By: KMS

Project #: 25200801

Plan Date: 01/25/25

Scale: 1" = 20'



Kevin Solli, P.E.  
MA 51952

#### PROPOSED ARLINGTON BREWING COMPANY

15 RYDER STREET  
ARLINGTON, MASSACHUSETTS  
OWNER: ABCJ LAND LLC  
MAP 57 BLOCK 2 LOT 11

Sheet Title:  
**PHASE II  
EROSION &  
SEDIMENT  
CONTROL PLAN**

Sheet #:

**2.02**





Arlington Brewing  
15 Ryder St, Arlington, MA 02476, USA

System Size (DC)     35.88 kW

☐ System			
SIL-520 QM Silfab Solar		Modules	Qty: 69
Sunny Tripower X 30-US SMA Solar Technology AG		Inverters	Qty: 4
TS4-A-O (700W) Tigo Energy Inc.		DC optimizers	Qty: 69



**Your System Details**

Electric consumption:	40,528 kWh
Utility Rate Escalation:	3.50%
System Size:	35.88 kW
System Annual Production:	43,473 kWh
Energy Offset %:	107%

Total System Cost:	\$113,022.00
--------------------	--------------

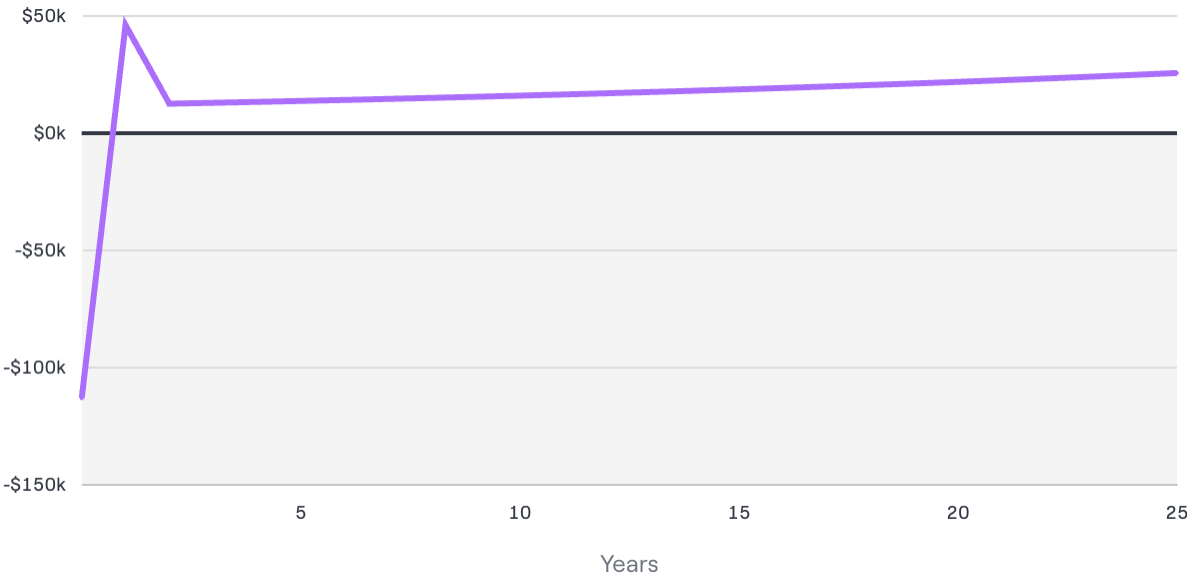
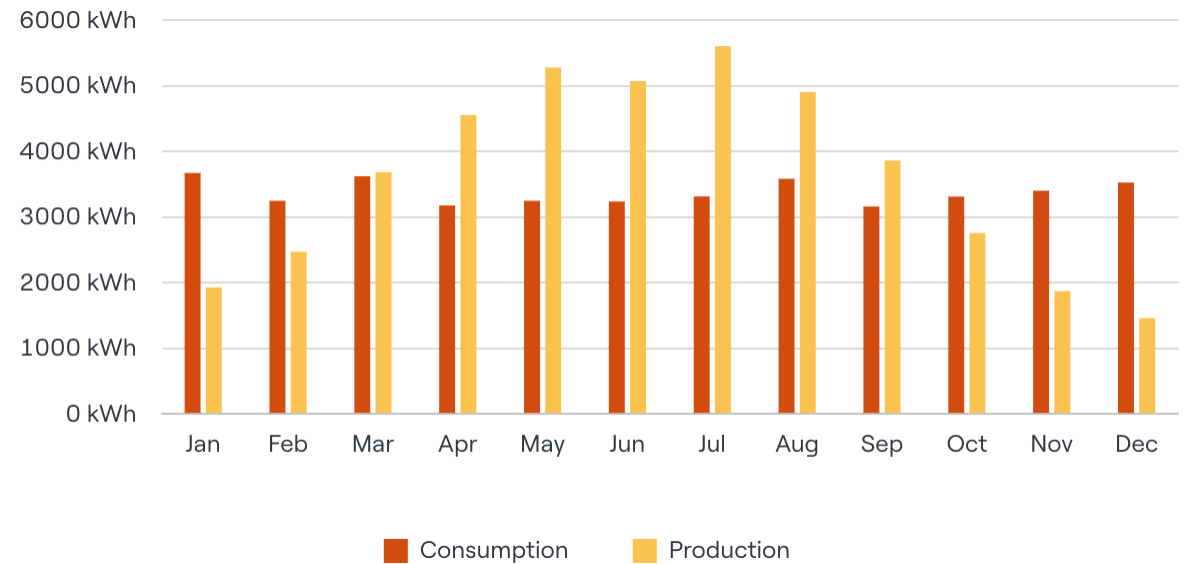
**Your Investment Details**

Upfront Cost	\$113,022.00
Federal Tax Credit	\$32,923.80
Year 1 Electric savings	\$10,489.53

**Breakeven:** **6 years**

Lifetime Incentives:	\$77,379.15
Lifetime Electric savings:	\$372,922.77

*This does NOT include MACRS Depreciation!*



**Your Annual Breakdown**

Year	Annual Bill Pre Solar	Annual Bill Post Solar	Annual Cashflows	Cumulative Cashflows
0	\$11,365.73	\$876.20	-\$113,022.00	-\$113,022.00
1	\$11,757.23	\$900.57	\$46,135.03	-\$66,886.97
2	\$12,162.43	\$925.79	\$12,595.56	-\$54,291.41
3	\$12,581.82	\$951.89	\$12,975.55	-\$41,315.86
4	\$13,015.88	\$978.91	\$13,368.83	-\$27,947.03
5	\$13,465.14	\$1,006.87	\$13,775.88	-\$14,171.15
6	\$13,930.12	\$1,035.81	\$14,197.17	\$26.02
7	\$14,411.37	\$1,065.76	\$14,633.21	\$14,659.23
8	\$14,909.47	\$1,096.76	\$15,084.51	\$29,743.74
9	\$15,425.00	\$1,128.85	\$15,551.61	\$45,295.35
10	\$15,958.57	\$1,162.06	\$16,035.05	\$61,330.40
11	\$16,510.82	\$1,196.43	\$16,535.42	\$77,865.82
12	\$17,082.40	\$1,232.01	\$17,053.30	\$94,919.12
13	\$17,673.99	\$1,268.83	\$17,589.30	\$112,508.42
14	\$18,286.28	\$1,306.94	\$18,144.06	\$130,652.48
15	\$18,920.00	\$1,346.38	\$18,718.24	\$149,370.72
16	\$19,575.90	\$1,387.20	\$19,312.52	\$168,683.24
17	\$20,254.75	\$1,429.45	\$19,927.60	\$188,610.84
18	\$20,957.37	\$1,473.18	\$20,564.20	\$209,175.04
19	\$21,684.58	\$1,518.45	\$21,223.09	\$230,398.13
20	\$22,437.24	\$1,565.29	\$21,905.03	\$252,303.16
21	\$23,216.24	\$1,613.78	\$22,610.85	\$274,914.01
22	\$24,022.51	\$1,663.96	\$23,341.37	\$298,255.38
23	\$24,857.00	\$1,715.90	\$24,097.45	\$322,352.83
24	\$25,720.69	56 of 232 \$1,769.65	\$24,880.00	\$347,232.83

## **Your Incentives**

### **Investment Tax Credit (ITC) - 30%**

The Inflation Reduction Act (IRA) of 2022 establishes and extends the federal Investment Tax Credit (ITC) for solar photovoltaic (PV) systems at a rate of 30% of the total PV system cost. The 30% ITC was extended for 10 years, through 2032. Unlike tax deductions, this tax credit can be used to directly offset your tax liability dollar for dollar. The IRA extended the carryback period to 3 years, and the carryforward period to 22 years, in cases where the tax credit exceeds a customer's tax liability in the 'placed-in-service' year. For PV projects greater than 1 MW AC in size, the IRA established prevailing wage and apprenticeship requirements in order to qualify for the full 30% "increased rate", rather than a "base rate" which would only qualify for a 6% ITC. Projects with an output of less than 1 megawatt qualify for the "increased rate" irrespective of if prevailing wage or apprenticeship requirements are met.

### **REC Incentive**

Renewable Energy Credits (RECs) are tradable certificates that represent the environmental benefits of generating one megawatt-hour (MWh) of electricity from a renewable energy source, such as solar power. In the context of residential solar systems, RECs serve as a way for homeowners to gain recognition for the clean energy their solar panels produce. RECs are a valuable aspect of residential solar energy systems, providing a means for homeowners to benefit financially while contributing to the broader goal of increasing renewable energy use.

### **MACRS Depreciation**

MACRS (Modified Accelerated Cost Recovery System) depreciation is a tax incentive mechanism used in the United States that allows businesses to recover the cost of certain assets, including renewable energy projects like solar installations, through accelerated depreciation over a defined period.

*As with all incentives, be sure to consult with your tax advisor!*



## Our Goals

At BD Electrical and Solar, we are committed to empowering communities in Eastern Massachusetts by providing locally-owned, sustainable solar energy solutions. Our mission is to ensure that ownership and benefits of clean energy remain within the towns we serve, fostering economic growth, environmental stewardship, and energy independence in the places we call home.

The mission of BD Electrical and Solar is to apply our expertise and deliver a full array of electrical services to our commercial, residential and solar customers. We are committed to using only high quality, state-of-the-art electrical and solar components, and completing projects on time, on budget with dependable results. At BD Electrical Services we embrace the new technologies that bring energy efficiency to homes and businesses, and we are confident in our capacity to implement complex electrical system designs into mainstream energy platforms.





## Solar Exemption

The project to bring a brewery and taproom to 15 Ryder St is exempt from section 6.4 of the zoning bylaws for the following reasons:

1. Per section 3.4.2, this EDR application is submitted for a change in use alone. The primary Restaurant use, as well as the accessory brewery use trigger EDR because they are combined into one space and are classified as mixed-use.
2. The alterations to the facade of the structure do not affect the architectural integrity of the building. While we are adding features to improve the energy efficiency of the building, the design maintains the industrial character of the building and retains key features such as windows, doors, masonry-style finish and overall visual impression.



**Town of Arlington, Massachusetts**  
Department of Planning and Community Development  
730 Massachusetts Avenue, Arlington, Massachusetts 02476

---

## Public Hearing Memorandum

*The purpose of this memorandum is to provide the Arlington Redevelopment Board and public with technical information and a planning analysis to assist with the regulatory decision-making process.*

**To:** Arlington Redevelopment Board  
**From:** Claire V. Ricker, AICP Secretary Ex-Officio  
**Subject:** Environmental Design Review, 15 Ryder St, Arlington, MA, Docket #3838  
**Date:** February 20, 2025

---

### I. Docket Summary

This is an application by Thomas Allen, ABC Beer Inc, dba Arlington Brewing Company, 251 Lowell St, Arlington, MA 02474, to open Special Permit Docket #3838 in accordance with the provisions of MGL Chapter 40A § 11, and the Town of Arlington Zoning Bylaw Sections 3.3, Special Permits, and 3.4, Environmental Design Review.

The Applicant proposes to construct an approximately 1,000-square foot addition and to renovate the interior of a two-story, vacant commercial building with off-street parking located at 15 Ryder Street, Arlington, MA, in the I Industrial District. The proposal would establish a single-story, mixed-use building with a brewery, including a taproom, and an approximately 3,900-square foot restaurant. The opening of the Docket is to allow the Board to review and approve the application under Section 3.3, Special Permits and Section 3.4, Environmental Design Review.

Materials submitted for consideration of this application include:

- EDR application for Special Permit
- Photos of existing conditions
- Architectural plans and drawings (Existing and Proposed)
- Site plan
- Solar evaluation
- LEED checklist
- Erosion & Sediment Control Plan

## II. Application of Special Permit Criteria (Arlington Zoning Bylaw, Section 3.3)

### 1. Section 3.3.3.A.

**The use requested is listed as a Special Permit in the use regulations for the applicable district or is so designated elsewhere in this Bylaw.**

As per Section 5.6.3, Use Regulations for MU, PUD, I, T and OS Districts, a mixed-use brewery and a restaurant greater than 2,000 square feet in size is allowed in the Industrial District by Special Permit. The Board can find this condition met.

### 2. Section 3.3.3.B.

**The requested use is essential or desirable to the public convenience or welfare.**

The Master Plan recommends incorporating new uses, such as restaurants, in mixed-use redevelopment in the Industrial district to help attract new businesses and jobs in growth industries and serve local employees, as well as residents throughout the region. Additionally, the proposed redevelopment is in close proximity to local attractions, the Ed Burns Arena and the Minuteman Bikeway. The Board can find this condition met.

### 3. Section 3.3.3.C.

**The requested use will not create undue traffic congestion or unduly impair pedestrian safety.**

The Applicant has proposed to remove the parking spaces directly in front of the building and convert that area to pedestrian walkways with short-term bike parking and landscaping. Existing, non-conforming parking partially located in the front yard setback will continue to be provided on site. The proposed brewery and restaurant uses will not create undue traffic congestion or impair pedestrian safety. The Board can find this condition met.

### 4. Section 3.3.3.D.

**The requested use will not overload any public water, drainage or sewer system or any other municipal system to such an extent that the requested use or any developed use in the immediate area or in any other area of the Town will be unduly subjected to hazards affecting health, safety, or the general welfare.**

Most recently, the building was used as a contractor office, shop and warehouse that generated significant large-vehicle traffic. Water usage of the brewery operation is comparable to that of other businesses of similar size and includes reuse where feasible to minimize consumption. 2,700 square feet of impervious surface will be converted into a permeable, landscaped outdoor beer garden located in the rear yard. The Project will not overload any public water, drainage, sewer system or other municipal system. The Board can find this condition met.

### 5. Section 3.3.3.E.

**Any special regulations for the use as may be provided in the Bylaw are fulfilled.**

There are no special regulations provided in the Bylaw for the proposed use. The Board can find this condition met.

**6. Section 3.3.3.F.**

**The requested use will not impair the integrity or character of the district or adjoining districts, nor be detrimental to the health, morals, or welfare.**

The use does not impair the integrity or character of the I district or adjacent residential districts and will not be detrimental to health or welfare. The surrounding uses are made up of residential, commercial, and industrial properties. The Board can find this condition met.

**7. Section 3.3.3.G.**

**The requested use will not, by its addition to a neighborhood, cause an excess of the use that could be detrimental to the character of said neighborhood.**

There will be no excess of mixed-use in the neighborhood as a result of this development; rather the Applicant's proposal will comport with the objectives of the Master Plan to re-use underutilized industrial property and encourage mixed-use redevelopment with new commercial uses and employment opportunities in the I district. The Board can find this condition met.

**III. Environmental Design Review Standards (Arlington Zoning Bylaw, Section 3.4)**

**1. EDR-1 Preservation of Landscape**

**The landscape shall be preserved in its natural state, insofar as practicable, by minimizing tree and soil removal, and any grade changes shall be in keeping with the general appearance of neighboring developed areas.**

The proposed renovation is an interior fit-out with the construction of a cold storage room in the rear of the building. Site improvements will include new landscaping in the front, rear and right side yards to create pedestrian walkways and a beer garden. An existing conifer tree in the right side yard shall be preserved. The Board can find this condition is met.

**2. EDR-2 Relation of the Building to the Environment**

**Proposed development shall be related harmoniously to the terrain and to the use, scale, and architecture of the existing buildings in the vicinity that have functional or visible relationship to the proposed buildings. The Arlington Redevelopment Board may require a modification in massing so as to reduce the effect of shadows on the abutting property in an R0, R1 or R2 district or on public open space.**

The proposed renovation is an interior fit-out of an existing building. Changes to the building's exterior will include a 960-square foot rear addition, updating façade materials, replacing doors and windows, and constructing a covered main entrance in front of the building. The Board can find this condition is met.

**3. EDR-3 Open Space**

**All open space (landscaped and usable) shall be so designed as to add to the visual amenities of the vicinity by maximizing its visibility for persons passing by the site or overlooking it from nearby properties. The location and configuration of usable open space shall be so designed as to encourage social interaction, maximize its utility and facilitate maintenance.**

There is minimal landscaping currently on the site. While no landscaped or usable open space is required in the I district, the proposed site improvements will increase overall open space on the parcel through the conversion of 2,700 square feet of surface parking behind the building to an outdoor drinking and dining space with a new deck, landscaping (including 3 shade trees), and permeable pavers. Additionally, parking in front of the building will be converted to a landscaped area (including 5 shade trees) with a new pedestrian walkway along Ryder Street. The Board can find this condition is met.

#### 4. EDR-4 Circulation

**With respect to vehicular and pedestrian and bicycle circulation, including entrances, ramps, walkways, drives, and parking, special attention shall be given to location and number of access points to the public streets (especially in relation to existing traffic controls and mass transit facilities), width of interior drives and access points, general interior circulation, separation of pedestrian and vehicular traffic, access to community facilities, and arrangement of vehicle parking and bicycle parking areas, including bicycle parking spaces required by Section 6.1.12 that are safe and convenient and, insofar as practicable, do not detract from the use and enjoyment of proposed buildings and structures and the neighboring properties.**

Restriping and other minor improvements are proposed for the parking area. The lot is accessed via Ryder Street with both right and left turn accessibility when exiting the site. The portion of the site leased for the brewery and restaurant will be fenced off from the rest of the parcel, and eleven (11) vehicle parking spaces in front of the building will be removed, as will the existing chain-link fence along Ryder Street. The table below shows the parking calculations for the Project using the exemption for mixed-use development provided in Section 6.1.10(C) of the Zoning Bylaw.

Vehicle Parking Requirements for Restaurant & Brewery		
Required parking spaces	17*	
Proposed parking spaces	20	
Loading Requirements		
Required loading spaces	0*	
Proposed loading spaces	0	
<i>*Includes exemption for 3,000 square of non-residential space per Section 6.1.10(C)</i>		
Bicycle Parking Requirements		
<u>Use</u>	<u>Long-Term Parking</u>	<u>Short-Term Parking</u>
Brewery	3	2
Restaurant	1	4
<b>Total Required Bicycle Parking</b>	4	6
<b>Total Proposed Bicycle Parking</b>	4	22

The Applicant has requested relief to continue the existing, non-conforming loading/delivery access in the parking lot located on the side of the building. The Board also may want to consider granting relief for the two existing parking spaces in the required front yard area. Loading/delivery hours will be primarily restricted to the hours of 9:00 – 11:00 a.m., Monday through Friday during non-peak tap room hours. The existing building contains a loading bay with roll-up door for forklift access for deliveries. Trash and recycling is contained in a fenced enclosure located at the rear of the lot.

New pedestrian walkways as proposed are adequate, although it is unclear from the submitted materials whether any curbing will also be constructed along the front or side lot lines adjacent to Ryder Street and the access drive for the Artemis Apartments. Short-term bicycle parking will be located in the front yard (22 spaces provided; 6 required). An outdoor bike shed with four (4) long-term spaces is proposed within a fenced area in the side yard.

Additionally, DPCD staff note:

- Staff was unable to confirm the proposed bike shed for long-term parking will provide an enclosed, limited access area as is typical for such amenities.
- Staff was unable to confirm whether any of the parking spaces will be EV ready, as required by the Stretch Code.
- The Project is required to comply with all applicable AAB regulations for accessible parking, as confirmed by the Town's Director of Inspectional Services.

## **5. EDR-5 Surface Water Drainage**

**Special attention shall be given to proper site surface drainage so that removal of surface waters will not adversely affect neighboring properties or the public storm drainage system. Available Best Management Practices for the site should be employed, and include site planning to minimize impervious surface and reduce clearing and re-grading. Best Management Practices may include erosion control and stormwater treatment by means of swales, filters, plantings, roof gardens, native vegetation, and leaching catch basins. Stormwater should be treated at least minimally on the development site; that which cannot be handled on site shall be removed from all roofs, canopies, paved and pooling areas and carried away in an underground drainage system. Surface water in all paved areas shall be collected in intervals so that it will not obstruct the flow of vehicular or pedestrian traffic and will not create puddles in the paved areas.**

**In accordance with Section 3.3.4., the Board may require from any applicant, after consultation with the Director of Public Works, security satisfactory to the Board to ensure the maintenance of all stormwater facilities such as catch basins, leaching catch basins, detention basins, swales, etc. within the site. The Board may use funds provided by such security to conduct maintenance that the applicant fails to do.**

**The Board may adjust in its sole discretion the amount and type of financial security such that it is satisfied that the amount is sufficient to provide for any future maintenance needs.**

The Applicant proposes to increase overall drainage capacity by converting 2,700 square feet of parking to a landscaped beer garden with permeable surfaces, while providing minimal treatment of stormwater generated on-site and reducing the amount of runoff from the property. The current drainage infrastructure will be adequate for the Project, as stated in the application, but it is unclear whether existing stormwater facilities are located on site. The Board can find this condition is met.

## **6. EDR-6 Utility Service**

**Electric, telephone, cable TV, and other such lines of equipment shall be underground. The proposed method of sanitary sewage disposal and solid waste disposal from all buildings shall be indicated.**

The building will utilize existing utility services. No changes are proposed to pole-mounted utilities located on Ryder Street. The building is served by underground sanitary sewage disposal. Organic and



compostable materials leftover from food service and brewing operations will be diverted from the waste stream to minimize the amount of trash sent to landfill or incineration. The Board can find this condition is met.

## **7. EDR-7 Advertising Features**

**The size, location, design, color, texture, lighting and materials of all permanent signs and outdoor advertising structures or features shall not detract from the use and enjoyment of proposed buildings and structures and the surrounding properties.**

Two wall signs and a window sign are proposed for the Project, which may require relief from the Board. The total number of signs exceeds the maximum allowable by right. The total proposed wall sign area (44 square feet) exceeds the allowable square footage for wall signs. The Applicant should submit a representative to-scale drawing of the proposed window signage for the Board to assess. Any signage and advertising will be in accordance with the provisions of Section 6.2 of the Zoning By-Law, compliant with the Industrial District requirements. Final signage will need to be submitted, reviewed, and approved administratively by the Department of Planning and Community Development or reviewed by the Board for a sign permit.

## **8. EDR-8 Special Features**

**Exposed storage areas, exposed machinery installations, service areas, truck loading areas, utility buildings and structures, and similar accessory areas and structures shall be subject to such setbacks, screen plantings or other screening methods as shall reasonably be required to prevent their being incongruous with the existing or contemplated environment and the surrounding properties.**

A structure for long-term bicycle parking and a transformer will be located in the right side yard, which is to be fenced. The Board can find this condition met.

## **9. EDR-9 Safety**

**With respect to personal safety, all open and enclosed spaces shall be designed to facilitate building evacuation and maximize accessibility by fire, police and other emergency personnel and equipment. Insofar as practicable, all exterior spaces and interior public and semi-public spaces shall be so designed to minimize the fear and probability of personal harm or injury by increasing the potential surveillance by neighboring residents and passersby of any accident or attempted criminal act.**

Roof and building-mounted downlight fixtures are proposed for exterior illumination. The property provides access to the building for fire, police and other emergency personnel and equipment from Ryder Street. The Board can find this condition met.

## **10. EDR-10 Heritage**

**With respect to Arlington's heritage, removal or disruption of historic, traditional or significant uses, structures or architectural elements shall be minimized insofar as practical whether these exist on the site or on adjacent properties.**

This property is not listed on the *Inventory of Historically or Architecturally Significant Properties in the Town of Arlington* and is not under the jurisdiction of the Arlington Historical Commission. The Board can find that this condition is met.

#### **11. EDR-11 Microclimate**

**With respect to the localized climatic characteristics of a given area, any development which proposes new structures, new hard surface, ground coverage or the installation of machinery which emits heat, vapor or fumes shall endeavor to minimize insofar as practicable, any adverse impacts on light, air, and water resources or on noise and temperature levels of the immediate environment.**

This project will have no adverse impacts on light, air and water resources or on noise and temperature levels of the immediately environment. The outdoor beer garden will not play live music past 9:00 p.m. The maximum noise level of equipment used in the brewing process is 65 dBA at 10 feet. Kitchen vents are located at the rear of the building and the heat stream for brewery equipment will be vented out of the top of the building. The Board can find this condition is met.

#### **12. EDR-12 Sustainable Building and Site Design**

**Projects are encouraged to incorporate best practices related to sustainable sites, water efficiency, energy and atmosphere, materials and resources, and indoor environmental quality. Applicants must submit a current Green Building Council Leadership in Energy and Environmental Design (LEED) checklist, appropriate to the type of development, annotated with narrative description that indicates how the LEED performance objectives will be incorporated into the project.**

The Applicant has provided the required LEED checklist and an evaluation of rooftop solar capacity. The project will utilize sustainable building practices and include energy-efficient systems. The Board can find this condition is met.

### **IV. Findings**

The following findings are for the Board's consideration:

1. The ARB finds that the project is consistent with Environmental Design Review per §3.4 of the Zoning Bylaw.
2. The ARB finds that the project is consistent with §3.3, Special Permits of the Zoning Bylaw.
3. The ARB finds that the pre-existing, non-conforming use of loading in the side yard is an acceptable alternative to providing loading/delivery access at the rear of the building per §6.1.10.F(2) of the Zoning Bylaw.
4. The ARB finds that the continued pre-existing non-conforming use of parking in the front yard is necessary and convenient to the public interest per §6.1.10.B of the Zoning Bylaw.
5. The ARB finds that the nature of the use being made of the building is such that allowing an additional sign for a total of 3 signs, and a total wall sign area that exceeds 40 square feet in size is in the public interest consistent with §6.2, Signs of the Zoning Bylaw.

## V. Conditions

### A. General

1. The final design, sign, exterior material, landscaping, and lighting plans shall be subject to the approval of the Arlington Redevelopment Board or administratively approved by the Department of Planning and Community Development.
2. Any substantial or material deviation during construction from the approved plans and specifications is subject to the written approval of the Arlington Redevelopment Board.
3. The Board maintains continuing jurisdiction over this permit and may, after a duly advertised public hearing, attach other conditions or modify these conditions as it deems appropriate in order to protect the public interest and welfare.
4. Snow removal from all parts of the site, as well as from any abutting public sidewalks, shall be the responsibility of the owner and shall be accomplished in accordance with Town Bylaws.
5. Trash shall be picked up only on Monday through Friday between the hours of 7:00 am and 6:00 pm. All exterior trash and storage areas on the property, if any, shall be properly screened and maintained in accordance with Article 30 of Town Bylaws.
6. The Applicant shall provide a statement from the Town Engineer that all proposed utility services have adequate capacity to serve the development. The Applicant shall provide evidence that a final plan for drainage and surface water removal has been reviewed and approved by the Town Engineer.
7. Upon installation of landscaping materials and other site improvements, the Applicant shall remain responsible for such materials and improvement and shall replace and repair as necessary to remain in compliance with the approved site plan.
8. All utilities serving or traversing the site (including electric, telephone, cable, and other such lines and equipment) shall be underground.
9. Upon the issuance of the building permit, the Applicant shall file with the Building Inspector and the Department of Community Safety the names and telephone numbers of contact personnel who may be reached 24 hours each day during the construction period.
10. Building signage shall be filed with and reviewed and approved by the Department of Planning and Community Development and Inspectional Services.
11. The Applicant must comply with the conditions set forth herein, with the State Building Code, including the Town of Arlington requirements, and, where applicable, with the Massachusetts Architectural Access Board regulations.
12. The Applicant must obtain the necessary building permits and work with the Town Engineer to ensure compliance with all applicable codes.



# ARLINGTON

BREWING

• CO •

**Arlington Brewing Company is the first-ever brewery in Arlington, Massachusetts.**

We are seeking a special permit to locate a new brewery, taproom and beer garden at 15 Ryder St.

# Great Community Deserves Great Beer

## Our mission is to spread joy

in our community and beyond by serving a memorable experience focused on incredible craft beer, delicious food, and authentic human connections.



# Leadership Team



**Tom Allen**  
Founder, President  
and Head Brewer



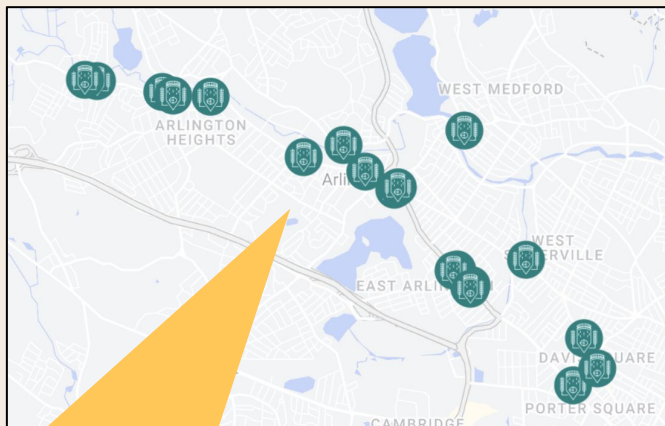
**Peter Caradonna**  
Head of Finance  
and Sales



**Rob Chalmers**  
Executive Chef  
and Director of Hospitality



# It's Not Just About Us



Arlington Beer  
is currently sold by 15 other  
businesses in Arlington.  
And more are on the way



We partner with local  
organizations to  
further their mission  
to help build a thriving  
community.

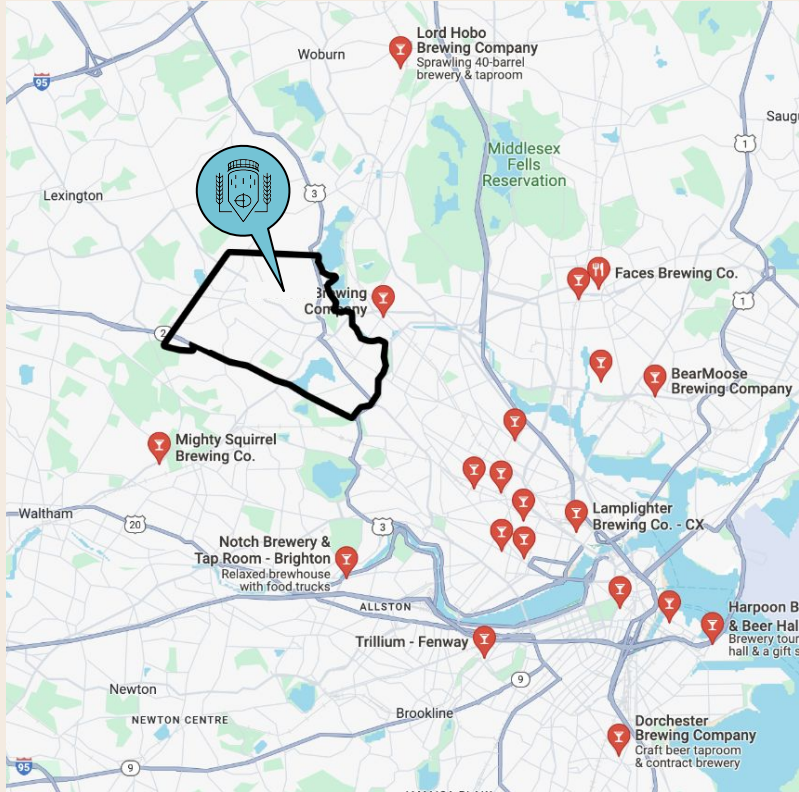




# Proposed Location - 15 Ryder St



# An Ideal Location



Arlington is a craft-brewing desert

Few buildings in town that meet our technical and size requirements

Proximity to popular recreational spots enhances the overall visitor experience and convenience

Location on the bike path allows for easy access without the need for

**cars**  
73 of 232

# 3 Key Components

## Beer Production



## Taproom



## Beer Garden





# Production - Microbrewery

15BBL Brewhouse Capacity

Focus on taproom

Seasonals, special releases, R&D

Canning through contract partners



75 of 232  
15 BBL brewhouse

# Taproom and Kitchen



Welcoming gathering place  
Great food  
Great beer  
Arts and special events





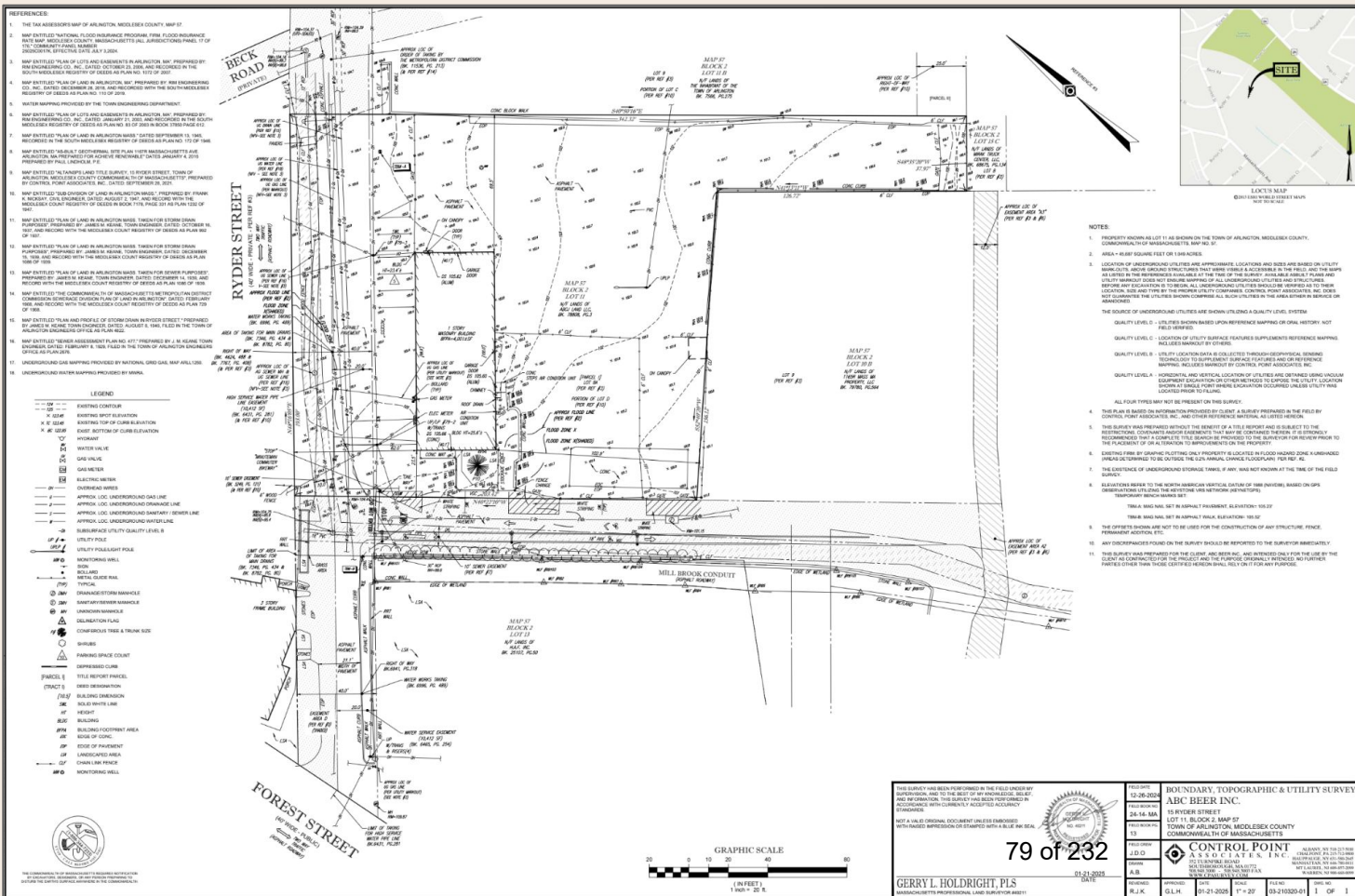
# Beer Garden

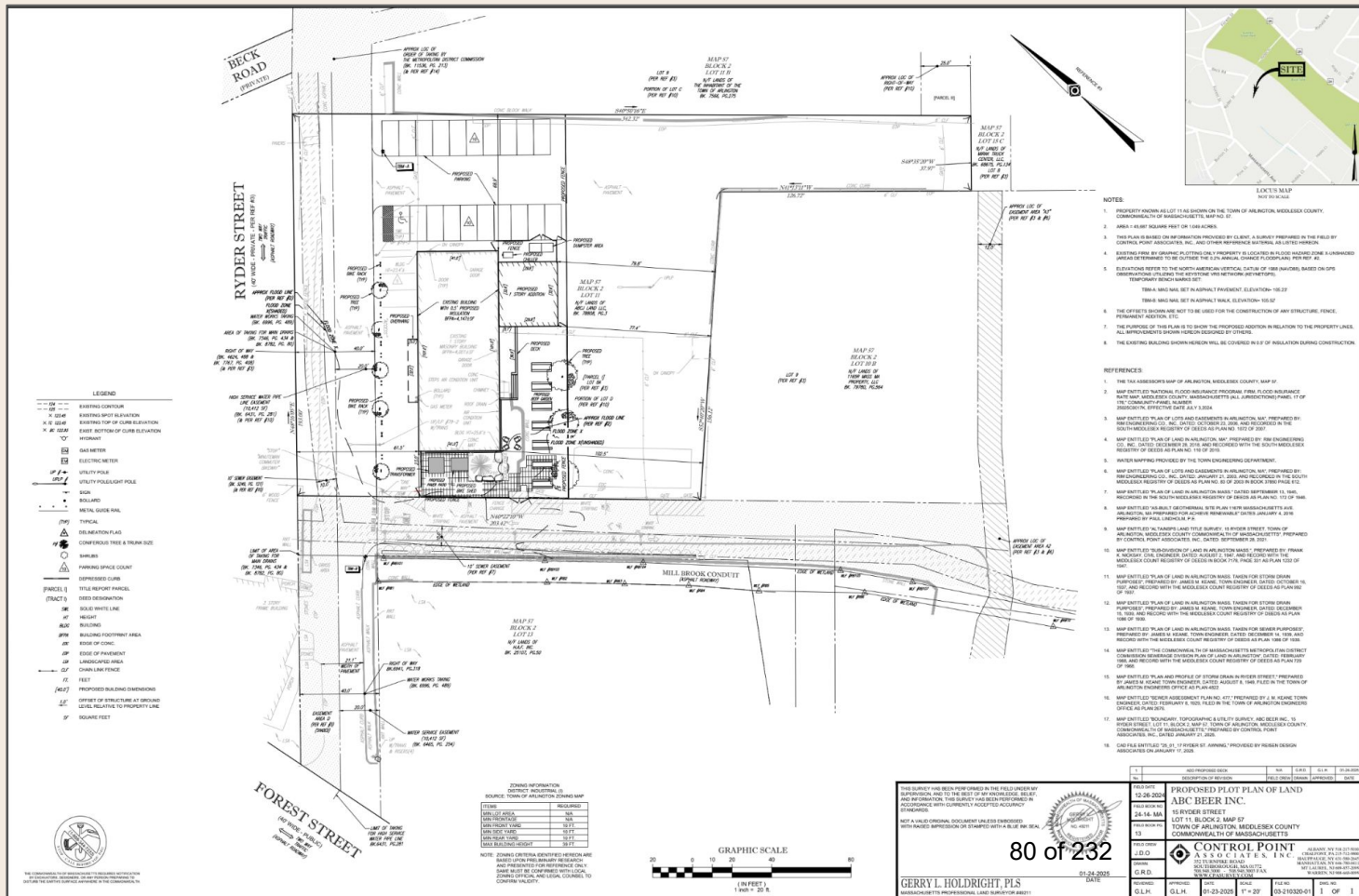
Enjoy the fresh air  
Outdoor space  
Plants & landscaping  
Bike access / parking





# Site - Existing Conditions





# View from corner of Ryder St and Artemis Driveway





# View from Ryder St looking north



# View from Ed Burns Arena parking lot





# View from Ryder St looking south



# View from Artemis driveway



# Rear of building looking north





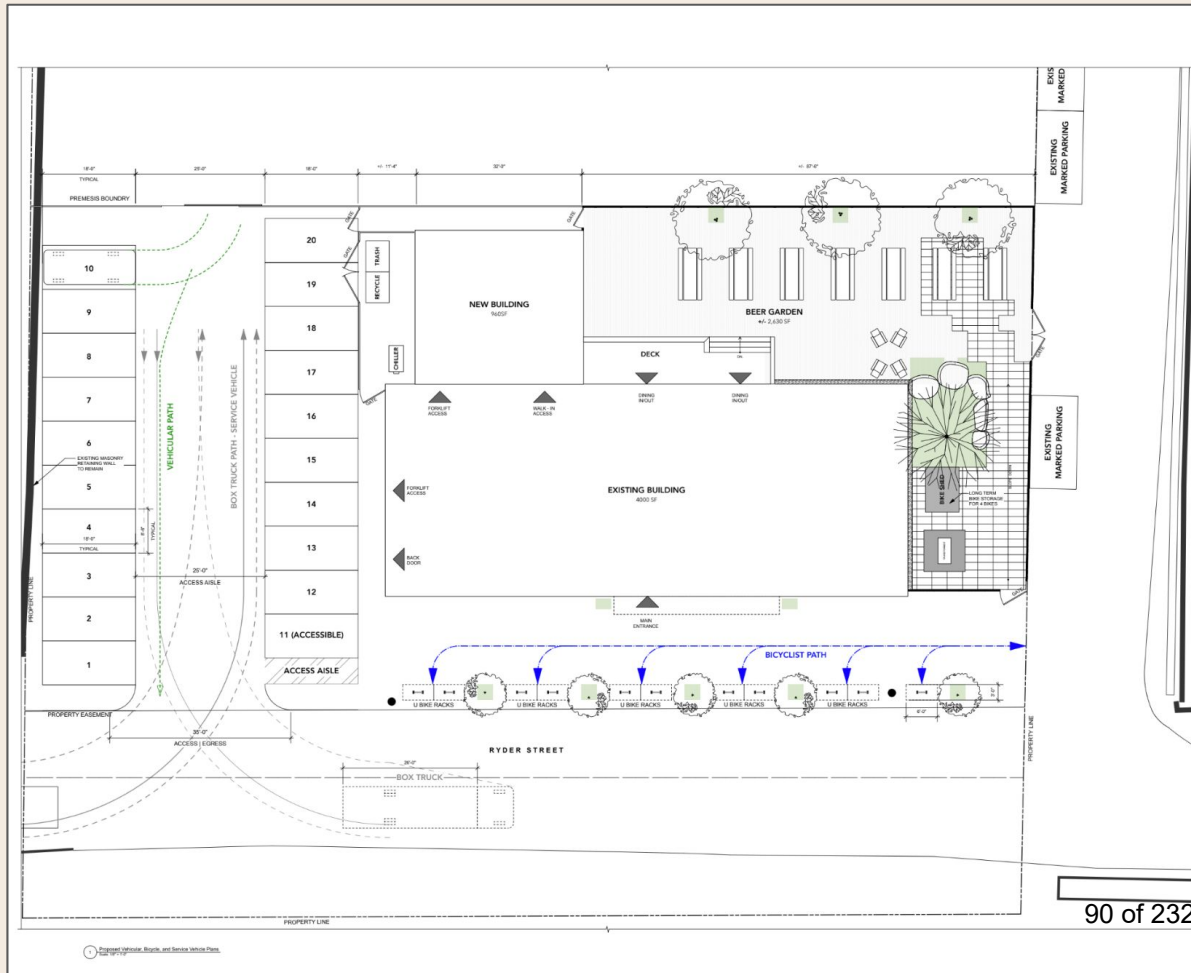
# Side of building looking north



# Proposed Site Plan







90 of 232

**NOTES**

1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20
---	---	---	---	---	---	---	---	---	----	----	----	----	----	----	----	----	----	----	----

**Reason Design Associates**  
ARCHITECTS  
320 Essex Street, Suite 6  
Cambridge, MA 02139  
(617) 875-7442

**Arlington Brewing Co.**  
15 Ryder St.  
Arlington, MA

Proposed Vehicular, Bicycle and Service Vehicle Plan

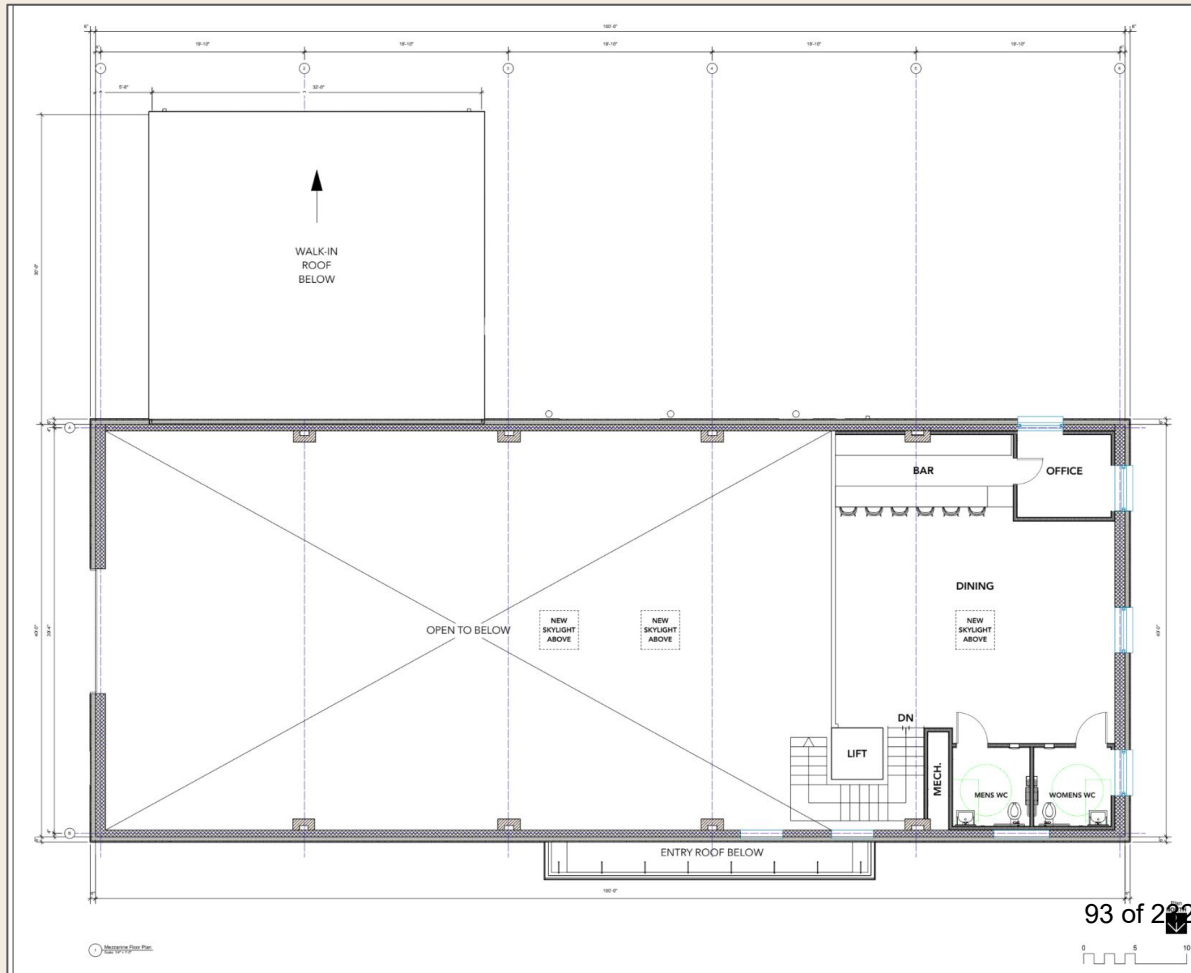
**NOT FOR CONSTRUCTION**

Project No. \_\_\_\_\_ Drawing No. **A020**

Drawn By: \_\_\_\_\_  
Reviewed By: \_\_\_\_\_  
Date: \_\_\_\_\_  
As Noted: \_\_\_\_\_  
Date: 01.27.2025

# Proposed Floor Plan





NOTES

NO.	DATE	DESCRIPTION	BY	CHKD.

Reisen Design Associates  
ARCHITECTURE  
308 Essex Street, Suite 6  
Cambridge MA 02139  
(617) 876-7840

Project Name  
**Arlington Brewing Co.**  
15 Ryder St.  
Arlington MA

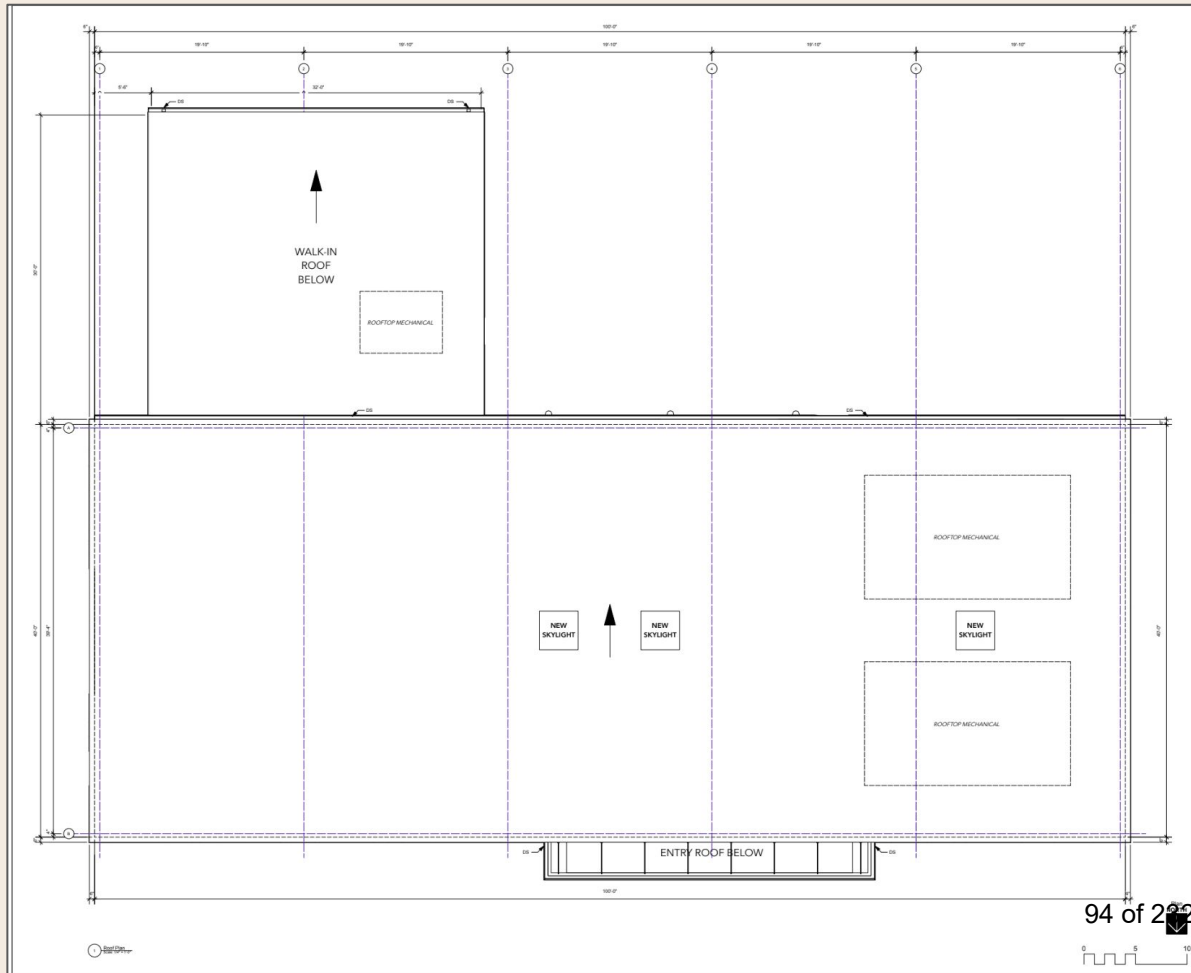
Sheet Title  
Proposed Mezzanine Floor Plan

NOT FOR CONSTRUCTION

Project #1	Drawing No.
Client #1	
Revised #1	
Date	As Noted
Drawn	01.27.2020

**A120**





**NOTES**

NO.	DATE	REVISION	BY	DATE	REVISION
1					

**Reisen Design Associates**  
ARCHITECTS  
328 Essex Street, Suite 6  
Cambridge MA 02139  
(617) 876-7940

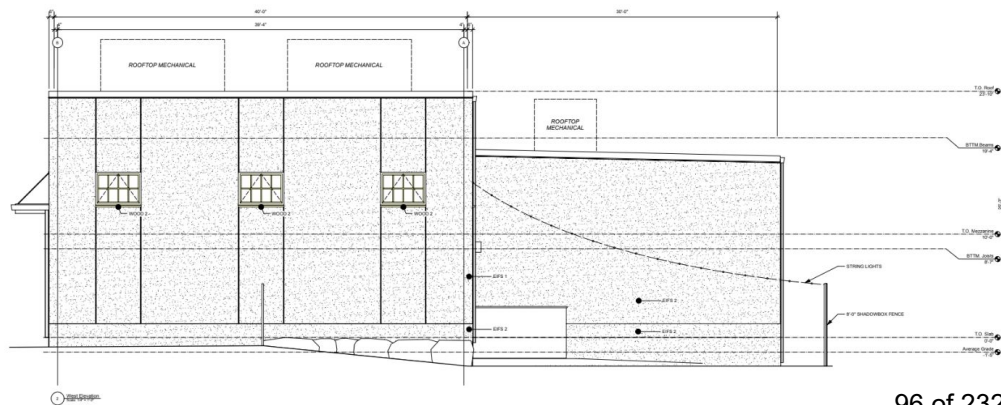
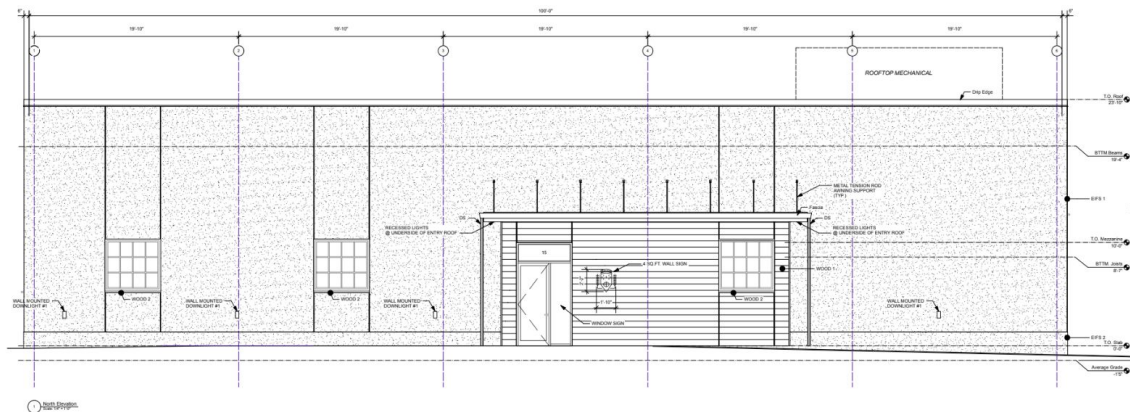
**Project File**  
**Arlington Brewing Co.**  
15 Ryder St.  
Arlington MA

**Sheet File**  
Proposed Roof Plan

**NOT FOR CONSTRUCTION**

Project ID	Drawing No.
01-27-2025	A130

# Proposed Elevations and Signage



**NOTES**

- EIFS COLOR 1
- EIFS COLOR 2
- WOOD 1
- WOOD 2
- WINDOW FRAMES
- STOREFRONT
- FASCIA & GUTTER @ AWNING
- DRIP EDGE & GUTTER @ BUILDING

No.	Date	Issue Number	By	Check	Revision Notes
1	01/27/2025	1	AR	AR	Initial Set

**Reisen Design Associates**  
**ARCHITECTURE**  
 308 Essex Street, Suite B  
 Cambridge MA 02139  
 (617) 875-7440

**Project Name**  
**Arlington Brewing Co.**  
 15 Ryder St.  
 Arlington MA

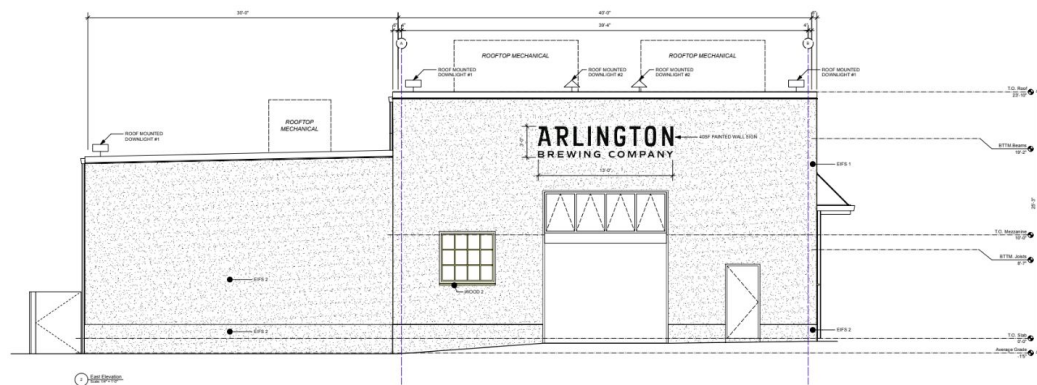
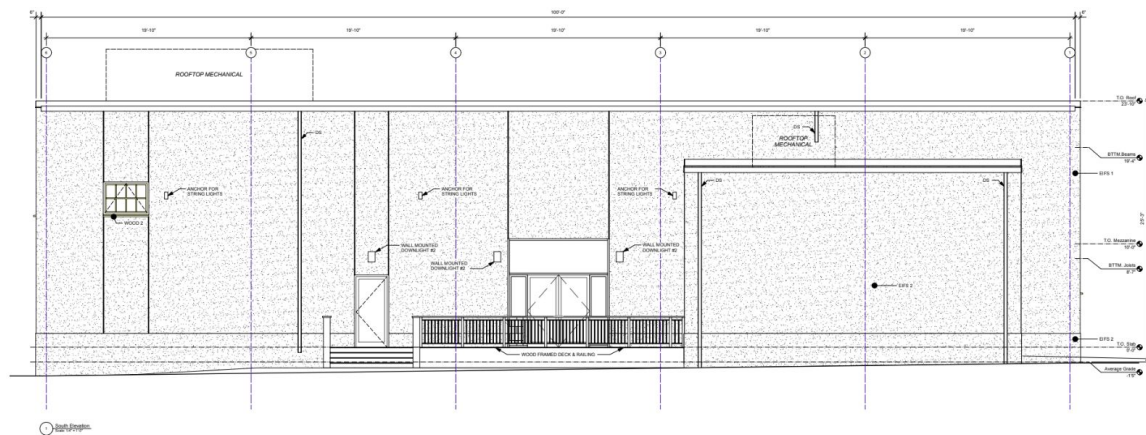
**Project No.**  
**A210**

**Proposed Building Elevations**

Project No.	Drawing No.
NOT FOR CONSTRUCTION	A210

**NOT FOR CONSTRUCTION**

**DATE**  
 01/27/2025



**NOTES**

- EIFS COLOR 1
- EIFS COLOR 2
- WOOD 1
- WOOD 2
- WINDOW FRAMES
- STOREFRONT
- FASCIA & GUTTER @ AWNING
- DRIP EDGE & GUTTER @ BUILDING

PROJECT			
No.	Date	Issue	Revision Notes

**Reisen Design Associates**  
ARCHITECTS  
1201 Essex Street, Suite 6  
Cambridge MA 02138  
(617) 876-7440

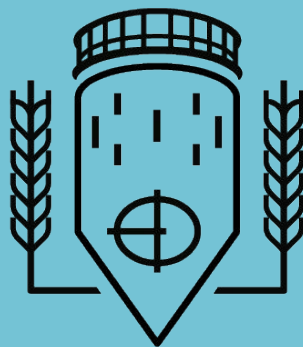
**Arlington Brewing Co.**  
15 Ryder St.  
Arlington MA

**Proposed Building Elevations**

Project ID	Drawing No.
NOT FOR CONSTRUCTION	A220

DATE: 01-27-2025





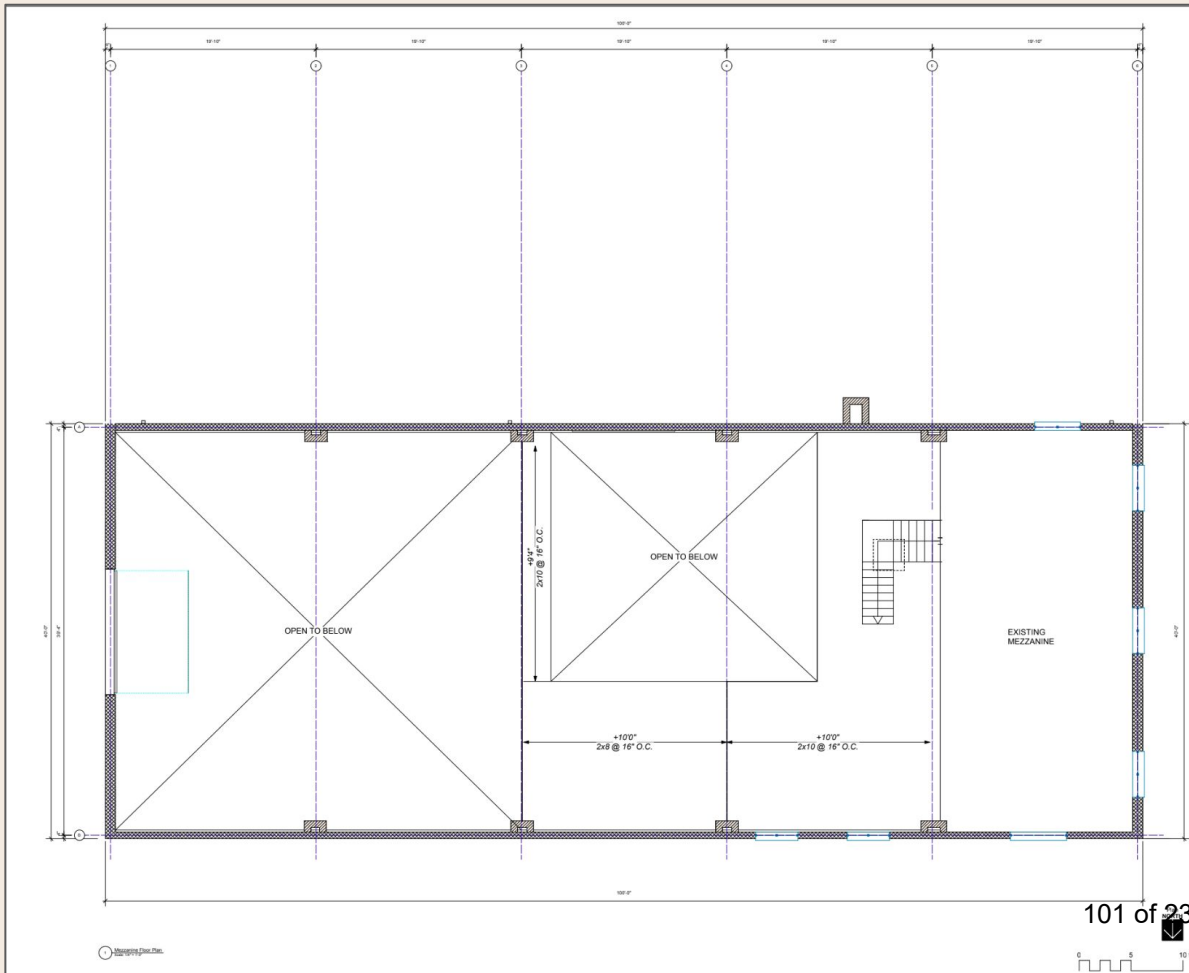
**ARLINGTON**

**BREWING**

**• CO •**

# Drawings of Existing Conditions





**NOTES**

No.	Date	Revised Notes	No.	Date	Revised Notes

**Reisen Design Associates**  
ARCHITECTURE  
120C Essex Street, Suite 6  
Cambridge MA 02139  
(617) 876-7440

**Project Title:** Arlington Brewing Co.  
15 Ryder St.  
Arlington MA

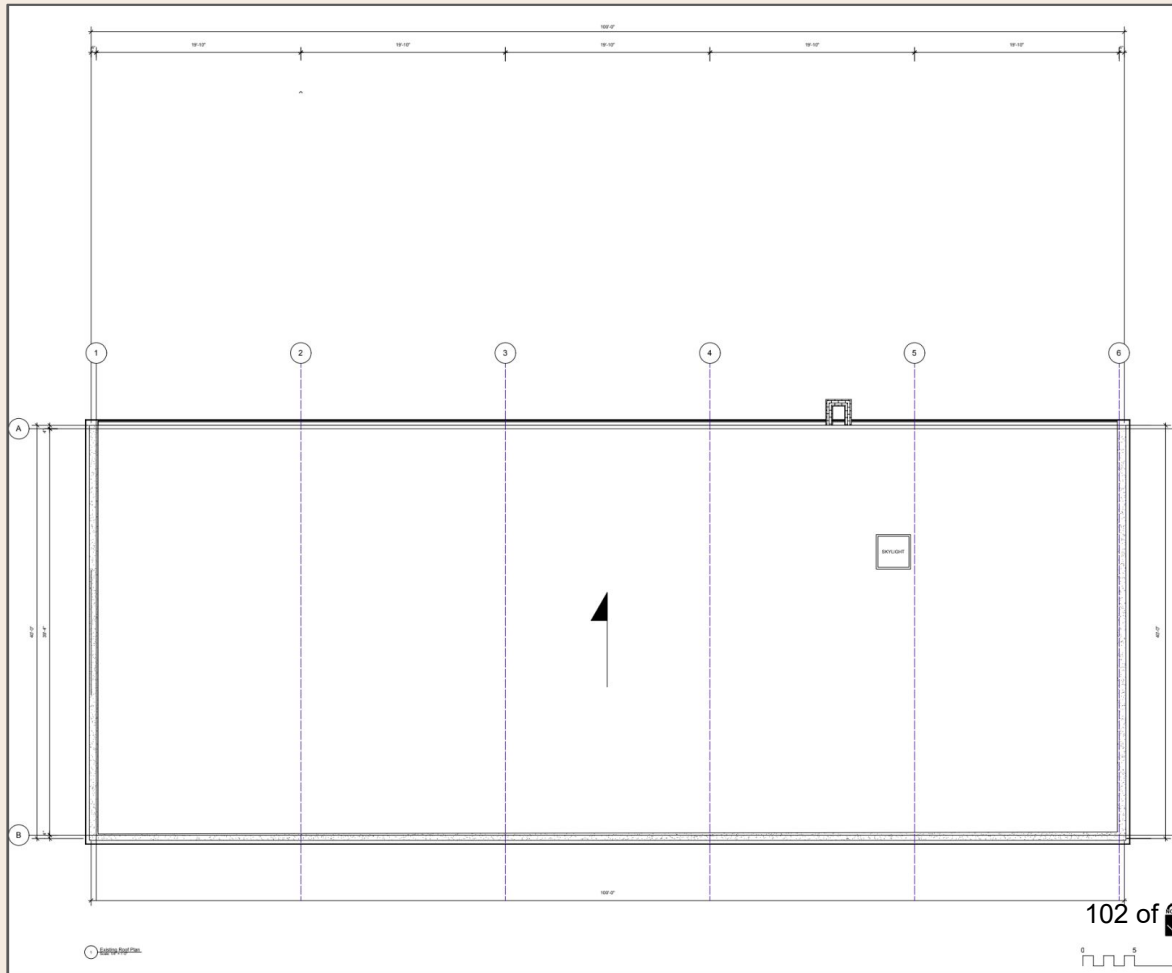
**Sheet Title:** Existing Mezzanine Floor Plan

Project ID	Drawing No.

**NOT FOR CONSTRUCTION**

**EC120**





**NOTES**

No.	Date	Revised By	No.	Date	Revised By

**Reisen Design Associates**  
**ARCHITECTURE**  
 320 Faxon Street, Suite 5  
 Cambridge MA 02139  
 (617) 675-7440

**Project Title**  
**Arlington Brewing Co.**  
 15 Ryder St.  
 Arlington MA

**Project Name**  
 Existing Roof Plan

**Project ID**  
 032

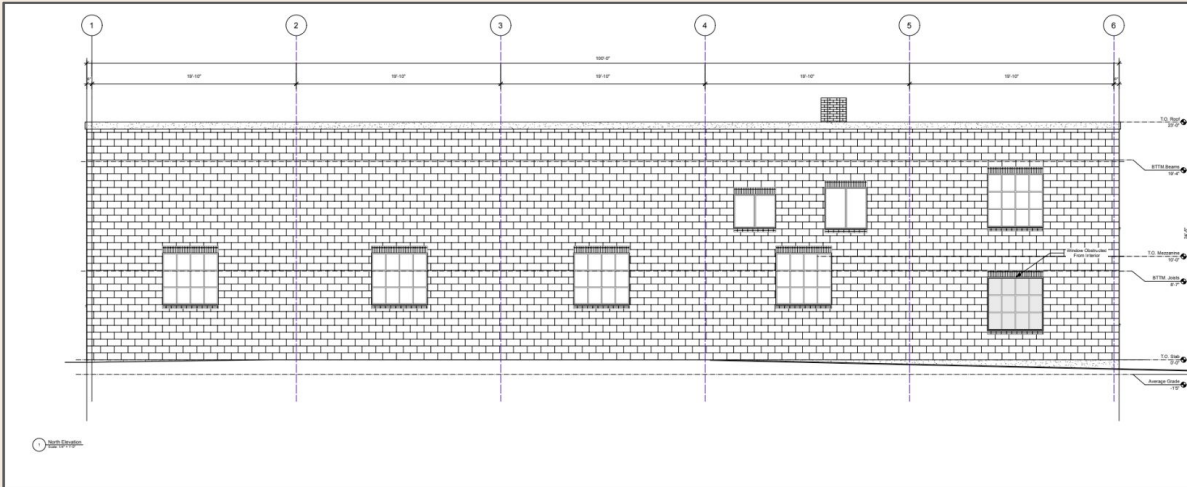
**Client By**  
 Not for Construction

**Revised By**  
 Not for Construction

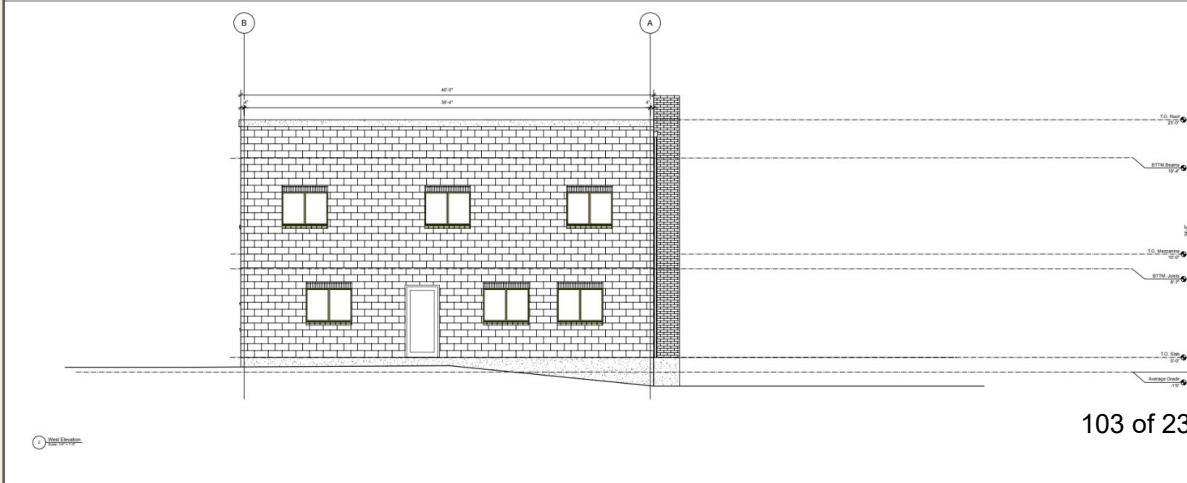
**Date**  
 As Noted

**Drawn**  
 01.27.2025

**Drawing No.**  
**EC130**



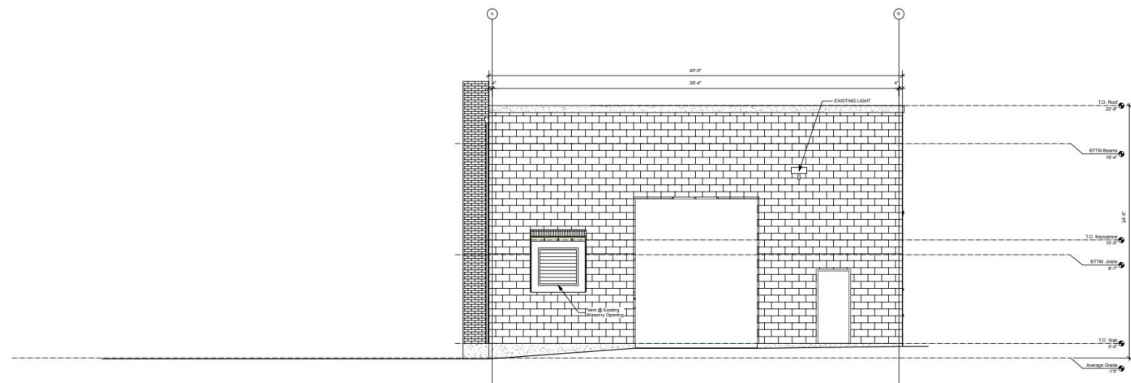
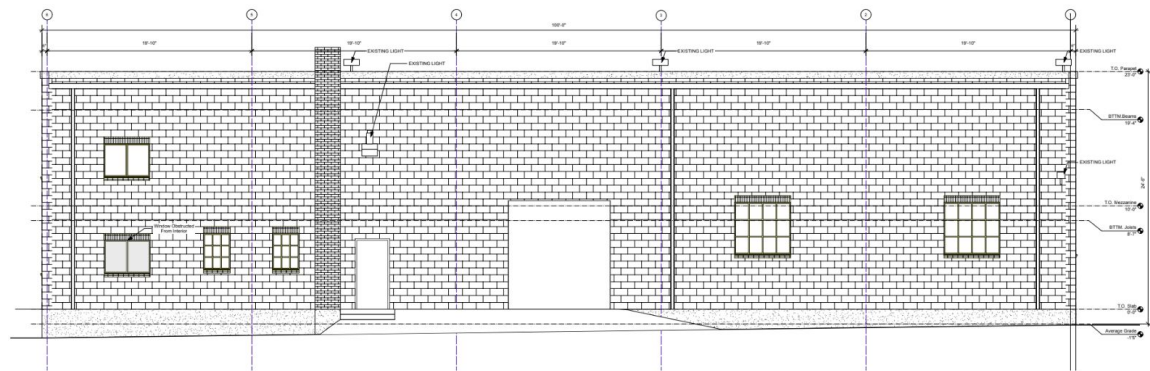
1. Elevation



2. Elevation

NOTES

Project Information					
No.	Date	Issue Number	Rev.	Drawn	Reviewed/Noted
<p><b>Reisen Design Associates</b>  <b>ARCHITECTURE</b>            308 Essex Street, Suite 6            Cambridge MA 02138            (617) 876-7446</p>					
<p><b>Project Name</b>            Arlington Brewing Co.            15 Ryder St.            Arlington MA</p>					
<p><b>Project Info</b>            Existing Building Elevations</p>					
<p><b>NOT FOR CONSTRUCTION</b></p>			<p><b>Drawing No.</b>  <b>EC210</b></p>		
<p>Project By: [Blank]            Drawn By: [Blank]            Reviewed By: [Blank]            Date: Aug 2020            Date: 01.27.2020</p>			<p>Project By: [Blank]            Drawn By: [Blank]            Reviewed By: [Blank]            Date: Aug 2020            Date: 01.27.2020</p>		



NOTES

Name		Phone Number		

**Reisen Design Associates**  
ARCHITECTURE  
32R Essex Street, Studio 6  
Cambridge MA  
02139  
(617) 876-7440

**Arlington Brewing Co.**  
15 Ryder St.  
Arlington MA

Existing Building Elevations

<div style="font-size: 48pt; font-weight: bold; margin-bottom: 10px;">2</div> <div style="font-size: 24pt; font-weight: bold;">NOT FOR CONSTRUCTION</div>	Project ID
	Drawn By
	Reviewed By
	Scale
	Date

## EC220



## Town of Arlington, Massachusetts

---

### Public Hearing: Warrant Articles for 2025 Annual Town Meeting

#### Summary:

8:20 pm

The Board will hear the proposed zoning amendments. The public hearing will include time both for public comment and for deliberation and discussion by the Board.

#### **ARTICLE 28**

##### **ZONING BYLAW AMENDMENT / DEFINITION OF LOT COVERAGE**

To see if the Town will vote to amend Section 2: Definitions, of the Zoning Bylaw, to add a new definition, Lot Coverage; or take any action related thereto.

#### **ARTICLE 29**

##### **ZONING BYLAW AMENDMENT / PARKING IN RESIDENTIAL DISTRICTS**

To see if the Town will vote to amend Section 6.1.10.A of the Zoning Bylaw, to amend and clarify standards for the location of parking in Residential districts; or take any action related thereto.

#### **ARTICLE 30**

##### **ZONING BYLAW AMENDMENT / SCREENING AND BUFFER REQUIREMENTS**

To see if the Town will vote to remove duplicate material by combining Section 5.3.7 and 5.3.21 of the Zoning Bylaw into Section 5.3.7 and thus deleting Section 5.3.21, revise Section 5.3.7 for clarity, move a subsection to Section 5.3.13, and amend a reference in Section 5.6.2; or take any action related thereto.

#### **ARTICLE 31**

##### **ZONING BYLAW AMENDMENT / REDEVELOPMENT BOARD JURISDICTION**

To see if the Town will vote to amend Section 3.4.2 of the Zoning Bylaw, to revise the special permit and environmental design review applicability for certain properties abutting the Minuteman Bikeway; or take any action related thereto.

#### **ATTACHMENTS:**

Type	File Name	Description
Reference Material	02242025_DPCD_Memo_to_ARB_Articles_28_29_30_31.pdf	02242025 DPCD Memo to ARB Articles 28,29,30,31





**TOWN OF ARLINGTON**  
DEPARTMENT OF PLANNING and  
COMMUNITY DEVELOPMENT

TOWN HALL, 730 MASSACHUSETTS AVENUE  
ARLINGTON, MASSACHUSETTS 02476  
TELEPHONE 781-316-3090

**MEMORANDUM**

To: Arlington Redevelopment Board  
From: Claire Ricker, AICP, Director, Planning and Community Development  
Sarah Suarez, AICP, Assistant Director, Planning and Community Development  
Date: February 20, 2025  
RE: Review of Warrant Articles 28, 29, 30, and 31 for 2025 Annual Town Meeting

---

Staff reviewed the following Warrant Articles to provide the Board with information for further consideration as part of the public hearing and review process. There are four articles with public hearings for the evening of February 24<sup>th</sup>. This memo provides information about each article being reviewed and additional factors for the Board's consideration.

---

**ARTICLE 28                      ZONING BYLAW AMENDMENT / DEFINITION OF LOT COVERAGE**

To see if the Town will vote to amend Section 2: Definitions, of the Zoning Bylaw, to add a new definition, Lot Coverage; or take any action related thereto.

(Inserted at the request of the Redevelopment Board)

**BACKGROUND**

---

Throughout the Zoning Bylaw, the term "Lot Coverage" is repeatedly used when describing dimensional requirements and development standards of a lot; however, there is no definition of the term in the Bylaw. This article proposes to add a new definition to the Zoning Bylaw which would formalize the Inspectional Services Department's standard practice to apply the definition of "Building Coverage" wherever "Lot Coverage" is used. The simplest solution is to add a definition for "Lot Coverage" that refers back to "Building Coverage" rather than change all references of "Lot Coverage" in the Bylaw.

**DRAFT AMENDMENT**

---

**Amend SECTION 2, Definitions:**

Building Coverage: The building area expressed as a percentage of the total lot area.

[Lot Coverage: Refer to definition for Building Coverage.](#)

## **ARTICLE 29            ZONING BYLAW AMENDMENT / PARKING IN RESIDENTIAL DISTRICTS**

To see if the Town will vote to amend Section 6.1.10.A of the Zoning Bylaw, to amend and clarify standards for the location of parking in Residential districts; or take any action related thereto.

(Inserted at the request of the Redevelopment Board)

### **BACKGROUND**

The Zoning Board of Appeals proposes revisions to Section 6.1.10.A – Parking in Residential Districts to increase readability, provide new guidance on dimensional standards for required vegetated buffers for side yard driveways and proximity of driveways to intersections, and clarify the required finding to allow second driveways for one- to three-family dwellings in the R0, R1, R2, R3 and R4 districts. The proposed changes to the section primarily reorganize and reformat the existing text for clarity and ease of reference, while adding two new dimensional requirements:

- (1) a driveway cannot be closer than 20 feet to an intersection, except by Special Permit; and
- (2) the required vegetated buffer for side yard driveways when abutting a residential lot shall be a minimum of 2.5 feet in width, or 1.5 feet in width with a minimum height of 4 feet, except as excluded by Section 5.3.12.B – Traffic Visibility for Driveways.

The amended finding as proposed requires a second driveway to provide for the safety of motorists, pedestrians and bicyclists, and to preserve Protected Trees as defined in Town Bylaws. Also proposed is a new, non-exclusive list of factors focused on transportation and safety for the ZBA to consider when rendering a decision on second driveways.

### **DRAFT AMENDMENT**

**Amend SECTION 6.1.10, Location of Parking Spaces, Subsection A, as follows:**

**A. Parking in Residential Districts.**

- (1) For single-family, two-family, duplex, and three-family dwellings in all Residential Districts:
  - a) Required off-street parking spaces shall are not be permitted in the area between the front lot line and the minimum front setback except on a driveway not exceeding 20 feet in width leading to the required parking space(s) that on a corner lot of less than 6,000 square feet the required off-street parking spaces are permitted in the longer of the two front yards, up to a maximum of 24 feet in width.
  - b) OffRequired off-street parking spaces are is permitted in (1) the side yard and rear yard on a paved driveway area, or in the case of a corner lot of less than 6,000 square feet in the longer of the two front yards, up to a maximum of 24 feet in width, or (2) in an attached or detached garage, or (3) and within the foundation of a dwelling provided the garaging garage is specifically designed for that purpose.
  - c) A driveway leading to off-street parking cannot exceed 20 feet in width and cannot be closer than 20 feet to an intersection. Any Further, a driveway leading to off-street parking on a lot cannot exceed a 15% downward slope, as measured from the farthest point from the front property line, except by Special Permit. A space designed for parking within an existing garage is determined to meet the requirements of an off-street parking space.

- d) ~~Side~~To minimize visual impacts, side yards used for parking shall have a vegetated buffer when abutting a lot used for residential purposes. ~~, to minimize visual impacts. Except as altered by Special Permit, such vegetated buffer shall be a minimum of two and one-half feet in width, or if its width is less than two and one-half feet it must have a minimum height of 4 feet, except no such buffer shall be over two and one-half feet in height above the adjacent ground within five feet of the front lot line unless it can be shown that such buffer will not restrict visibility in such a way as to hinder the safe entry of a vehicle from the driveway to the street.~~
- (2) For single-family, two-family, duplex, and three-family dwellings in R0, R1, R2, R3 and R4 districts:
- a) ~~not more than~~Only one driveway ~~shall be~~ is permitted on a lot unless ~~there is a finding by~~ the Special Permit Granting Authority ~~for the development finds~~ that a second driveway, or a driveway that makes more than one intersection with the street, may be added in a manner that:
1. ~~a~~Avoids an undue concentration of population,
  2. ~~a~~Allows adequate provision of transportation,
  3. Allows for the safety of those using the street and sidewalk, ~~and~~
  4. Preserves Protected Trees as defined in Town Bylaws,
  5. ~~e~~Conserves the value of land and buildings in the vicinity, ~~and~~
  6. Does not violate any other ~~In no case may a second driveway for a single-family, two-family, duplex, or three-family dwelling violate any other~~ driveway, parking, dimensional, or density ~~regulations~~ requirements for the district in which it is located. ~~For single-family, two-family, duplex, and three-family dwellings in R0, R1, R2, R3, and R4 districts, not more than two driveways are permitted.~~
- b) In making the findings required by subsection 6.1.10.A.(2)a., the Special Permit Granting Authority shall consider relevant factors that may include, among other relevant factors:
1. The functional classification and traffic volumes on the affected street or streets;
  2. Whether the affected street or streets are one-way or two-way;
  3. Proximity to signalized or unsignalized intersections;
  4. Sight lines;
  5. Potential conflicts with others using the street and sidewalk; and
  6. The presence of children or others who may be unable to protect themselves from roadway dangers.
- c) No more than two driveways are permitted on a lot.

## **ARTICLE 30 ZONING BYLAW AMENDMENT / SCREENING AND BUFFER REQUIREMENTS**

To see if the Town will vote to remove duplicate material by combining Section 5.3.7 and 5.3.21 of the Zoning Bylaw into Section 5.3.7 and thus deleting Section 5.3.21, revise Section 5.3.7 for clarity, move a subsection to Section 5.3.13, and amend a reference in Section 5.6.2; or take any action related thereto.

(Inserted at the request of the Redevelopment Board)

### **BACKGROUND**

This article proposes administrative corrections relative to duplicate sections of the Zoning Bylaw that remain following a previous Zoning Bylaw recodification. Sections 5.3.7 and 5.3.21 both address screening and buffering requirements and are proposed to be combined while retaining Sections 5.3.7.D and 5.3.21.B which are not duplicated. As part of this administrative correction, references made to dimensional standards for yards, setbacks, and screens or buffers in other parts of the Bylaw are also amended for clarification.

### **DRAFT AMENDMENT**

**Amend SECTION 5.3.7, Screening and Buffers, as follows:**

#### **5.3.7. Screening and Buffers: Industrial and Business Districts and Parking Lots**

- A. Screening and space buffers shall be required in any Industrial (I) or Business (B) district that abuts ~~certain~~ buildable residential lots. The minimum width of the buffer shall be as follows:

I or B District	Abutting R District	Minimum Buffer
I, B5	R0 through R5	25 ft.
B3, B2A, B4	R0 through R5	15 ft.
I	R6 through R7	10 ft.
B1, B2	R0 through R5	10 ft.

- (1) The ~~strip buffer~~ shall contain a screen of plantings not less than three feet wide and six feet high at the time of occupancy of such lot.
- (2) Individual shrubs or trees shall be planted not more than 20 feet on center and shall be maintained by the owner or occupants so as to maintain a dense screen year-round. At least 50 percent of the plantings shall consist of evenly-spaced evergreens ~~and they shall be evenly spaced.~~
- (3) A solid wall or solid wooden fence, five to six feet high, complemented by suitable plantings, may be substituted for one-half the required width of such landscaped buffer strip; ~~however, provisions of this section shall not supersede the minimum setbacks for parking lots per Section 6.1 nor the minimum yard requirements of the district. No screen shall be closer than 10 feet to a public or private way.~~ Where deemed appropriate by the property owner and immediate abutters, and as approved by the Building Inspector, another wall or fence height or fence type, including but not limited to coated chain link or “wrought iron” types may be substituted for the required wall or fence.
- (4) No screen shall be closer than 10 feet to a public or private way.



- (5) The requirements of this Section 5.3.7.A. shall not supersede the minimum setbacks for parking lots per Section 6.1 nor the minimum yard requirements of the applicable I or B district.
- (6) For any area used for the parking of more than five vehicles, the requirements of this Section 5.3.7.A. shall not supersede the screening provisions of Section 6.1, Off-Street Parking.
- ~~B. For any area used for the parking of more than five vehicles, the screening provisions of Section 6.1, Off-Street Parking and Loading, shall apply.~~
- ~~C. Screening and space buffers shall not be required where abutting railroad track or railroad right-of-way if railroad is to be utilized for loading or unloading.~~
- ~~D.~~ In Industrial Districts, screening along the Minuteman Bikeway shall be limited to a vegetative screen, guardrail, and/or low fence under 4 feet in height only. Such screening shall either have gaps or vary in height to provide lines of sight from the Minuteman Bikeway to the adjoining property to promote safety for pedestrians and bicyclists. Pedestrian amenities such as seating, bins for recycling and refuse collection, and appropriate supplementary lighting shall be integrated within the landscaped area of the buffer.

**Amend SECTION 5.3.13, Accessory Buildings and Other Structures, as follows:**

**5.3.13. Accessory Buildings and Other Structures**

- B. An accessory building attached to the principal building shall be considered as an integral part thereof and shall be subject to front, side, and rear yard requirements applicable to the principal building per Section 5.4.2.
  - (1) In ~~Residence~~ Residential districts, a minor accessory building shall be exempt from side and rear yard requirements if said building dimensions result in a gross floor area of not more than 80 square feet and a building height of not more than 7 feet.
  - (4) An accessory private swimming pool shall be completely enclosed by a fence the top of which shall be at least 5 feet in height above the pool, having a self-closing gate with a latch. Above-ground pools may be unfenced if they are less than 24 inches in depth or with walls 4 feet or greater in height and removable. The unnumbered side of a corner lot shall be considered as a side yard for the purposes of establishing minimum setback requirements for accessory private swimming pools in all ~~Residence~~ Residential districts. See Section 5.4.2.(A) District Yard and Open Space Requirements.
- C. In Business and Industrial districts, accessory structures must comply with the minimum yard, maximum height, and minimum open space requirements of the district in which they are located.

Delete SECTION 5.3.21, Supplemental Requirements in the Business and Industrial Districts, as follows:

~~5.3.21. Supplemental Requirements in the Business and Industrial Districts~~

~~A. Screening and Buffers: Industrial and Business Districts and Parking Lots~~

- ~~(1) Screening and space buffers shall be required in any Industrial (I) or Business (B) district that abuts certain buildable residential lots. The minimum width of the buffer shall be as follows:~~

<del>I or B District</del>	<del>Abutting R District</del>	<del>Minimum Buffer</del>
<del>I, B5</del>	<del>R0 through R5</del>	<del>25 ft.</del>
<del>B3, B2A, B4</del>	<del>R0 through R5</del>	<del>15 ft.</del>
<del>I</del>	<del>R6 through R7</del>	<del>10 ft.</del>
<del>B1, B2</del>	<del>R0 through R5</del>	<del>10 ft.</del>

~~The strip shall contain a screen of plantings of vertical habit not less than three feet in width and six feet in height at the time of occupancy of such lot. Individual shrubs or trees shall be planted not more than 20 feet on center, and shall thereafter be maintained by the owner or occupants to maintain a dense screen year-round. At least 50% of the plantings shall consist of evergreens and they shall be evenly spaced. A solid wall or solid wooden fence, five to six feet in height, complemented by suitable plantings, may be substituted for one-half the required width of such landscaped buffer strip; however, provisions of this section shall not supersede the minimum setbacks for parking lots per Section 6.1 nor the minimum yard requirements of Sections 5.5 and 5.6. No screen shall be closer than 10 feet to a public or private way. Where deemed appropriate by the property owner and immediate abutters, and as approved by the building inspector, another wall or fence height or fence type, including but not limited to coated chain link or "wrought iron" types may be substituted for the required wall or fence. See Section 5.3.7 for screening and buffer requirements for Business districts, Industrial districts, and parking lots.~~

- ~~(2) For any area used for the parking of more than five vehicles, the screening provisions of Section 6.1, Off-Street Parking, shall apply.~~

~~B. Accessory Structures. Accessory structures must comply with the minimum yard, maximum height, and minimum open-space requirements of the district in which they are located.~~

~~C. Minimum side and rear yards in Industrial Districts and minimum front, side, and rear yard are not required when abutting railroad track or railroad right-of-way if railroad is utilized for loading or unloading.~~

Amend SECTION 5.6.2, Dimensional and Density Regulations, Subsection D, as follows:

5.6.2. Dimensional and Density Regulations

- D. Development Standards. In the Industrial District, the following requirements apply to all new development or additions over 50% of the existing footprint:

(2) Yards

- Where feasible, the principal façade of the principal building on the site shall be no more than 10 feet from the front lot line.
- The use of rain gardens, bioswales, and wetlands restoration to control runoff and manage stormwater on-site within setbacks is strongly encouraged. Such systems shall be integrated with the surface water drainage systems in Section 3.4.4.E. See Section 6.1.11.F(3) for relationship to parking areas.
- Fences greater than 4 feet tall within the abutting setback to the Minuteman Bikeway shall be prohibited. See Section 5.3.7.~~DB~~ for additional requirements.

## **ARTICLE 31 ZONING BYLAW AMENDMENT / REDEVELOPMENT BOARD JURISDICTION**

To see if the Town will vote to amend Section 3.4.2 of the Zoning Bylaw, to revise the special permit and environmental design review applicability for certain properties abutting the Minuteman Bikeway; or take any action related thereto.

(Inserted at the request of the Redevelopment Board)

### **BACKGROUND**

Last year, two applications were filed with the Department of Planning and Community Development for an Environmental Design Review to add a dormer to a single-family home and to a two-family home, respectively, as both were located along the Minuteman Bikeway. Under Section 3.4.2 of the Zoning Bylaw, any alterations to the façade in a manner that affects the architectural integrity of the structure on certain residential properties abutting the Minuteman Bikeway require review and approval by the Arlington Redevelopment Board. During the public hearing for the dormer addition on the two-family dwelling, the ARB decided the proposal did not alter the façade in a manner that would trigger an Environmental Design Review even though it is along the Bikeway and, therefore, should not be subject to its review. It was suggested that DPCD and ISD staff make future determinations administratively. Further, the ARB does not typically review low-density residential development so after conferring with the Chair of the Zoning Board of Appeals, this article proposes to remove single-family, two-family, and duplex properties in a R1 or R2 zoning district along the Minuteman Bikeway from the ARB's jurisdiction and place those within the ZBA's jurisdiction. This change would benefit impacted property owners who undertake certain types of renovation projects. Where additional relief from the Zoning Bylaw is required (such as for a large addition, or to build a porch that extends into the minimum yard regulations beyond what is allowed by right), owners of a single-family, two-family, or duplex dwelling along the Minuteman Bikeway would be able to apply to the ZBA for a hearing. This would streamline the process of obtaining all the relief needed for a residential renovation project from a Special Permit Granting Authority.

### **DRAFT AMENDMENT**

**Amend SECTION 3.4.2, Environmental Design Review, Subsection A, as follows:**

#### **3.4.2. Applicability**

In any instance where a new structure, or a new outdoor use, or an exterior addition or a change in use (a) requires a building permit and special permit in accordance with use regulations for the applicable district or (b) alters the façade in a manner that affects the architectural integrity of the structure, and (c) is one of the uses listed in subparagraphs A through I below, the special permit shall be acted upon by the Arlington Redevelopment Board in accordance with the environmental design review procedures and standards of this Section 3.4.

A. Construction or reconstruction on a site abutting any of the following:

- (1) Massachusetts Avenue, Pleasant Street, Mystic and Medford Streets between Massachusetts Avenue and Chestnut Street, ~~or~~ Broadway, ~~or the Minuteman Bikeway.~~
- (2) The Minuteman Bikeway, unless the site is located in a R1 or R2 district and concerns a single-family, two-family or duplex dwelling, where the applicable Special Permit Granting Authority is the Zoning Board of Appeals.





## Town of Arlington, Massachusetts

---

### Discussion of B1 Rezoning

#### Summary:

9:15 pm      The Board will discuss the B1 Rezoning warrant article.

#### ATTACHMENTS:

	Type	File Name	Description
▢	Reference Material	Parcels_Zoned_B1.pdf	Parcels Zoned B1

## Parcels Zoned B1

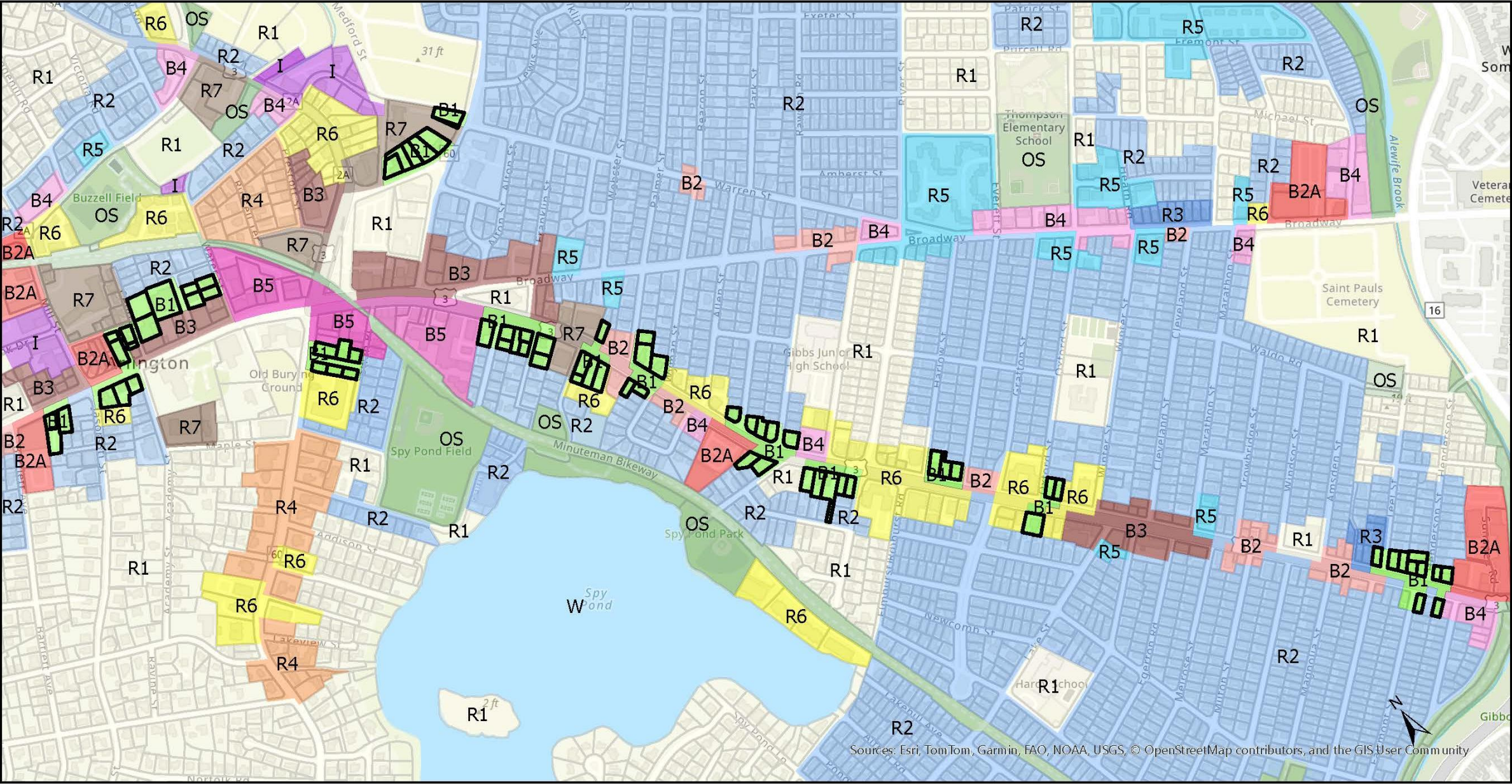
Full Address	GIS SqFt	Landuse Description	Parcel ID
43-45 MASS AVE	5,258	111 - Apts. 4-8	23-5-6
0LOT MASS AVE	3,426	132 - Undevelopable Land	23-5-7
48-50 MASS AVE	4,659	104 - Two Family	1-3-3
58-60 MASS AVE	5,121	013 - Res. / Comm.	1-3-1
63 MASS AVE	3,983	340 - Office	23-3-12
67 MASS AVE	3,993	104 - Two Family	23-3-13
8-10 HENDERSON ST	5,597	105 - Three Fam.	23-3-11
70-72 MASS AVE	5,261	031 - Comm. / Res.	1-2-4
71-73 MASS AVE	5,551	013 - Res. / Comm.	23-3-14
77 MASS AVE	6,109	109 - Multi-House	23-3-15
89 MASS AVE	4,964	340 - Office	23-1-14
221 MASS AVE	5,081	104 - Two Family	28-3-9
223 MASS AVE	5,200	340 - Office	28-3-10
226 MASS AVE	12,103	031 - Comm. / Res.	6-2-3.A
251 MASS AVE	5,638	102 - Condo	29.A-3-11.1 / 29.A-3-11.2
255 MASS AVE	6,423	111 - Apts. 4-8	29-3-12.A
259 MASS AVE	8,207	343 - Condo-Comm	29.A-3-13.A / 29.A-3-13.B / 29.A-3-13.C / 29.A-3-13.D
286 MASS AVE	5,302	105 - Three Fam.	8-1-5.B
288 MASS AVE	5,403	104 - Two Family	8-1-5.A
290 MASS AVE	14,285	013 - Res. / Comm.	8-1-4
292 MASS AVE	8,521	031 - Comm. / Res.	8-1-3
294-298 MASS AVE	8,133	013 - Res. / Comm.	8-1-2
305 MASS AVE	6,673	101 - One Family	29-1-3
310 MASS AVE	7,695	102 - Condo	9.A-2-24.1 / 9.A-2-24.2 / 9.A-2-24.3
311 MASS AVE	6,448	101 - One Family	31-5-18
314 MASS AVE	7,027	109 - Multi-House	9-2-23
315-317 MASS AVE	5,729	104 - Two Family	31-5-19
319 MASS AVE	4,815	340 - Office	31-5-1
325-327 MASS AVE	5,184	105 - Three Fam.	31-3-13
355 MASS AVE	9,616	013 - Res. / Comm.	31-1-14.A
358 MASS AVE	3,949	013 - Res. / Comm.	9-2-5
360 MASS AVE	4,269	013 - Res. / Comm.	9-2-4
361 MASS AVE	11,342	031 - Comm. / Res.	31-1-1.A
7-9 PALMER ST	6,213	111 - Apts. 4-8	31-1-2
370 MASS AVE	9,534	954 - Funct. Hall	10-4-3

Full Address	GIS SqFt	Landuse Description	Parcel ID
373-375 MASS AVE	5,036	013 - Res. / Comm.	32-2-18
374 MASS AVE	8,715	102 - Condo	10.A-4-2 / 10.A-4-3 / 10.A-4-4 / 10.A-4-5
378 MASS AVE	8,009	013 - Res. / Comm.	10-4-1.A
10 POND LN	3,107	101 - One Family	10-4-1.B
390 MASS AVE	10,149	031 - Comm. / Res.	10-3-1
8-10 AVON PL	7,129	104 - Two Family	10-3-12
400-402 MASS AVE	4,194	031 - Comm. / Res.	10-2-3.A
7 AVON PL	4,040	101 - One Family	10-2-3.B
404 MASS AVE	5,823	105 - Three Fam.	10-2-2
406 MASS AVE	6,002	031 - Comm. / Res.	10-2-1
6 WHITTEMORE ST	4,272	102 - Condo	10-2-15.A
8 WHITTEMORE ST	4,272	102 - Condo	10-2-15.B
418 MASS AVE	10,209	355 - Funeral	10-1-7
2 SWAN ST	8,443	102 - Condo	11.A-2-2
4 SWAN ST	8,443	102 - Condo	11.A-2-3
5 SWAN ST	4,792	102 - Condo	11-4-2.B
7 SWAN ST	4,792	102 - Condo	11-4-2.A
10 SWAN ST	7,385	102 - Condo	11.A-2-10.1 / 11.A-2-10.2
13 SWAN ST	4,296	102 - Condo	11.A-4-13
15 SWAN ST	4,296	102 - Condo	11.A-4-15
16 SWAN ST	4,663	111 - Apts. 4-8	11-2-10.A
22 PLEASANT ST	8,443	102 - Condo	11.A-2-1
40 PLEASANT ST	4,468	102 - Condo	11.A-4-40.1 / 11.A-4-40.2
42 PLEASANT ST	4,468	343 - Condo-Comm	11.A-4-42.A
6-8 WATER ST	5,966	105 - Three Fam.	50-6-3
12 WATER ST	6,272	102 - Condo	50.A-6-12.1 / 50.A-6-12.2 / 50.A-6-12.3
OLOT COURT ST	4,862	337 - Parking Lot	50-6-8
9 COURT ST	6,917	340 - Office	50-6-7
10 COURT ST	27,686	900 - U.S. Govt.	51-4-9
OLOT CENTRAL ST	15,090	900 - U.S. Govt.	51-4-2
6 CENTRAL ST	1,603	101 - One Family	51-1-6
7 CENTRAL ST	14,444	340 - Office	51-4-1
8-10 CENTRAL ST	5,917	109 - Multi-House	51-1-5
OLOT BACON ST	3,267	962 - Other	51-1-3
734-736 MASS AVE	6,284	013 - Res. / Comm.	124-2-9
735 MASS AVE	9,159	960 - Church	51-1-8
742 MASS AVE	9,547	031 - Comm. / Res.	124-2-8
754 MASS AVE	9,616	340 - Office	124-2-7

Full Address	GIS SqFt	Landuse Description	Parcel ID
792 MASS AVE	9,075	340 - Office	124-1-3
800 MASS AVE	5,097	340 - Office	124-1-1
OLOT MASS AVE	11,176	337 - Parking Lot	124-1-2
1007 MASS AVE	15,174	935 - Improved-Mun	55-2-15
1011 MASS AVE	4,695	105 - Three Fam.	55-2-16
1013R MASS AVE	4,488	101 - One Family	55-2-17
1017 MASS AVE	8,120	104 - Two Family	55-2-18
1025 MASS AVE	47,085	112 - Apts. 8 Plus	55-2-20
1087-1089 MASS AVE	8,031	105 - Three Fam.	56-2-7
1090 MASS AVE	6,450	013 - Res. / Comm.	148-4-4
1122 MASS AVE	7,245	013 - Res. / Comm.	148-3-2
1145 MASS AVE	8,258	355 - Funeral	57-2-18
OLOT QUINN RD	9,593	337 - Parking Lot	57-2-19
1171 MASS AVE	11,378	031 - Comm. / Res.	57-2-15.A
1173 MASS AVE	3,998	105 - Three Fam.	57-2-15.B
5 FOREST ST	11,178	316 - Comm. Whs.	57-2-13
1471 MASS AVE	2,457	105 - Three Fam.	62-1-2
OLOT MASS AVE	1,281	930 - Vacant, Municipal	62-1-3
1491-1493 MASS AVE	5,484	104 - Two Family	62-1-3.B
1497 MASS AVE	3,206	101 - One Family	62-1-6
1500-1502 MASS AVE	7,611	013 - Res. / Comm.	174-2-9
1501 MASS AVE	2,954	101 - One Family	62-1-7
1507-1511 MASS AVE	8,254	013 - Res. / Comm.	62-1-8
1508-1510 MASS AVE	1,636	326 - Restaurant	174-2-11.A
4-6 LANCASTER RD	3,428	104 - Two Family	174-2-12.A
1513-1515 MASS AVE	4,059	104 - Two Family	62-1-10
1516 MASS AVE	5,080	101 - One Family	174-1-6
1517-1519 MASS AVE	4,679	106 - Accessory Land	62-1-11.A
1520 MASS AVE	5,171	013 - Res. / Comm.	174-1-5
1521-1523 MASS AVE	4,808	104 - Two Family	62-1-12.A
1530 MASS AVE	7,006	104 - Two Family	174-1-3
58-60 MEDFORD ST	10,802	340 - Office	46-1-13
OLOT MEDFORD ST	1,188	132 - Undevelopable Land	46-1-14
1 CHESTNUT ST	13,145	340 - Office	45-2-5
5 CHESTNUT ST	11,853	355 - Funeral	45-2-4
9 CHESTNUT ST	8,006	342 - Pro-Office	45-2-3
13-15 CHESTNUT ST	8,836	102 - Condo	45.A-2-13 / 45.A-2-15
17 CHESTNUT ST	9,147	013 - Res. / Comm.	45-2-1

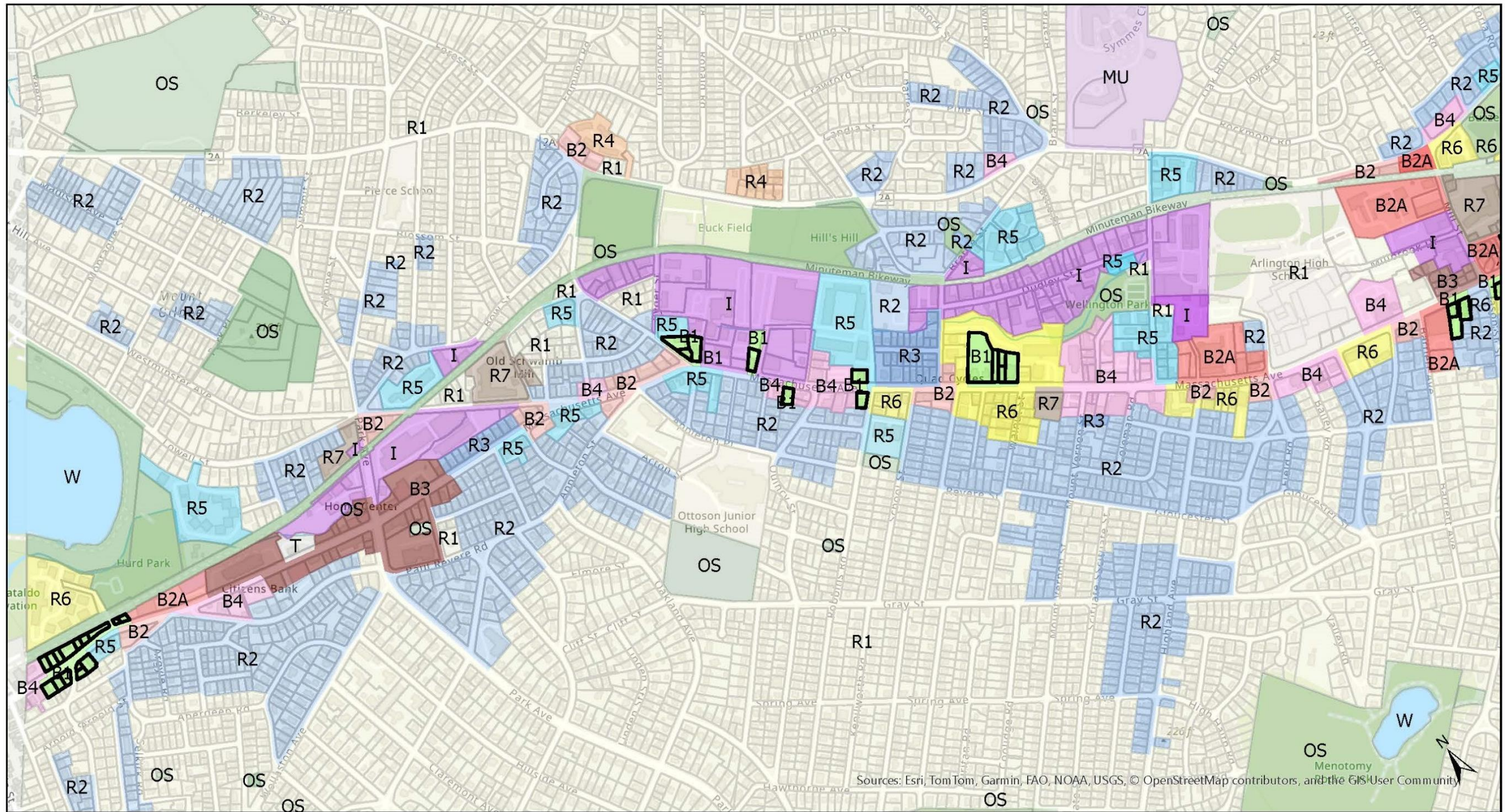


Zoning Along Mass Ave  
East Arlington to Town Center



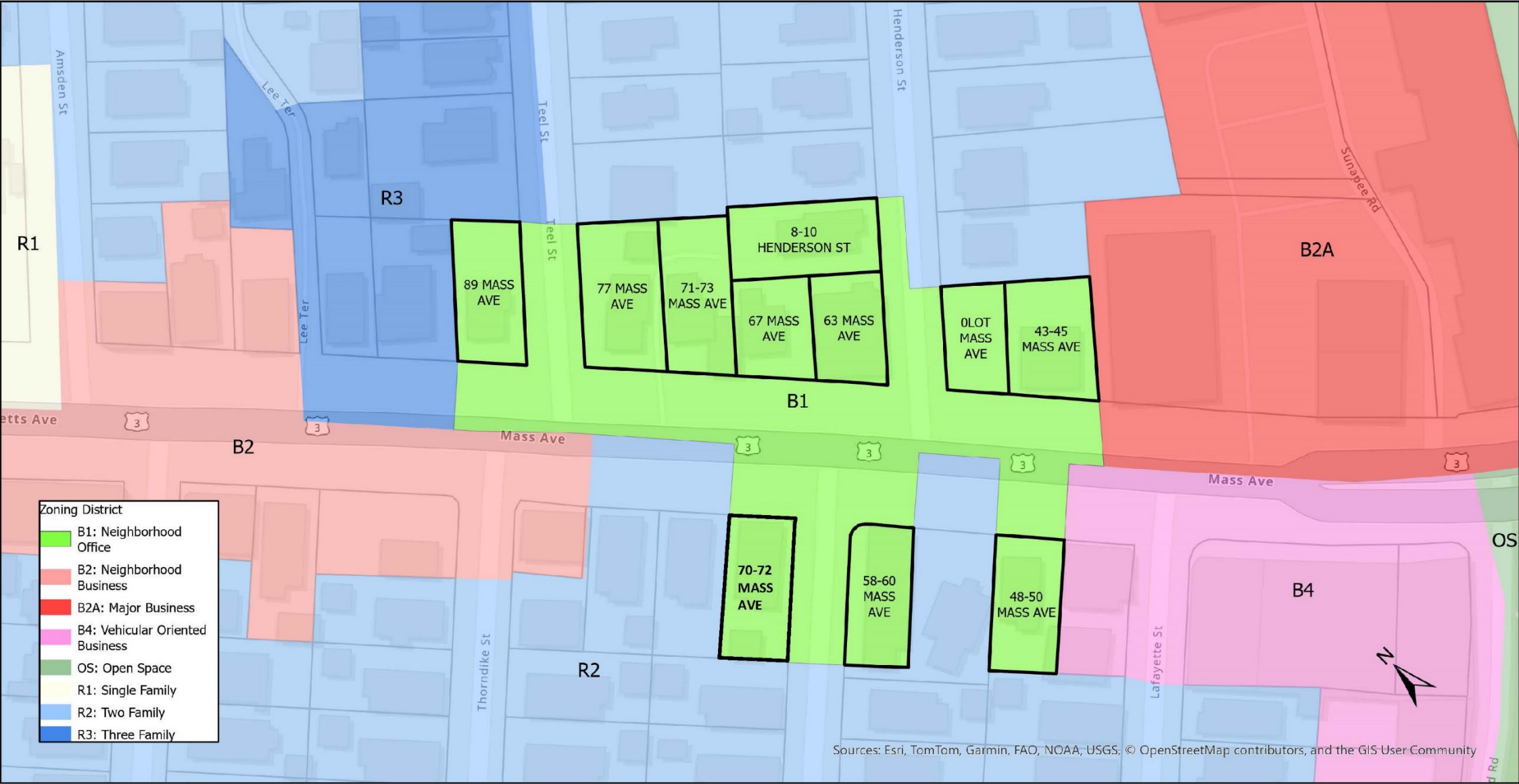


# Zoning Along Mass Ave Town Center to Arlington Heights

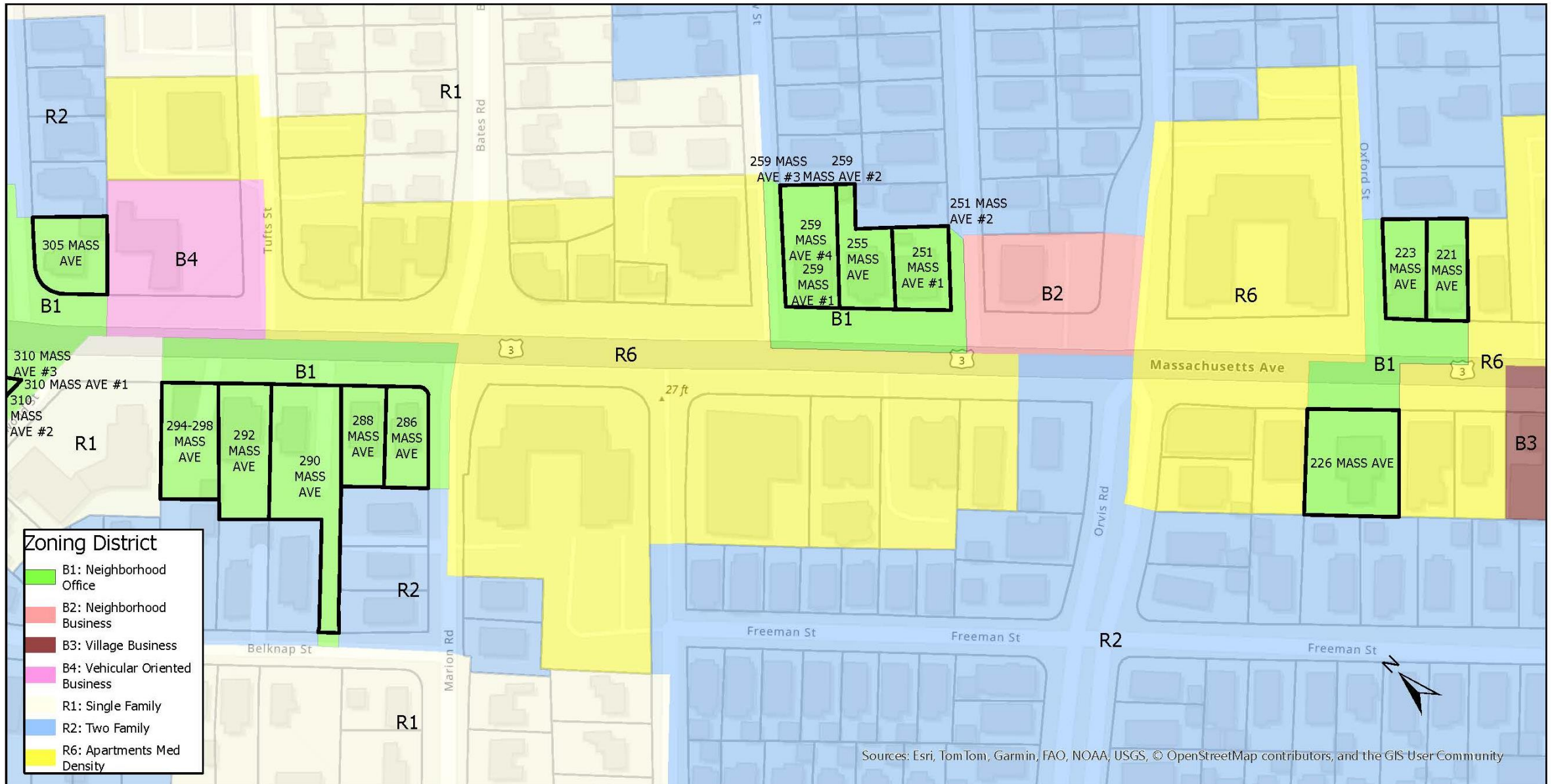




# B1 Zoned Parcels 43-89 Mass Ave



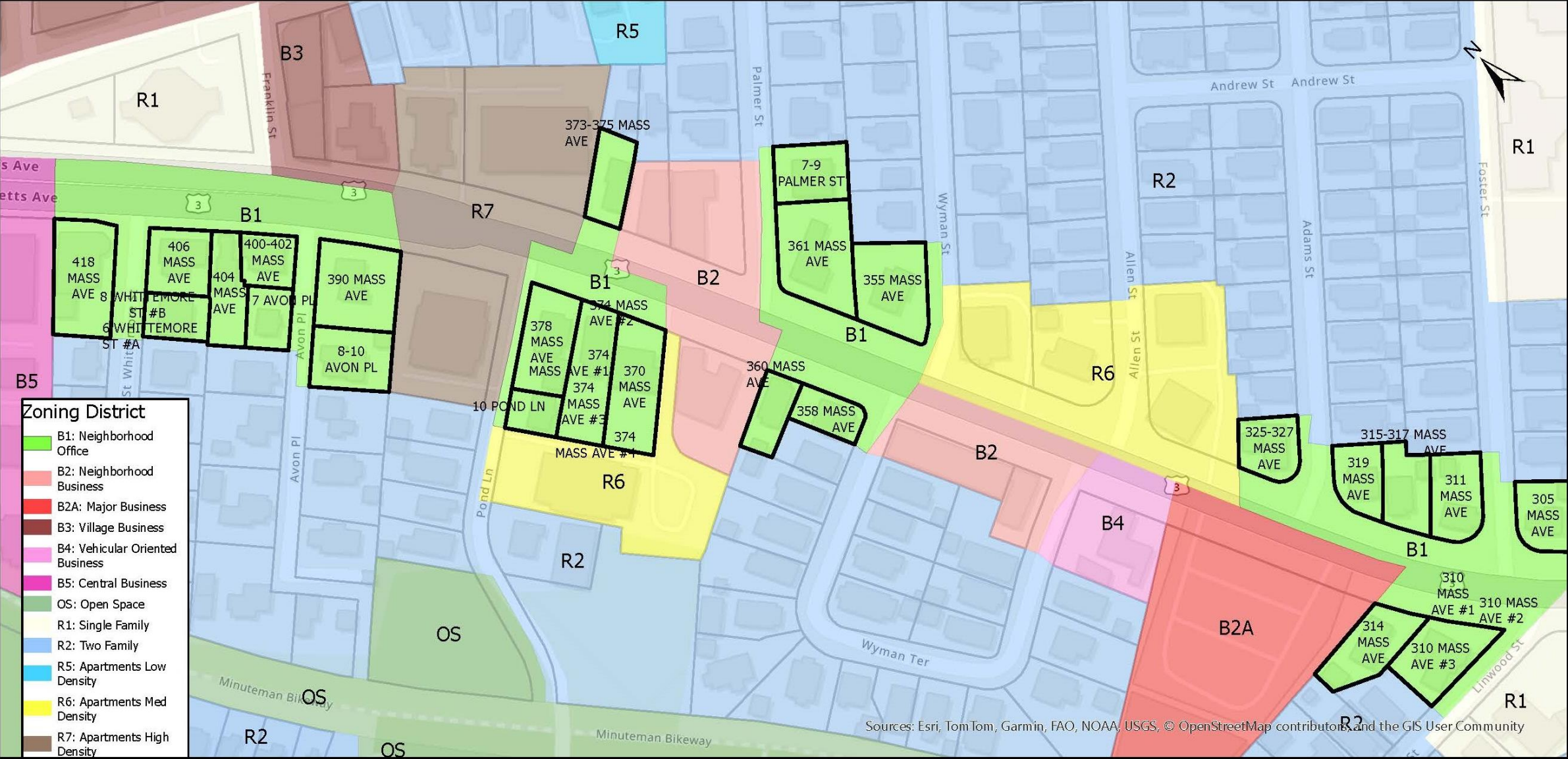
# B1 Zoned Parcels 221-305 Mass Ave





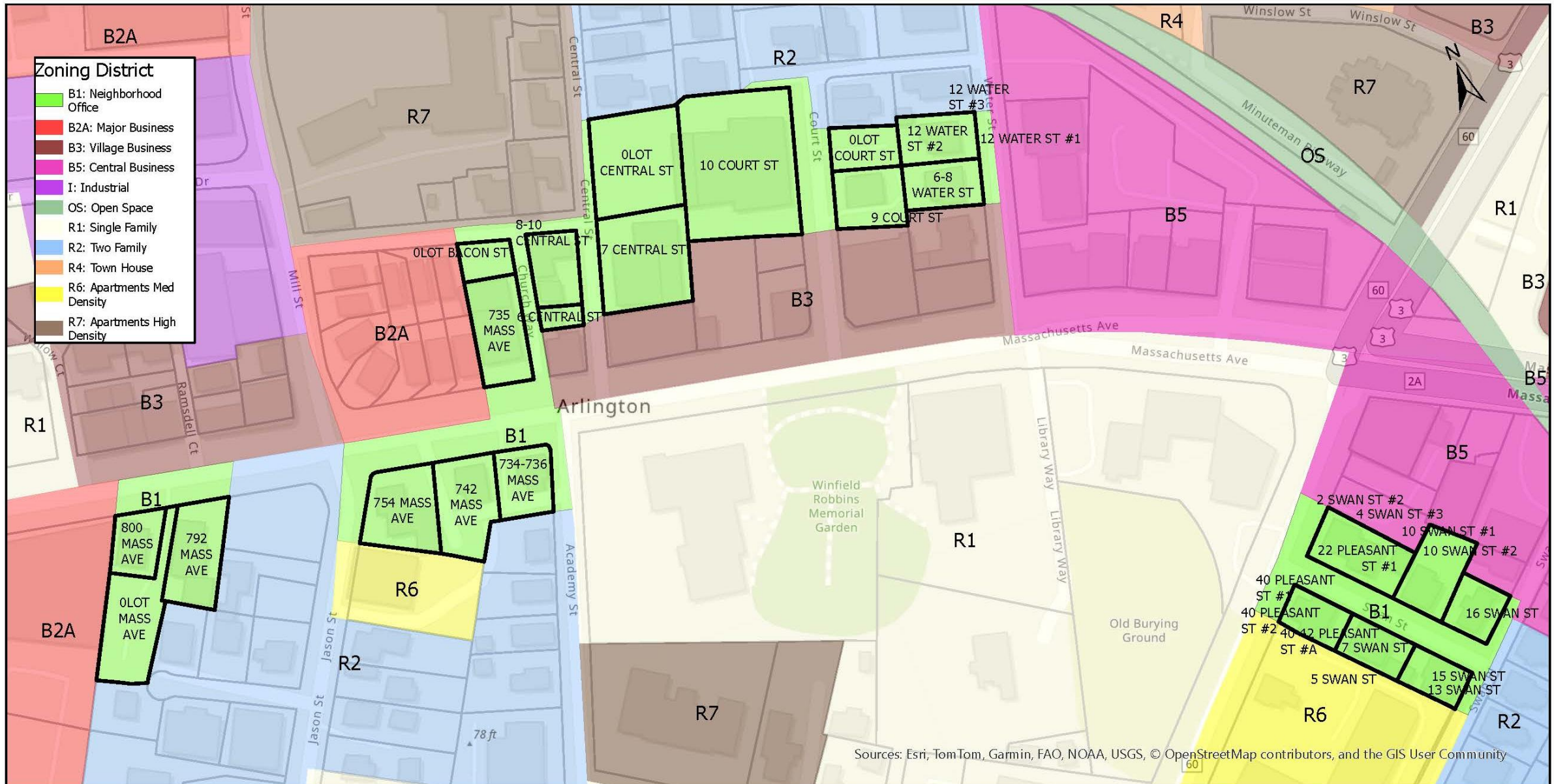
# B1 Zoned Parcels

## 305-418 Mass Ave



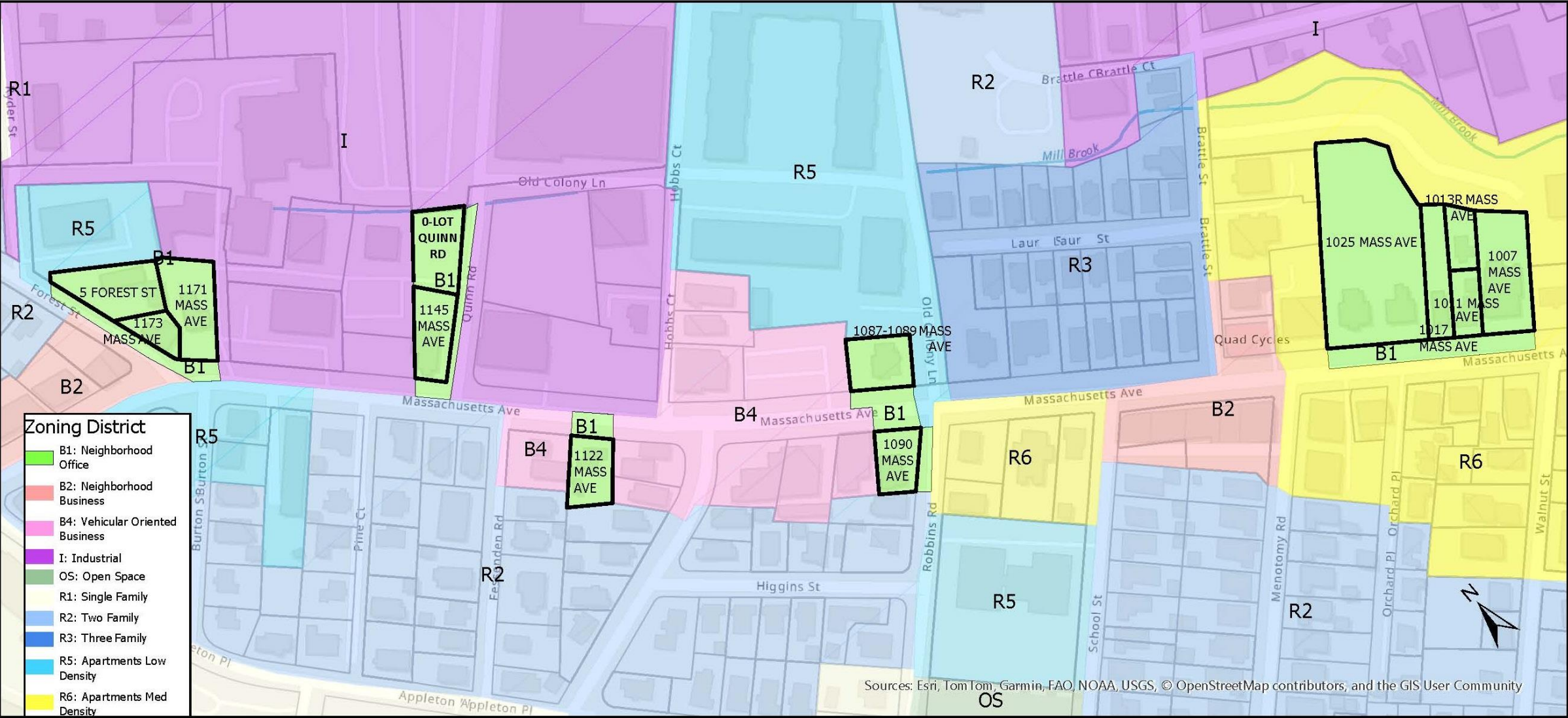


# B1 Zoned Parcels 734-800 Mass Ave Swan, Winter, Court, and Central St

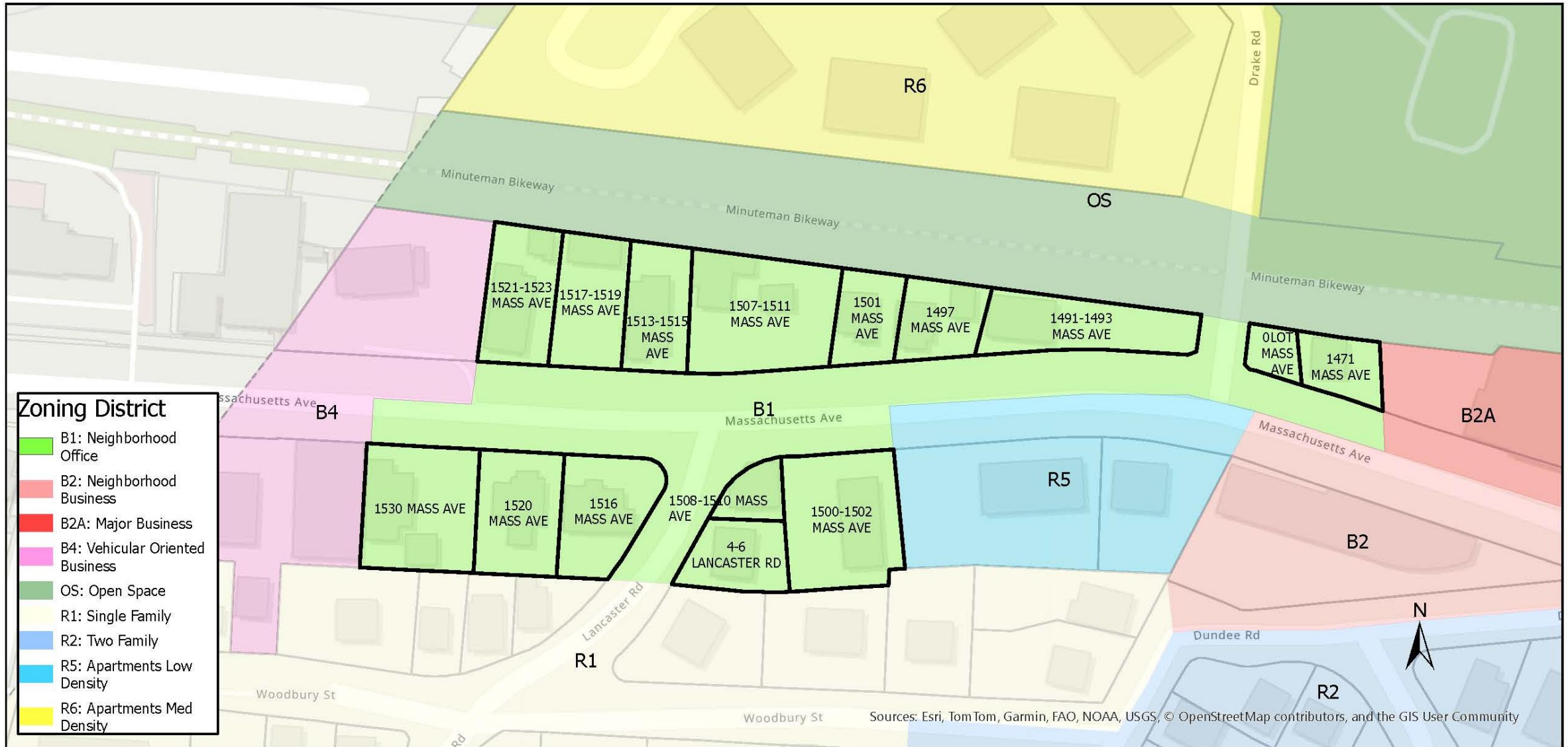




# B1 Zoned Parcels 1007- 1171 Mass Ave

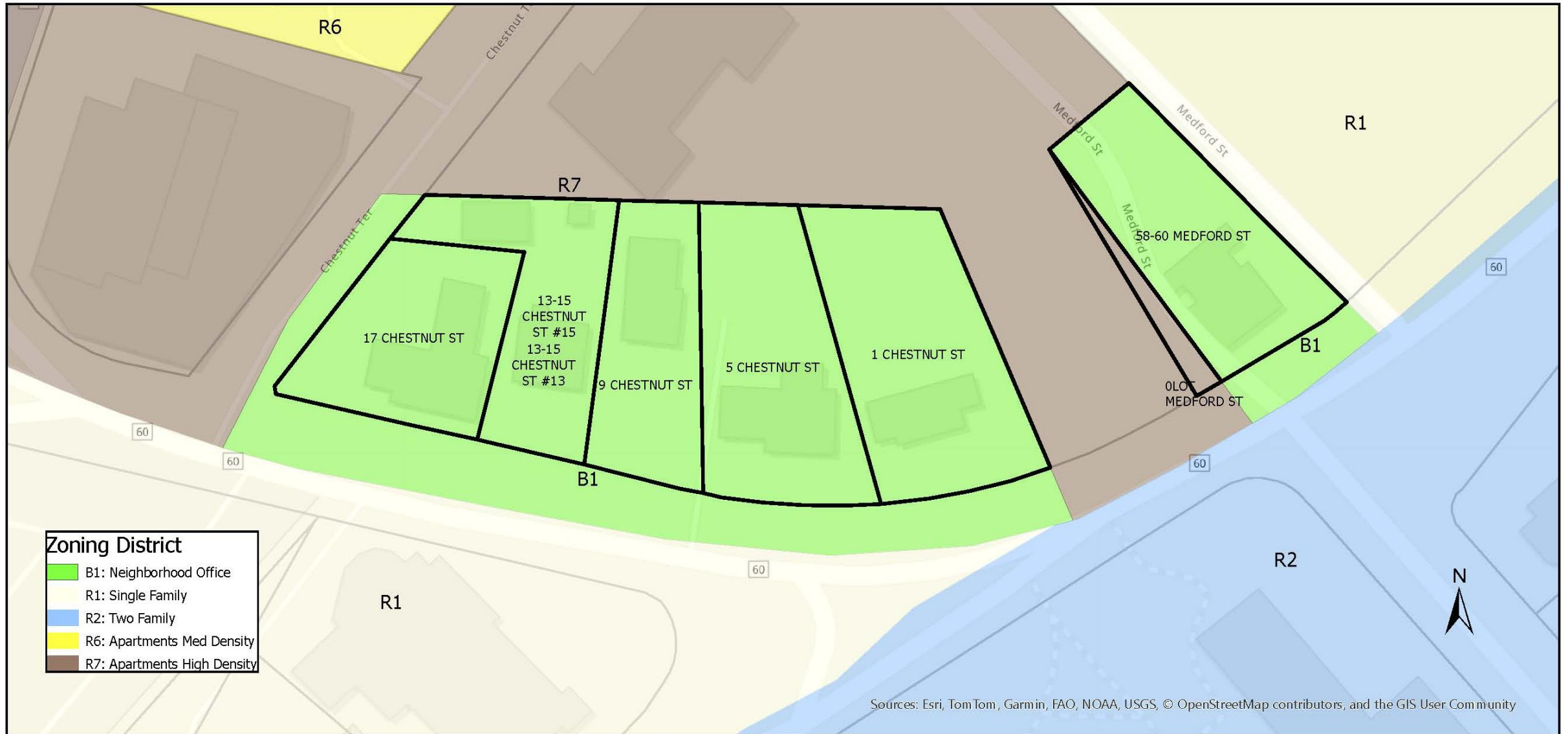


# B1 Zoned Parcels 1471-1530 Mass Ave





# B1 Zoned Parcels Medford and Chestnut St.





## Town of Arlington, Massachusetts

---

### Correspondence

#### Summary:

15 Ryder St:

- R. Mascarenhas, 2/10/2025
- D. King, 2/11/2025
- S. Mohan, 2/17/2025
- Arlington Eats, 2/18/2025
- C. Klein, 2/18/2025
- Zoning Board of Appeals, 2/18/2025
- M. Adamik, 2/19/2025
- T. Brennan, 2/19/2025
- D. Buske, 2/19/2025
- E. Campbell, 2/19/2025
- K. Depardieu, 2/19/2025
- C. Hartshorne, 2/19/2025
- S. Huber, 2/19/2025
- P. & A. Juodawlkis, 2/19/2025
- L. Marshall, 2/19/2025
- K. Moon, 2/19/2025
- M. Plamondon, 2/19/2025
- B. Sandrof, 2/19/2025
- G. Smith, 2/19/2025
- E. Sohn, 2/19/2025
- V. Stabinger, 2/19/2025
- H. Tatlock, 2/19/2025
- J. Winston, 2/19/2025
- P. Smith, 2/20/2025
- Residents of Beck & Ryder - 2/21/2025
- D. Seltzer - 2/22/2025
- C. Campbell - 2/23/2025
- L. Gitelson - 2/23/2025
- K. Lawrence - 2/23/2025
- L. Magoun - 2/23/2025
- M. Basham - 2/24/2025
- D. Brownell - 2/24/2025
- M. Contreras - 2/24/2025
- N. Green - 2/24/2025

#### Warrant Article Hearings:

- L. Wiener, 2/11/2025
- M. Marx, 2/14/2025

#### ATTACHMENTS:

Type	File Name	Description
Correspondence	15_Ryder_St_-_02102025_Mascarenhas__R.pdf	15 Ryder St - 02102025 Mascarenhas, R

Correspondence	15_Ryder_St_-_02112025_King__D.pdf	15 Ryder St - 02112025 King, D
Correspondence	15_Ryder_St_-_02172025_Mohan__S.pdf	15 Ryder St - 02172025 Mohan, S
Correspondence	15_Ryder_St_-_02182025_Arlington_Eats.pdf	15 Ryder St - 02182025 Arlington Eats
Correspondence	15_Ryder_St_-_02182025_Klein__Christian.pdf	15 Ryder St - 02182025 Klein, Christian
Correspondence	15_Ryder_St_-_02182025_Zoning_Board_of_Appeals.pdf	15 Ryder St - 02182025 Zoning Board of Appeals
Correspondence	15_Ryder_St_-_02192025_Adamik__M.pdf	15 Ryder St - 02192025 Adamik, M
Correspondence	15_Ryder_St_-_02192025_Brennan__T.pdf	15 Ryder St - 02192025 Brennan, T
Correspondence	15_Ryder_St_-_02192025_Buske__D.pdf	15 Ryder St - 02192025 Buske, D
Correspondence	15_Ryder_St_-_02192025_Campbell__E.pdf	15 Ryder St - 02192025 Campbell, E
Correspondence	15_Ryder_St_-_02192025_Depardieu__K.pdf	15 Ryder St - 02192025 Depardieu, K
Correspondence	15_Ryder_St_-_02192025_Hartshorne__C.pdf	15 Ryder St - 02192025 Hartshorne, C
Correspondence	15_Ryder_St_-_02192025_Huber__S.pdf	15 Ryder St - 02192025 Huber, S
Correspondence	15_Ryder_St_-_02192025_Juodawlakis__P_A.pdf	15 Ryder St - 02192025 Juodawlakis, P&A
Correspondence	15_Ryder_St_-_02192025_Marshall__L.pdf	15 Ryder St - 02192025 Marshall, L
Correspondence	15_Ryder_St_-_02192025_Moon__K.pdf	15 Ryder St - 02192025 Moon, K
Correspondence	15_Ryder_St_-_02192025_Morgan__W.pdf	15 Ryder St - 02192025 Morgan, W
Correspondence	15_Ryder_St_-_02192025_Plamondon__M.pdf	15 Ryder St - 02192025 Plamondon, M
Correspondence	15_Ryder_St_-_02192025_Sandrof__B.pdf	15 Ryder St - 02192025 Sandrof, B
Correspondence	15_Ryder_St_-_02192025_Smith__G.pdf	15 Ryder St - 02192025 Smith, G
Correspondence	15_Ryder_St_-_02192025_Sohn__E.pdf	15 Ryder St - 02192025 Sohn, E
Correspondence	15_Ryder_St_-_02192025_Stabinger__V.pdf	15 Ryder St - 02192025 Stabinger, V
Correspondence	15_Ryder_St_-_02192025_Tatlock__H.pdf	15 Ryder St - 02192025 Tatlock, H
Correspondence	15_Ryder_St_-_02192025_Winston__J.pdf	15 Ryder St - 02192025 Winston, J
Correspondence	15_Ryder_St_-_02202025_Smith__P.pdf	15 Ryder St - 02202025 Smith, P
Correspondence	15_Ryder_St_-_02212025_Residents_of_Beck__Ryder.pdf	15 Ryder St - 02212025 Residents of Beck & Ryder
Correspondence	15_Ryder_St_-_02222025_Seltzer__D.pdf	15 Ryder St - 02222025 Seltzer, D
Correspondence	15_Ryder_St_-_02232025_Campbell__C.pdf	15 Ryder St - 02232025 Campbell, C
Correspondence	15_Ryder_St_-_02232025_Gitelson__L.pdf	15 Ryder St - 02232025 Gitelson, L
Correspondence	15_Ryder_St_-_02232025_Lawrence__K.pdf	15 Ryder St - 02232025 Lawrence, K
Correspondence	15_Ryder_St_-_02232025_Magoun__L.pdf	15 Ryder St - 02232025 Magoun, L
Correspondence	15_Ryder_St_-_02242025_Basham__M.pdf	15 Ryder St - 02242025 Basham, M
Correspondence	15_Ryder_St_-_02242025_Brownell__D.pdf	15 Ryder St - 02242025 Brownell, D
Correspondence	15_Ryder_St_-_02242025_Contreras__M.pdf	15 Ryder St - 02242025 Contreras, M
Correspondence	15_Ryder_St_-_02242025_Green__N.pdf	15 Ryder St - 02242025 Green, N
Correspondence	Warrant_Articles_-_02112025_Wiener__L.pdf	Warrant Articles - 02112025 Wiener, L
Correspondence	Warrant_Articles_-_02142025_Marx__M.pdf	Warrant Articles - 02142025 Marx, M

**From:** Rui monteiro mascarenhas  
**Sent:** Monday, February 10, 2025 7:20 PM  
**To:** Claire Ricker; Rachel Zsembery  
**Subject:** Unwavering Support for Arlington Brewing Company

Rui Mascarenhas  
99 Montague st Arlington  
3516669384  
[Rui.m.Mascarenhas@gmail.com](mailto:Rui.m.Mascarenhas@gmail.com)

Dear Claire and Dear Rachel,

I am writing this letter to express my enthusiastic support for Arlington Beer Company (ABC), a remarkable local beer business that will hopefully be situated in the heart of Arlington, as I will regrettably not be in Arlington for the public hearing next Monday 24th February.

As a dedicated member of this community for the last five years, I have had the pleasure of witnessing the positive impact that Arlington Beer Company has had on our town, and I believe it is a cornerstone of our local economy and culture, putting Arlington in the map. Since its establishment, Arlington Beer Company has consistently demonstrated a commitment to quality, innovation, and community engagement. Their exceptional craft beers are not only a testament to their skill and creativity but also a reflection of their dedication to using locally sourced ingredients and sustainable practices. It is clear that their passion for brewing goes beyond mere business interests, as they continually strive to contribute positively to our community. Moreover, Arlington Beer Company has fostered a sense of camaraderie and togetherness among Arlington residents. Their welcoming atmosphere and community events have provided a space for locals to connect, share stories, and build lasting relationships, regardless if it is organized at the Reservoir or at local coffee shops, like the Roasted Granola. Whether it is a casual evening with friends or a larger community gathering, Arlington Beer Company has created an environment where people feel valued and included. I firmly believe that Arlington Beer Company is an invaluable asset to Arlington. Their unwavering dedication to quality, community, and sustainability sets them apart as a model local business, very well plastered by their relentless pursuit of a headquarter here in Arlington (when they could have “caved” and chose other close towns).

I wholeheartedly support their continued success and growth, and I am confident that they will continue to enrich our town for years to come.

Thank you for your time and consideration and I am of course available for any clarifications or questions you might have.

Sincerely,  
Rui

Ps Cheers to supporting our local gems! 🍻



**From:** Darrell King  
**Sent:** Tuesday, February 11, 2025 11:48 AM  
**To:** Claire Ricker  
**Subject:** ABCo new facility

To the ARLINGTON REDEVELOPMENT BOARD:

The founders of the Arlington Brewing Company have made amazing progress in establishing themselves as premier brewers and marketers. The popularity of their brand and beers has been well established by the Arlington Reservoir Beer Garden and local establishments. They got their start by contract brewing at other facilities, but to take it to the next level they need their own brewery.

As a resident of neighboring Belmont, and occasional attendee at various events in Arlington over the past several decades, I am whole-heartedly in favor of a new facility for the folks at ABCo. Craft brewing has amply demonstrated that it is a most positive contributor to a town's business community and the overall quality of life.

As I am unable to attend the upcoming hearing, I wish to offer this letter as an endorsement in favor of any required special permit to move this forward.

Darrell J. King  
former Belmont Town Meeting Member  
former Belmont Conservation Commission Member  
30 Long Avenue  
Belmont, MA  
[darrell.j.king@verizon.net](mailto:darrell.j.king@verizon.net)

February 17, 2025

Dear Claire and Rachel,

As I cannot be at the hearing on 2/24, I am writing ahead to express my wholehearted support for Arlington Brewing Company who are looking to establish a physical presence in Arlington. As a resident who has been a regular customer of Arlington Brewing Company and cares deeply about the vitality and sense of community within our town, I believe that this new establishment will bring numerous social, cultural, and economic benefits to our neighborhood.

Arlington Brewing Company has already distinguished itself as an innovative and socially minded business with a strong commitment to high quality products. Their dedication to crafting exceptional and truly unique beer and fostering a welcoming environment has earned them a reputation as a local gem. The addition of their own physical space will build on that foundation, providing a new, relaxed space for residents and visitors alike to gather, connect, and enjoy our town's welcoming spirit.

Having lived in Europe, I have seen firsthand the positive impact of similar communal spaces. These venues serve as informal, intergenerational gathering places where friendships are formed, ideas are exchanged, and community ties are strengthened. Arlington currently lacks such a space—a place that encourages organic social interactions beyond formal or structured events. I believe Arlington Brewing Company can fill that gap, fostering a sense of belonging and togetherness that will only enhance our town's already strong community spirit.

Beyond the social benefits, I am also certain that the establishment will contribute to Arlington's economic vitality. It will create jobs, support local suppliers, and attract visitors who will, in turn, support other nearby businesses. As we continue to recover from the economic challenges of recent years, such investments in our local economy are more important than ever.

I am confident that Arlington Brewing Company will approach this project with the same professionalism, responsibility, and community-mindedness that have defined their operations thus far.

Thank you for your time and consideration.

Sincerely,

Sreejit Mohan  
41 Westminster Avenue, Arlington, MA 02474



*Neighbors Serving Neighbors*

February 17, 2025

Dear Ms. Ricker,

I'm writing in support of Arlington Brewing Company's special permit application to be reviewed by the Arlington Redevelopment Board. Arlington Brewing Company has not only become a beloved institution in Arlington but they are also a great community supporter. Arlington Brewing Company has supported Arlington EATS and our work to address food insecurity by donating a portion of their beer sales at our largest annual fundraiser, Beats for EATS.

Tom and his staff are nothing but professional and have the community's interest in mind. I fully support their application to open a facility at 15 Ryder St.

Sincerely,

A handwritten signature in cursive script that reads "Andi Doane".

Andi Doane  
Executive Director  
Arlington EATS

From: **Christian Klein**

Date: Tue, Feb 18, 2025 at 7:27 PM

Subject: ARB Hearing - Docket 3838 - 15 Ryder Street

To: Claire Ricker

Dear Members of the ARB,

I am writing as a concerned private citizen regarding the proposed restaurant use at 15 Ryder Street. I was very pleased to see the outreach that the applicant's have done to the nearby residents, and I hope the changes made by the applicants serve to allay some of their concerns. Ryder Street is a private way which usually operates as if it was a dead-end street. Due to unfortunate development in the past, the private residences on the west side of the street do not appear to have property rights to the middle of the street (as would be usual for a private way), and the street is owned by the property owners on the east side of the street. The residences are very close to the street, and some of their yards are technically in the street. This makes it very difficult to shield those properties from the dramatic increases in traffic generated by The Artemis, let alone the now proposed brewery. I encourage the board to very carefully consider how it can best serve the residents who live on that street, protect them from unintended consequences, and provide meaningful, enforceable conditions for the use of the property. Those conditions should address noise, light, hours, traffic, parking, traffic flow, and similar impacts.

I am very pleased to see that a sidewalk is to be provided along Ryder Street. (The drawings appear to show a curb line, but also call for the existing asphalt to remain, so I am uncertain what is truly being proposed.) This is a major pedestrian route for school children, and the lack of a sidewalk has required students to walk in the street behind parked cars. This sidewalk will be a big improvement. Given the school route, it would be prudent to consider restricting delivery hours to times when students are least likely to be walking to and from school. I would also recommend that the board request better definition for how the sidewalk will be detailed and how the south end adjacent to the exit drive for The Artemis will be delineated; neither is very clear in the drawings.

I would also recommend that the board review how the remaining portion of the site is to be used and accessed. If that access is through the proposed parking area, that should be carefully considered.

I have met the owners of the proposed brewery at several functions around town, and I firmly believe they want to operate a business that will be a benefit to the town and be a good neighbor to the residents. I appreciate the efforts that have taken place so far, and I hope that a decision can be reached that respects the concerns of all parties.

Sincerely,

Christian Klein

54 Newport Street



**From:** Christian Klein, ZBA Chair  
**Sent:** Tuesday, February 18, 2025 6:34 PM  
**To:** Claire Ricker; Sarah Suarez  
**Cc:** ZBA; Patrick Hanlon; Michael Ciampa; Marisa Lau  
**Subject:** ARB Docket 3838 - 15 Ryder Street

Dear Members of the ARB,

I have been reviewing the proposed renovations to 15 Ryder Street which are subject to review by your Board on February 24, 2025. As you are aware, the ZBA approved a comprehensive permit for The Artemis, a residential development at 1165R Mass Ave., which abuts this property. In reviewing the site plan, it appears that a portion of the exit drive for Artemis may have been constructed on the 15 Ryder Street property. (This is at the west end of the exit drive, where a sign post and a portion of the stop line appear to cross the property line.) If that is the case, and the Artemis exit drive needs to be adjusted, I would request that the required changes be made in conformance with the approved decision for 1165R Mass. Ave.

If you have any questions, please let me know.

Sincerely,

Christian Klein  
Chair, Arlington Zoning Board of Appeals

**From:** Mary Adamik  
**Sent:** Wednesday, February 19, 2025 1:54 PM  
**To:** Claire Ricker  
**Subject:** I support Arlington Brewing Company

I am writing to voice a strong yes for supporting the Arlington Brewing Company in being approved for all permitting and licensing needed to open their brick and mortar business in Arlington. The new facility will be a wonderful gathering place for our community.

thank you in advance for supporting a small local business

Mary Adamik  
70 Lennon Road  
Arlington MA 02474

**From:** Taylor Brennan  
**Sent:** Wednesday, February 19, 2025 9:32 AM  
**To:** Claire Ricker  
**Subject:** Letter of Support for Arlington Brewing Company

To the Director of Planning and Community Development,

I am writing today to voice my support for the plans for Arlington Brewing Company's brick and mortar location.

Even without a home location, ABC has created a sense of community and gathering in times when it is most needed. Every event and town function that Tom and Peter have participated in over the years has been done with a true love for Arlington and of course their delicious beer. They are true collaborators and partners in town, whether hosting pop-ups at local small businesses or running fundraisers with the sale of their beers, they care about this town and it makes me proud to be able to support their business.

It is a joy and a privilege to have members of our community who truly thrive on gathering the town together. I am never happier to be a member of this community than I am when I am surrounded by my friends and neighbors at events that ABC has been a part of.

I know that with a permanent location, Tom and Peter will only be able to provide more times of gathering, more times of charity, and more joy for the town for years to come.

Thank you for your time, and I hope that you are able to fully endorse the plans for Arlington Beer Company, and that we'll all be enjoying some fun, music, and a great beer in the upcoming future!

Thank you,

Taylor Brennan  
25 Crescent Hill Ave, Arlington MA

**From:** Dana Buske  
**Sent:** Wednesday, February 19, 2025 11:01 AM  
**To:** Claire Ricker  
**Subject:** support of Arlington Brewing Company

Ms. Ricker,

I am writing in support of the propoal by the Arlington Brewing Company to build a brewery on Ryder St, Arlington, MA. I think that this business will be a great addition to the community.

Thanks,  
Dana Buske  
12 Martin St  
Arlington, MA 02474



**From:** Elaine Campbell  
**Sent:** Wednesday, February 19, 2025 7:49 AM  
**To:** Claire Ricker  
**Subject:** I support the Arlington Brewing Company

Please join in with me and others in supporting the Arlington Brewing Company. I believe they would be a great asset to our town.

Elaine Campbell  
12 O

**From:** Katherine Depardieu  
**Sent:** Wednesday, February 19, 2025 11:21 AM  
**To:** Claire Ricker  
**Subject:** Support for Proposed Arlington Brewing Company

Hi there,

I'm writing in support of the proposed Arlington Brewing Company location at 15 Ryder St in Arlington!

I cannot adequately express my excitement at having this business become part of the Heights community, especially given that it's estimated to be a ~5 minute walk from where we live on Blossom street. One of the reasons we love Arlington is the walkability, and we're thrilled at the prospect of another great spot within walking distance.

Although Arlington Heights has a few wonderful local businesses and gathering spots (e.g. the Heights Pub, Roasted Granola), we are SO excited at the prospect of a large community gathering space where you can drop by for just a drink or an entire meal and meet up casually with friends w/o needing a reservation. We often find ourselves heading to Arlington Center to businesses like Tatte and Kickstand because there's definitely a limited selection of late afternoon/ early evening gathering spots locally. We are especially excited about the potential outdoor space/ beer garden!

The proposed plan for the brewery is a huge improvement over the current building/ facade, and coupled with the proposed improvements to the bike path at Ryder street/ Burns area, this will be a great improvement for that area overall! (And those bike path safety improvements will be even more relevant once you have folks headed to the bike path after a couple beers.)

Overall, VERY excited for this addition to the community!

Best,

Kat Depardieu & Eric Garofano

--

5 Blossom St  
Arlington MA 02474

From: **Charles K. Hartshorne**

Date: Wed, Feb 19, 2025 at 8:36 AM

Subject: We support Arlington Brewing Company and hope the ARB will approve their new space

To: Claire Ricker

To the ARB:

We have known Tom Allen for many years, and believe the facility he is proposing for Arlington Brewing Company will be a wonderful asset for the town.

It will bring a much-needed family-friendly gathering place for residents and visitors and we know Tom will be very mindful of the concerns of neighbors.

Thank you,

--

Charles Hartshorne and Laurie Ellis  
11 Marrigan St  
Arlington, MA

**From:** Sarah Huber  
**Sent:** Wednesday, February 19, 2025 1:34 PM  
**To:** Claire Ricker  
**Subject:** Support for Arlington Brewing Company

Hello,

I would like to voice my support for the Arlington Brewing Company to be able to open a Brewery at 15 Ryder St. The Arlington Brewing Company has shown time and again that it can bring people together throughout the community to relax and hang out together. Arlington Brewing Company builds both interpersonal bonds and town spirit. Its location along the bike path would also promote safe bike travel to and from the venue and bring in people from other communities to spend their money in Arlington. I sincerely hope the ARB approves their plan.

Thank you!  
Sarah Huber  
24 Amherst St, Arlington, MA 02474



19 February 2025

Ms. Claire Ricker  
Director of Planning and Community Development  
Town of Arlington  
730 Mass Ave., Town Hall Annex  
Arlington, MA 02476

Dear Ms. Ricker,

*"You get one chance," ...*

...were the exact words I said to the Arlington Brewing Company (ABCo) team member as they handed me the glass of Spy-P-A beer I had just purchased at their pop-up beer garden that they had setup at Arlington Town Day in September 2023. While my wife Amy and I had heard very positive things about the pop-up events that ABCo had hosted at the Arlington Res over the summer, this was our first opportunity to try their product. I'll be the first to admit that there are plenty of tasty microbrews to choose from today, but there are also some less tasty ones that you try once and then try to avoid. I was interested to see where ABCo fit into this spectrum.

The Spy-P-A was delicious - definitely one of the best New England IPAs that I have had. Amy, not a big fan of IPAs, had a glass of their Jedermann Helles Lager and enjoyed it as well. It was great to have a couple of beers at Town Day after the bicycle ride that we had just finished on the Minuteman Bike Trail. The tables around us were filled with families enjoying local beer, food from local restaurants, talking, laughing, and being together. It was a great community event.

After finishing our beers, we purchased an ABCo t-shirt to show our support. We met Tom Allen, one of the co-founders, and talked to him about his near-term plans and long-term vision for ABCo. He patiently listened to our suggestions for potential permanent locations ("definitely close to the bike path") and events that we could help organize ("we could organize runs or bicycle rides that begin and end at ABCo"). Amy and I both came away very excited about this new business in the town where we have now lived for 26 years. In the year and a half since our first meeting, we have enjoyed engaging with Tom and the ABCo team around Arlington at events like summer evenings at the Res, Patriot's Day at Town Hall, and at pop-up gatherings at the Roasted Granola Café and the Mill Street Café.

We are providing this letter to offer our strong support for their application to establish a brewery, restaurant, and taproom at 15 Ryder Street in Arlington. Once upon a time, almost every town had its own brewery, which was as central to the community as the shops, churches, schools, and town hall. It is our belief that breweries like ABCo, with the thoughtful ownership of people like Tom Allen, Peter Caradonna, and Rob Chalmers can reinvigorate this facet of community here in Arlington.

ABCo had their one chance with us, and they succeeded. Now, we want to help them continue to succeed.

Sincerely,

Paul and Amy Juodawlkis  
26 Bailey Road  
Arlington, MA 02476  
781-777-8602  
[teamj923@verizon.net](mailto:teamj923@verizon.net)

From: Lisa Marshall  
Sent: Wednesday, February 19, 2025 8:07 AM  
To: Claire Ricker  
Subject: Arlington Brewing Company

Dear Claire,

I can't make it to next week's ARB meeting but I just wanted to add my name to the long list of people who are very excited about Arlington Brewing Company's plans for an establishment in our neighborhood!

Thanks,

Lisa Marshall  
8 Park Ave Ext.

**From:** Katherine Moon

**Sent:** Wednesday, February 19, 2025 12:48 PM

**To:** Claire Ricker

**Subject:** support for arlington brewing company at 15 ryder st

Hi Claire,

I am writing to express my strong support of the Arlington Brewing Company's proposal to build their new home at 15 Ryder St in Arlington, in advance of their special permit hearing in front of the Arlington Redevelopment Board.

I am a resident of Cambridge but regularly spend time in Arlington with my family and friends who live in Arlington. In full disclosure, I am friends with the owner, Tom Allen, and have known him for almost 15 years. He is a wonderful human being and I have every expectation, based on both his personal characteristics and professional background, that the expansion of the Arlington Brewery business will be a success. Their beer is delicious, sells well in the local market, and I look forward to spending time at their new location as soon as they open.

Thank you,

Kat Moon

--

Katherine Moon

(617) 335-5445

[katherine.a.moon@gmail.com](mailto:katherine.a.moon@gmail.com)

**From:** Will Morgan

**Sent:** Wednesday, February 19, 2025 8:44 PM

**To:** Claire Ricker

**Subject:** Letter of support for Arlington Brewing Company's special permit

Ms. Ricker,

I am writing in strong support of Arlington Brewing Company's (ABC) application for a special permit from the Arlington Redevelopment Board. Having a dedicated brewery space in Arlington would be a huge boost for the appeal and livability of the town. My family and I are often looking for a "third place" to spend time in at night in Arlington, and a brewery space would be the perfect fit.

Many thanks,

Will Morgan

189 Spring Street, Arlington



February 19, 2025

Dear Members of the Arlington Redevelopment Board,

I am writing to express my full support for the Arlington Brewing Company's proposal to open a brewery at 15 Ryder Street. As a longtime resident of Arlington, Massachusetts, I have seen firsthand how the brewery has become an integral part of our community and the positive impact it has had on local residents.

The team at Arlington Brewing Company has been nothing short of exceptional, not only producing high-quality craft beer but also fostering a welcoming and inclusive environment for all who visit. Their commitment to the community and the care they put into their business make them an ideal neighbor.

Additionally, the location at 15 Ryder Street is easily accessible by walking and biking, making it a convenient spot for residents to enjoy. The proximity to the Minuteman Bike Path is particularly beneficial, encouraging active, sustainable transportation while allowing visitors to easily access the brewery from surrounding neighborhoods.

I firmly believe that Arlington Brewing Company's continued success will benefit not only the local economy but also contribute to the vibrant and growing culture that makes Arlington such a wonderful place to live. I encourage the Board to approve this project and help ensure that our town remains a hub for local businesses and community engagement.

Thank you for your time and consideration.

Sincerely,

Michael Plamondon  
64 George Street

From: ben sandrof  
Sent: Wednesday, February 19, 2025 3:20 PM  
To: Claire Ricker  
Subject: Arlington Beer Company

Hello Claire, my name is Ben Sandrof and I live at 14 Nourse Street here in Arlington. I just wanted to send a note voicing my support for Arlington Beer Company and hope their permits get granted! Thanks very much.

Ben Sandrof  
781-258-4374

From: Geoffrey Smith  
Sent: Wednesday, February 19, 2025 8:34 AM  
To: Claire Ricker  
Subject: Docket 3838 2/24/25

Dear Director Ricker,

As an Arlington resident, I am writing to express my support for the 15 Ryder St Special Permit Application (Docket #3838) by Arlington Beer Co. The company has already been a great presence at Arlington community events, including the summer concert series at the reservoir and town day, and I am excited to see the brewery and restaurant come together at 15 Ryder St. The proximity to the bike path is ideal and I expect many will use that as a safe way to get to and from the restaurant (well, maybe not in February). The location right off Mass Ave in a largely industrial area next to the town recycling lot is well positioned from a traffic perspective with minimal impact on the surrounding Arlington Heights neighborhoods. I hope the board will work with ABC to build a welcoming, sustainable gathering place for Arlington.

Sincerely,

Geoffrey Smith  
11 Randolph St

**From:** Elizabeth Kelley Sohn  
**Sent:** Wednesday, February 19, 2025 8:46 AM  
**To:** Claire Ricker  
**Subject:** Letter of support for Arlington Brewing Company

Hi there,

I'm writing to express my support for the new brewery on Ryder St. It will be a wonderful addition to our community! As someone who lives nearby, I love that it will bring new life to that corridor.

Best,  
Liz Sohn  
23 Chatham St, Arlington, MA 02474



**From:** Veronika Stabinger  
**Sent:** Wednesday, February 19, 2025 7:46 AM  
**To:** Claire Ricker  
**Subject:** Support for Arlington Brewery – Special Permit Hearing

Dear Claire Cricker,

I just wanted to take a moment to express my enthusiastic support for the Arlington Brewery's special permit application.

Tom and Peter have already proven that there's a real demand for a community-oriented brewery in town—just look at the incredible success of the temporary beer garden at the Jason Russell House! The atmosphere there was exceptional—welcoming, inclusive, and spanning all ages. It became a true gathering place for Arlington, where neighbors could connect, families could enjoy a relaxed afternoon, and friends could share a pint of exceptionally well-crafted beer.

It's clear that their vision aligns beautifully with what makes Arlington such a great place to live. A permanent brewery would not only support local business and craftsmanship but also give the town a much-needed community space to bring people together year-round.

I hope the ARB sees the value in this and grants their permit—I know many of us would love to see this happen!

Best,  
Veronika Stabinger

---

**Veronika Stabinger**  
Founder | Managing Partner  
**HumanCentered Solutions LLC**

Innovation & Change Management through  
Top-Down and Bottom-Up Facilitation,  
with a Focus on Food Systems and Beyond

US: +1 781 539 4688  
DE: +49 151 533 43736  
Email: [veronika@humancentered.solutions](mailto:veronika@humancentered.solutions)  
[www.humancentered.solutions](http://www.humancentered.solutions)  
[www.hellotrellis.com](http://www.hellotrellis.com)

*You can focus on feeding appetites- or nourishing futures.  
Both work, but nourishing futures will take you further.*

**From:** hugh tatlock  
**Sent:** Wednesday, February 19, 2025 7:44 PM  
**To:** Claire Ricker  
**Subject:** I support arlington brewing company

Hi,

I am writing voice my support for the arlington brewing company. Small local breweries were once a integral part of every community and I believe they should be again. Please support these kind folks

Thank you  
Hugh Tatlock  
Arlington resident

From: Jen Winston  
Sent: Wednesday, February 19, 2025 8:24 AM  
To: Claire Ricker  
Subject: Arlington Brewing Company

Good morning, I'm writing to you in regards to the Arlington Brewing Company special permit. Unfortunately I'm not able to attend the hearing next Monday, but wanted to add my support as a neighbor of the new brewery. I live quite close to the site of the proposed tap room and am very excited for the possibility of a community gathering spot within walking distance to my home. All of the ABC popup events that I have attended have been extremely warm, welcoming, and family friendly. The community in Arlington, the Heights especially, has benefited from the hospitality and closeness that are evident at these events and I think the community will benefit even more from this tap room, as well as being a destination for others outside the community to come and spend money in our town. And, the beer is excellent!!

Thank you,

Jennifer Winston  
1Watermill PL. unit 208  
Arlington, MA 02476

**Letter of support for Arlington Brewing Company**

Peter Smith  
173A Franklin Street  
Arlington  
MA 02474  
20<sup>th</sup> February 2025

To whom it may concern,

I am writing to provide my enthusiastic support for Arlington Brewing Company's goal to open a facility in Arlington.

For our community, this would be a fantastic addition to Arlington providing a differentiated entertainment option for residents that does not currently exist. Amongst my friends in the area there is strong desire for such an establishment to drink locally brewed beer and good food. The plans are clearly family oriented which fits with the growing community in Arlington; in addition, the facility will also serve long standing residents with more choices for food/drink.

Personally, I've known Thomas Allen for many years. He has chosen to make Arlington his home for his family and is heavily invested in the town and community and wants to make Arlington a better place to live. He is honest, reliable, hard working and has the best intentions for making Arlington a better place to live. Part of his goal for the facility is to provide a location that brings people together and grows community. I know he also recognizes the challenges of running a facility that serves alcohol and I know him to be a very responsible person that can be trusted to run his facility in a very responsible manner.

In short, Arlington will be a better place having Arlington Brewing Company and Tom's team running a new entertainment option.

Sincerely,

Peter Smith



To the Members of the Arlington Redevelopment Board:

The purpose of this summary and supporting documentation is to express the concerns of the Ryder and Beck Street residents regarding the proposed brewery and restaurant at 15 Ryder Street (Arlington Brewing Company, hereafter “ABC”). Thank you for the opportunity to engage your Board.

As a group of homeowners who have lived in this small, mixed-use neighborhood, we want to provide contextual insights to the members of the Arlington Redevelopment Board into the lived experience of the abutters to the proposed ABC. Our neighborhood consists of two private ways, Ryder Street and Beck Street, and the private driveway of the Artemis complex- all surrounding the proposed ABC at 15 Ryder Street. First, we highlight that this small, residential neighborhood has existing safety issues that are a constant source of concern for the well-being of our own families, properties, and our community members who use the private ways of Ryder/Beck to access the Bike Path and the Ed Burns Ice Arena complex. Second, we highlight that although living in a mixed-use neighborhood means living with commercial schedules and vehicles, our households have come to rely on the relative peace and quiet we experience after 5pm on weekdays and most all of the weekends.

As currently proposed, the ABC at the 15 Ryder Street site presents detriments to the public safety of all community users of Ryder/Beck, while also introducing new, significant quality of life and environmental burdens to the families who reside on the three private ways surrounding the proposed ABC site.

**\*\*Please refer to the attached presentation for more information and visuals\*\***

## Our Main Concerns

### Safety Issues

Ryder Street is an unsafe street. Full stop. There are *scarce commercial operations out of 15 Ryder historically and at present* so anything that a new commercial venture presents at that site is added on top of our street’s unsafe baseline. As residents, we are very concerned about the implications of adding alcohol to the unsafe conditions of this street. We have stated this as our primary concern to the applicant in past meetings and are worried that somehow our voice has not been understood or heard. And we are alarmed that the applicant has not once addressed the implications of serving alcohol to patrons in the application questions about safety and public health.

- We do not see the issues of **traffic and pedestrian safety** adequately addressed in the proposal to-date.

We anticipate that increased traffic from ABC patrons and commercial delivery vehicles poses increased risks to pedestrians, especially children and recreational users of Ryder/Beck, cyclists, and other vehicle drivers because Ryder/Beck are unregulated, often chaotic, mixed-use streets. **Alcohol use and alcohol-impairment of ABC patrons-** pedestrians, cyclists, and vehicle drivers- will exacerbate safety risks.

### Quality of Life

**Noise.** Although the mixed-use of our neighborhood means large construction vehicles with trailers and DPW trucks are navigating our narrow private ways, these disruptive noises and traffic burdens are predictable: these commercial and Town entities typically have a morning commute out from 7am until about 9am and an evening commute in from 3:30pm to 5:00pm. There is sparse commercial use of the private ways on the weekends. **The proposed ABC operating hours would change this dramatically and take over the relative peace and quiet that our neighborhood enjoys** from 5pm to 11pm with patrons, and well after midnight with employees closing up shop. Our quiet weekend hours would be filled with the ABC patrons, traffic, car doors slamming, outdoor/indoor music, and exhaust fans operating.

**Odor/Air Quality.** Our mixed-use neighborhood does not currently include restaurant operations or brewery operations, both of which have emissions that affect air quality for our mostly residential neighborhood, many who live under 150ft from the site of 15 Ryder.

**Light Pollution.** We have learned recently how redevelopment introduces light pollution with the addition of the 2-story open garage and new street lights of the Artemis complex: this disruptive lighting affects normal use of neighboring residential properties 200ft away. Given the site is surrounded by residents on 3-sides, it is important to use best practices to minimize light pollution burden on abutters.

- We do not see the full scale of **noise disturbances** adequately addressed in the proposal to-date.
- We do not see the full scale of **odor and air quality emissions** adequately addressed in the proposal to-date.
- We do not see the full scale of **light pollution** adequately addressed in the proposal to-date.

### Environmental and Health Concerns

**Waste Management.** Proper, timely disposal of spent grains and other waste products from the brewery is crucial to prevent attracting vermin and pests, which can pose health risks to our residents. Additionally, improper waste management can lead to unpleasant odors and potential contamination of local water sources, impacting both the environment and public health.

**Smoking.** Designated smoking areas are necessary to prevent littering of cigarette butts, which can cause harm in dry brush areas and pollute the environment. The litter, which inevitably blows all over the street, is also unwelcome by private property owners. Moreover, second-hand smoke exposure poses significant health risks to nearby residents, particularly children and individuals with respiratory conditions. Implementing well-defined smoking zones with proper disposal facilities can mitigate these concerns.

- We do not see the full scale of **waste management** adequately addressed in the proposal to-date.
- We do not see a **patron/staff smoking area plan** addressed in the proposal to-date.

## Specific Requests and Recommendations to the Board

**Traffic Management:** Arlington must start to act proactively to avoid near-misses and tragic accidents. We need to ensure, *especially in the context of introducing an alcohol-centric business*, the enhanced safety for pedestrians and cyclists who use Ryder/Beck. ABC must also have a plan to minimize anticipated traffic congestion.

- ☐ We request that a baseline traffic/usage study be conducted on Ryder/Beck to understand the potential impacts of the proposed commercial and patron use of the ABC site.
- ☐ We request ARB's help in understanding the (a) proposed number of Parking Spaces, (b) cited exemptions, (c) assumptions underlying these plans, and (d) whether these are reasonable for a site in a small multi-use neighborhood consisting of 3 private ways.
- ☐ We request the Town and applicant/developer to correct and implement effective traffic calming measures on Ryder/Beck.
- ☐ We request that the Middle School Commuter times be No-Delivery time zones.
- ☐ We request that the ARB require that ABC hire an experienced Freight Broker to oversee adherence to appropriate dock hour/deliveries, (a recommended practice of breweries), and especially protect Middle School Commuter times.
- ☐ We request the ARB sees that the applicant ensure clear wayfinding for drivers (both patrons and commercial)
- ☐ We request the ARB sees that the applicant designate rideshare pick-up/drop-off zones.
- ☐ We request the ARB scrutinize how the applicant will manage adherence to Idling Laws of its commercial and patron vehicle users ([MA General Laws Part I, Title XIV, Chapter 90, Section 16A](#))

**Operating Hours:** It is our understanding [Arlington Zoning By-laws Section 5.6.4-D](#) states that “tap room hours of operation open to the public shall not represent disturbance to adjacent residential uses...” and that this more typically suites commercial districts; the site of 15 Ryder is surrounded by residences on 3-sides, with over 200 apartments, condos, and single family homes making it heavily residential. The introduction of the proposed use of this space represents a significant departure from the night-time and weekend relative calm and quiet residents get.

- ☐ We request that the ARB limit the Tap Room hours to 5PM in order to minimize disturbances to the residents surrounding the site and align with ordinances.

**Noise and Odor Control:** It is our understanding that [MA 310 CMR 7.10](#) “prohibits sources of sound from increasing broadband sound levels by more than 10 dB(A) above ambient background noise... measured at property lines and nearest inhabited residences”. Despite what the application suggests, there is no “back side” to the proposed building, residents surround the building on 3 sides, and patron access is on the fourth side. The overwhelming majority of the street is comprised of the ~200 residential units of the Artemis, Ryder Street, and Beck Street who have the right to a reasonably quiet and clean environment.

- ☐ We request that a baseline noise study be conducted

- ☐ We request that the ARB further scrutinize the degree and quality of soundproofing such that the applicant provides more specific details: and that these plans represent best practices for a *residential* area
- ☐ We request that the ARB further scrutinize the intended odor mitigation strategies for brewery ventilation operations, restaurant ventilation operations, and waste management across all seasons: and that these plans represent best practices for a *residential* area.
- ☐ We request that the ARB examine the applicant's plan around minimizing abutter and public burden around patron and staff smoking.

**Community Engagement:** Increasing population density and, in turn, moving toward redevelopment of spaces to meet public interest is here to stay in Arlington. Our small neighborhood did not ask to be a multiple test-case of this change, but here we are, and we have invested hundreds of hours of our own time since January 2021 persistently advocating for the safety of our community and asking for reasonable protections for our quality of life and environmental health. We seek the Town to meet us in fostering a collaborative approach and ensure residents' concerns are addressed. Our neighborhood today will be your neighborhood in the near future, and we hope the best for your neighborhood's safety, quality of life, and environmental health.

- ☐ We request ARB and other pertinent Town officials take regular consultations with residents regarding the proposed ABC.
- ☐ We request transparent, timely communication of ABC plans as they develop from construction to fully operational that incorporate community feedback.

Although as neighborhood residents and Arlington community members we recognize the potential *social* and *commercial* benefits of a community brewery, we know the current proposal raises significant concerns about physical safety for abutters, intended ABC patrons, and the broader community who uses Ryder/Beck. Given the novel and unprecedented proposed use of the 15 Ryder Street site in a heavily residential neighborhood, we are very concerned about the detriments to the quality of life for the residential abutters who surround the site on three sides. We intend this documentation to be heard and seen as a call to action: We request that the Town and applicant not just consider these concerns but work toward an effective set of solutions that balance development with the well-being of all who reside in and use the neighborhood.

We relay these materials with our best intentions for public safety and well-being.

Respectfully submitted,

Residents of Ryder Street and Beck Street

# A Hybrid Neighborhood

How might we make Ryder a suitable home for the Arlington Brewing Company and serve as a model for how to achieve increased density that accounts for safety and quality of life.

Arlington Redevelopment Board – Residents of Ryder Feedback  
February 2025



## We Do Get It...A Brewery Is Exciting...On Paper

Many Arlington residents miss the vibrancy of communities like Somerville and Cambridge



The building at 15 Ryder is right-sized for a local business to scale



The location is adjacent to several public amenities





## However The Lived Reality On Ryder Is A Much Different Picture

Once a system in balance, we accepted chaotic weekdays for quiet peaceful nights and weekends.



Transformed overnight, our neighborhood grew ~500%, from about 28 units to 152 in the last 2-years.



We see daily safety incidences and inter-personal hostility with no clear line of accountability.



Resident of Artemis trying to go wrong-way down one-way driveway and threatening manager of property by revving engine impatiently.



## We Are Not Against ABC...We Are For Safety & Quality of Life

There are long-standing safety and congestion problems, that remain unaddressed



A typical weekday scene as pedestrians seek to access the Bike Path

Problems that would be exacerbated by introduction of more distracted, disoriented, and/or impaired drivers.



16161232

For us, this forum is the only place where we can come together to avoid a situation like this







## Window of Opportunity

The challenges we face have all been solved somewhere else before.

Our challenge is organizing ourselves. In the past individuals have given up because of the complexities of coordinating improvements on a private street.

There is too much at stake to not find a way to come together and make this work.

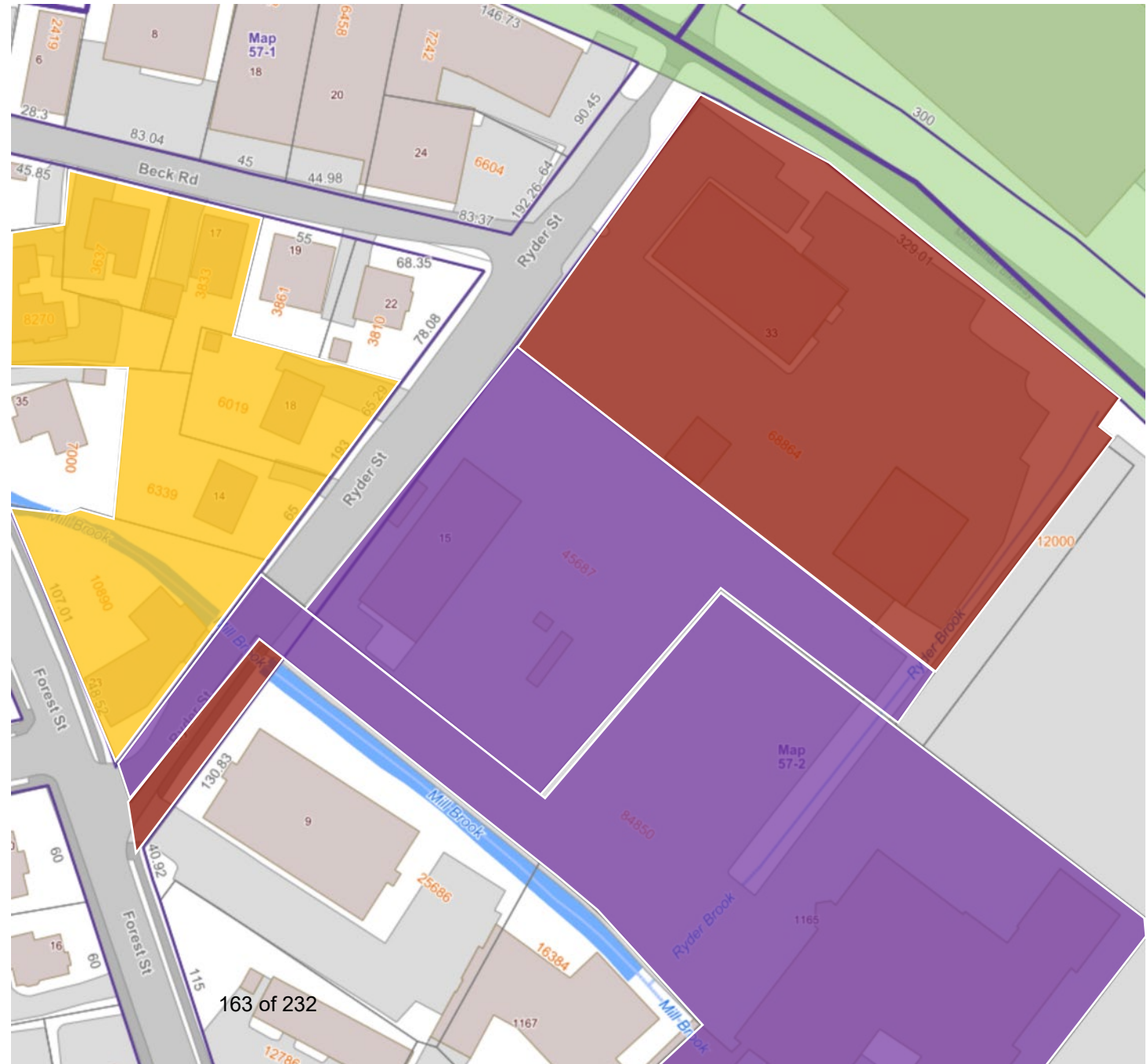
**It is imperative that we find workable solutions BEFORE ABC opens.**

**We have been supplied empty promises before:  
they will not keep our community safe.**

## Holding Companies assoc. with Julia Mirak Kew

# Town of Arlington

## Residential Abutters





# SAFETY





# Let's Start With Safety

Our neighborhood is often described by outsiders as a 'Dumpster Fire', which feels appropriate:

## Merriam-Webster

"An utterly calamitous or mismanaged situation or occurrence: Disaster"

## Urban Dictionary

1. A complete disaster.
2. Something very difficult that nobody wants to deal with.

**Why would we want to add fuel to this fire before resolving the current issues?**





## Applicant's Depiction of Ryder Street

The views of 15 Ryder Street and its surrounding supplied in the application have a central theme: they lack humans.

Unless you live here, it's impossible to understand the frequency and severity of close calls with pedestrians and cyclists, and inter-personal friction that exist here.

- Drivers flipping you off when you try to point-out they're violating one-way signs.
- Individuals following us to our home accosting us for taking photos of them parking illegally in a fire lane or a neighbor's driveway.
- DPW dismissing our multiple concerns over their speeding drivers- once when they almost hit your child in a stroller. (That employee still drives- irresponsibly- for the Town).
- Watching as a friend's 3-year old comes within 2ft of getting hit by a car because of a distracted driver.

The following slides are a first attempt to illustrate to the Board Members the severity and magnitude of these daily issues, so that these concerns are given the legitimacy they deserve.



The applicant's depictions of Ryder consistently lack the most important element of our community: the humans who use Ryder on foot and bike.



# Here Are The Users of Ryder Street

## Commercial/ Industrial

Weekday mornings and afternoons are busy with crews gearing up and closing out the day.



## Middle Schoolers & Preschoolers

Our town's 11-14 year-olds navigate this industrial traffic twice per day on way to Ottoson. EECs seek playgrounds.



## Families (Possibly Your Family?)

Weekday evenings and weekends have been enjoyed by families and joggers accessing bike path.





# Chronic Safety Issues

## Speeding

The speed table agreed to in the 1165 ZBA approval has been ineffective: its design & installation flawed with insufficient approach angle or height.



## Driver Frustration & Impatience

The 3 one-lane private ways intersecting, 2 supporting two-way traffic, fuels driver frustration encouraging aggressive behaviors.



## Lack Enforcement

As a private way, there are no effective or sustainable ways to enforce predictable, safe driving behaviors.





# Chronic Safety Issues

## No Wayfinding

Lack of signage or lane markers, coupled with haphazard parking distracts drivers, increasing chances of a pedestrian not being seen.



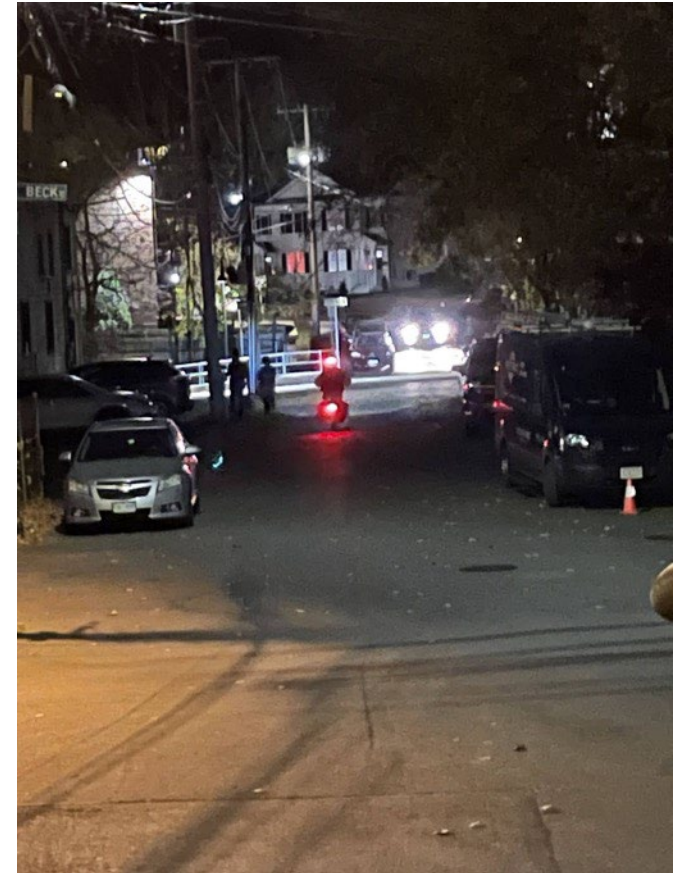
## Blocked Fire Lanes

Today drivers routinely ignore No Parking signs (even knocking them over) prioritizing their convenience over respecting fire lane.



## Poor Visibility

With increased vehicle speed, and non-standard layout, it can be very hard for cars to anticipate who's coming.





# Chronic Safety Issues

## Arlington DPW Blindspot at 33 Ryder

The large blocks and recently relocated donation bins block sightlines between pedestrians and departing trucks. (Barbed wire fence violates town laws too)



## D. Muzzioli Associates

Although crews are *mostly* considerate, the constant use of the top of Ryder at the Bike Path obstructs pedestrians- and oils create slippery downslope for walkers, especially in winter.



## Lack Accessible Ramp

Witnessed several people, including elderly woman, slip due to the lack of an accessible ramp to sidewalk.





## Adding Alcohol & More Drivers Adds Risk

## Impaired Drivers

A brewery serves beer.

Any amount of alcohol consumption reduces the margin of safety.

Safety is already razor thin on our street.

Why this hasn't been included in the application is alarming to us residents.

“Every day, about 37 people in the United States die in drunk-driving crashes — that's one person every 39 minutes.”

“In 2022, alcohol was a factor in nearly half (48%) of crashes resulting in pedestrian deaths, involving either a driver or pedestrian with a BAC of 0.08 or higher.”

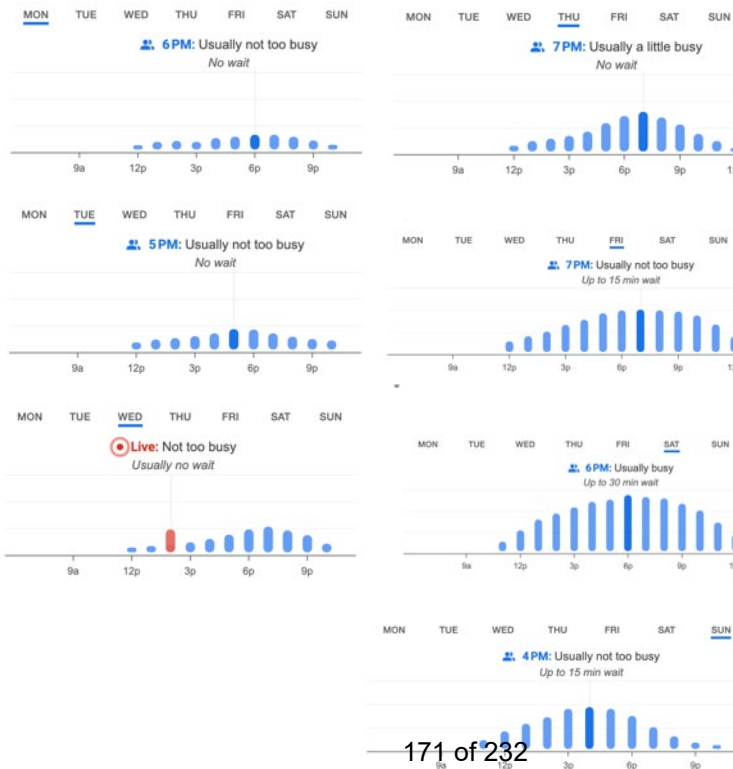
“Although the body of literature on alcohol-linked bicycle injuries is limited, alcohol consumption among cyclists has been shown to increase the risk of an injury or fatal crash [5–7]”

“A staggering number of traffic fatalities have been linked to alcohol; of the 858,741 traffic fatalities from 1982–2001 in the United States (US), 43% were associated with alcohol use”

## Peak Volume

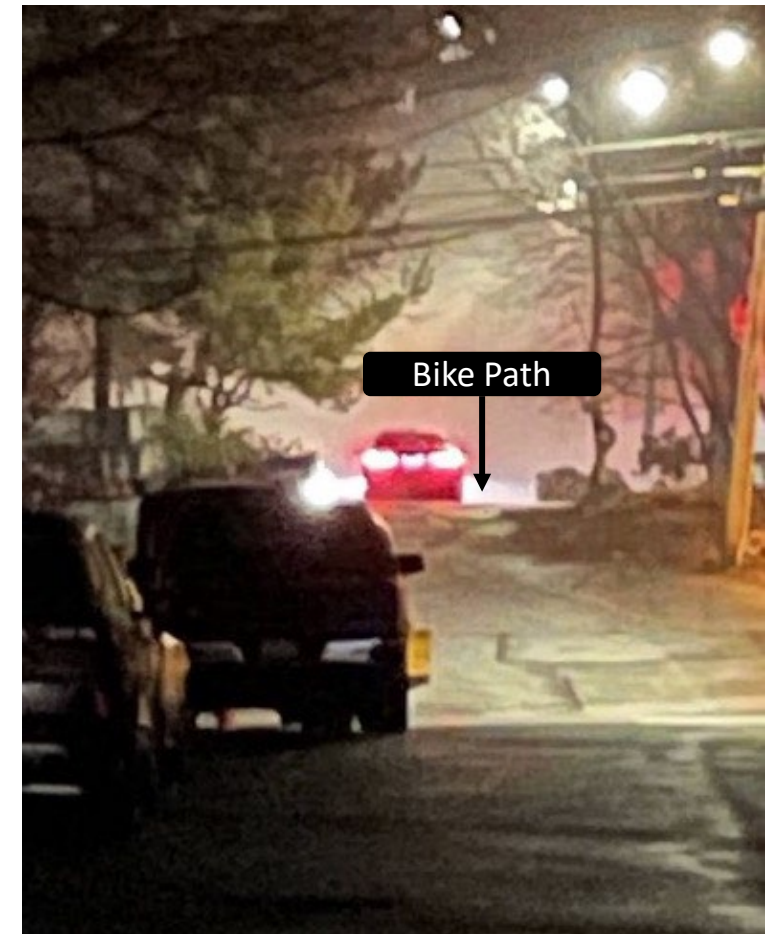
Currently, industrial and recreational hours do not overlap. Adding traffic during recreating hours increases risk for vulnerable users.

## The Great American Beer Hall



## Driver Mix

Drivers unfamiliar with area (out of towners, rideshare, delivery) are more likely to violate rules for convenience.





# QUALITY OF LIFE





# Residential Quality of Life

Although 15 Ryder is in an industrial zone, it directly abuts residential neighborhoods on three sides, with children's bedrooms as close as 80 feet from the front door of the proposed establishment.

Understanding how ABC's proposed uses could disturb its residential neighbors is critical.

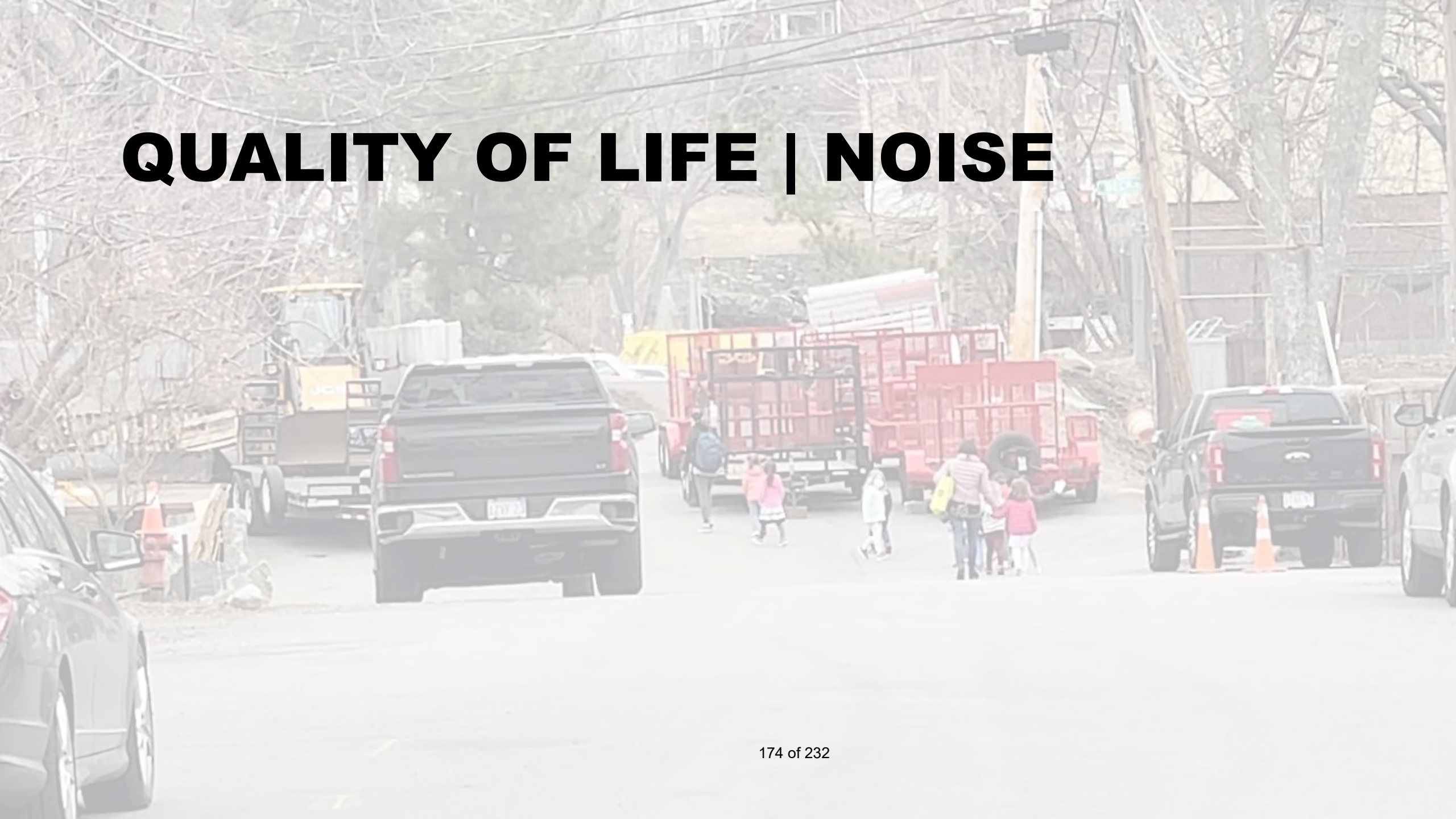
## Quality of Life Considerations for the Members of the Board:

- ☐ Noise
- ☐ Parking
- ☐ Odors
- ☐ Smoking
- ☐ Pests





# QUALITY OF LIFE | NOISE



# Noise Laws – A Basis for Understanding Resident Rights

We as citizens with no legal training are doing our best to identify and interpret applicable laws in all domains and these are the reference points for noise we’ve come across.

As Arlington’s Board of Health points out, the town lacks the resources to enforce healthy standards with regards to noise limits, again highlighting a discrepancy between our aspiration as a community to achieve density and our ability to manage that density thoughtfully.

If this project is to be approved, we must agree on absolute thresholds based on law (MA 310 CMR 7.10) or research (Cambridge 8.16) and find a way to monitor and enforce those limits. Subjective complaints that will frustrate everyone.

As a starting point it would make sense to conduct longitudinal readings to establish a baseline to work from.

**Just because the Town doesn’t have a plan doesn’t give the Town license to saddle residents with the burden.**

## MA 310 CMR 7.10

Massachusetts noise regulation 310 CMR 7.10 prohibits sources of sound from increasing broadband sound levels by more than **10 dB(A) above ambient background noise**, or producing a "pure tone" condition, measured at property lines and nearest inhabited residences.


## Comparable – City of Cambridge

Noise Control – Chapter 8.16

Permitted Daytime Noise Levels 7am - 6pm

TABLE OF ZONING DISTRICT NOISE STANDARDS						
Maximum Allowable Octave Band Sound Pressure Levels						
Octave Band Center Frequency Measurement (Hz)	Residential Area		Residential in industrial		Commercial Area	Industry Area
	Daytime	Other Times	Daytime	Other Times	Anytime	Anytime
31.5	76	68	79	72	79	83
63	75	67	78	71	78	82
125	69	61	73	65	73	77
250	62	52	68	57	68	73
500	56	46	62	51	62	67
1,000	50	40	56	45	56	61
2,000	45	33	51	39	51	57
4,000	40	28	47	34	47	53
8,000	38	26	44	32	44	50
Single Number Equivalent (dBA)	60	50	65	55	65	70

## Town of Arlington



Town of Arlington  
Department of Health and Human Services  
Office of the Board of Health  
27 Maple Street  
Arlington, MA 02476

Tel: (781) 316-3170  
Fax: (781) 316-3175

MEMO

To: Board of Selectmen  
From: Natasha Waden, Public Health Director  
Date: February 26, 2017  
RE: Health Department Comments- Article 15

The Health Department has reviewed Article 15, which looks to amend Title V, Article 12 of the Town Bylaws as it relates to noise abatement and has the following comments:

The current bylaw establishes a maximum sound level of 85 dB (A) for sounds other than those associated with construction or maintenance work; practically speaking, a noise meter would not be used to measure the sound generated from a party or event. The effective intent of the noise abatement bylaw as it relates to maximum noise levels is to ensure that fixed, continuous sources of noise do not exceed 85 dB(A), as lasting exposure to sounds above this threshold could be detrimental to public health... Common examples of applications of the decibel threshold include HVAC units in disrepair, the placement of backup generators, restaurant exhaust systems and other equipment using motors. Whereas an event or party is a single, temporary occurrence and the noise generated by a group of people can be fleeting and highly variable, it would be challenging to accurately measure this type of noise given the fluctuating magnitudes, and would need to be conducted over some extended period of time. For sounds associated with human activity, including the operation of public address loud speakers, and not the operation of equipment or machinery, the effective intent of the bylaw is not to place sound limits, but rather to limit the timeframe the activity is permitted. This type of activity is commonly dealt with as a non-measured noise disturbance by APD who manage issues related to disturbing the peace. As the proponent noted, the Health Department would not have personnel available at the this time of day, or on weekends to measure noise levels of parties. Additionally, if the Health Department were to be the responsible department to enforce a neighborhood communication plan for parties of more than 10 people, it would be difficult for our department to ensure compliance with such a requirement and/or conduct spot checking. The Health Department's recommendation would be to investigate the number of complaints received by APD and the time of day that these types of complaints are received, prior to making any changes to the Bylaw.



# Operating Hours

While the brewing equipment has been considered in the applicant’s materials, as referenced by the 65dB and sound wall proposal, *nothing is mentioned regarding controls around patron noise.*

The proposed operating hours until 11pm, 6-nights per week, *represent a significant departure from* our current experience as residents today.

Approving these hours would suggest that we as residents should expect to endure car doors closing, engines revving and outdoor conversations all disrupting our and our children’s sleep.

**We do not find this an acceptable starting or end point.**

Our Questions Include:

- How will noise be measured and monitored?
- If by complaint only how will those be substantiated? How many complaints until intervention is required?
- Will operating hours be curtailed until a sustainable solution is found?

## Per Section 5.6.4-D Arlington Zoning Bylaws

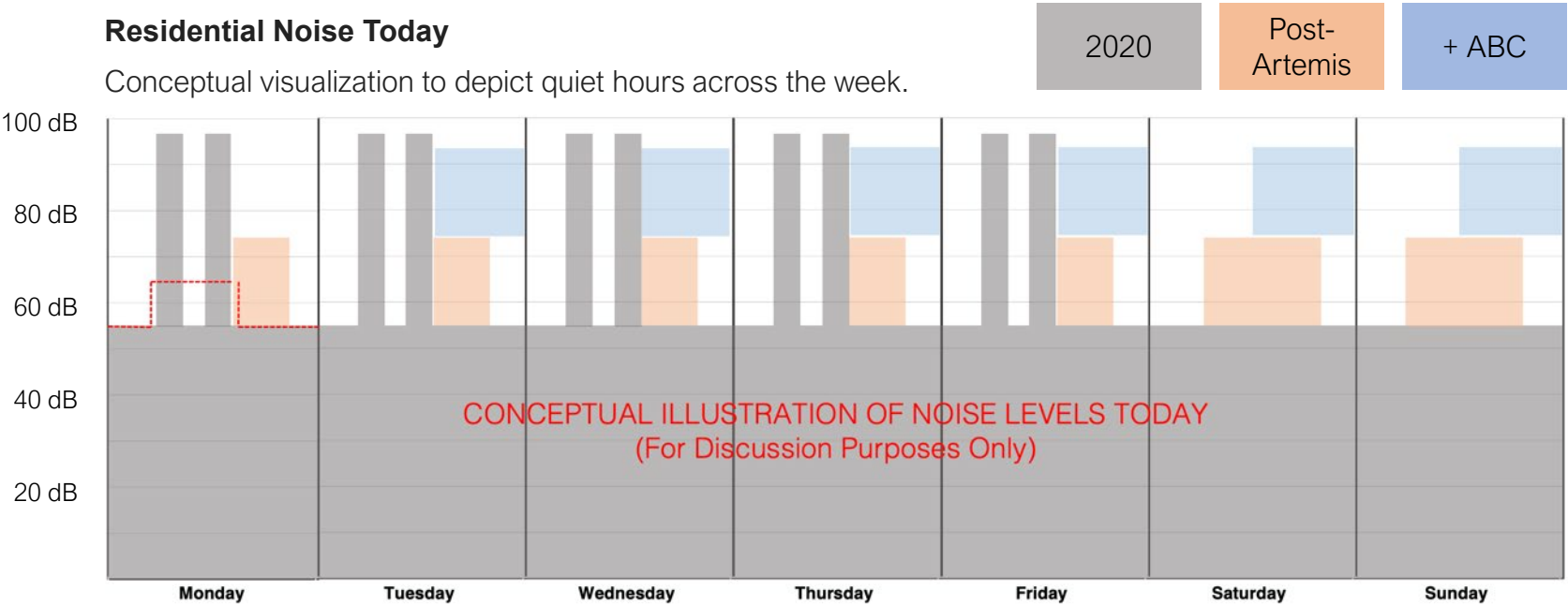
Tap room hours of operation open to the public shall not represent disturbance to adjacent residential uses and such hours must follow the Commonwealth of Massachusetts requirements for licensing and operations.

## Reference Noise Levels

- Talking (60dB)
- Shouting (88dB)
- Car Engine (70dB)
- Car Door (100dB)

## Residential Noise Today

Conceptual visualization to depict quiet hours across the week.





# Noise Case Study – DownEast v. Boston

Downeast Cider had to spend ~\$25,000 on sound mitigation on the outdoor glycol chiller after neighbors submitted grievances about the noise and the city ordered them to install a sound barrier.





# QUALITY OF LIFE | PARKING








# Parking & Traffic Congestion

As shown here, our 500ft street is easily overwhelmed. This picture of parking spillover from Ed Burns during high-school playoff hockey shows cars double and triple parked, displacing residents, and blocking fire lanes.

While is this manageable 2-3 times per year, **this would not be acceptable on a daily or weekly basis.**



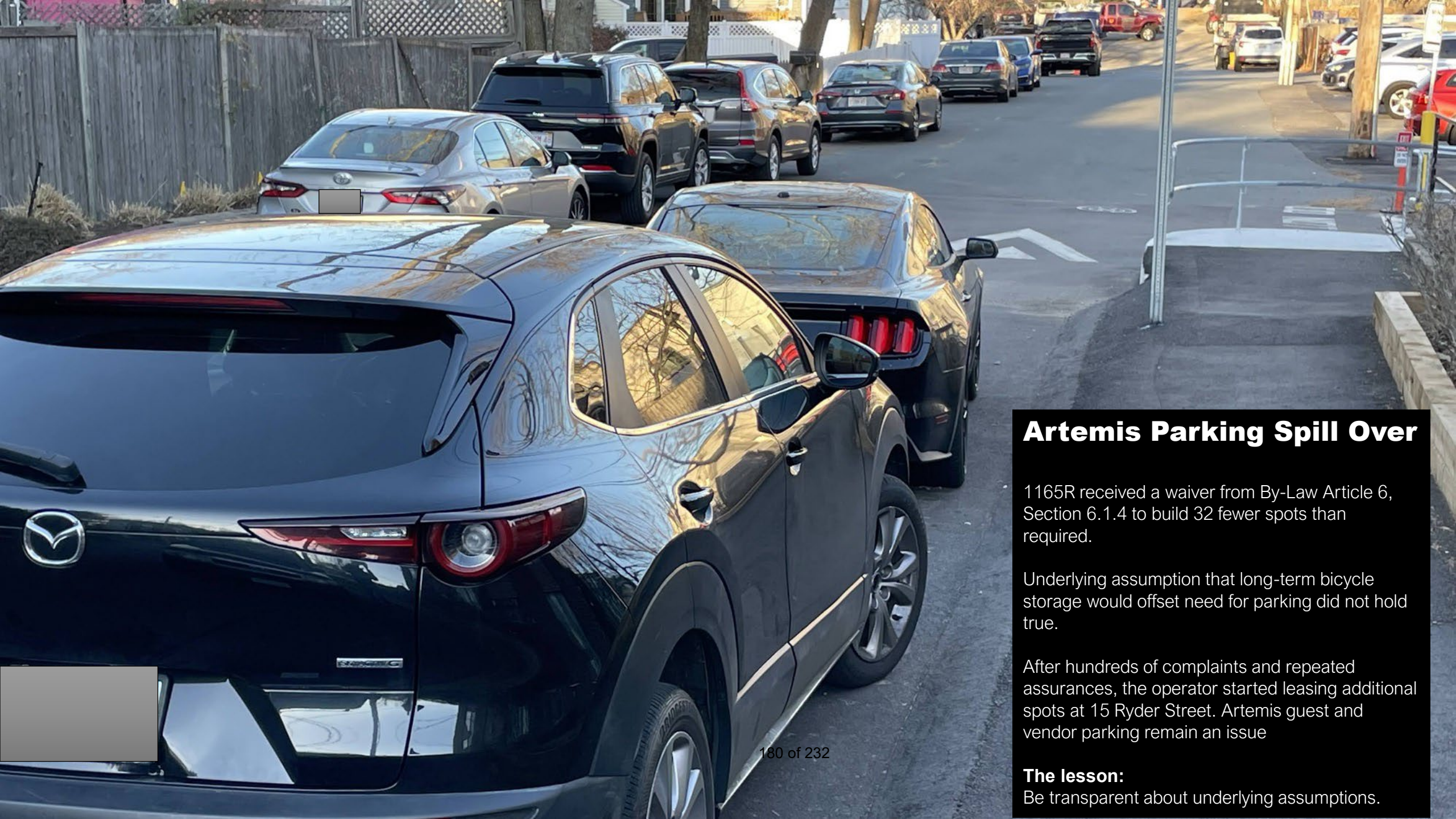
## Shown:

- Only 2 cars visible here are residents of Ryder Street
- All the parking shown is on private property, only 2 cars have property owner's permission to park.

## Not Shown:

- Additional parking spill-over onto Peirce, Frazer, and Forest Street.
- Congestion from ride share, package delivery and food delivery vehicles turning into 1165 driveway.





## Artemis Parking Spill Over

1165R received a waiver from By-Law Article 6, Section 6.1.4 to build 32 fewer spots than required.

Underlying assumption that long-term bicycle storage would offset need for parking did not hold true.

After hundreds of complaints and repeated assurances, the operator started leasing additional spots at 15 Ryder Street. Artemis guest and vendor parking remain an issue

### **The lesson:**

Be transparent about underlying assumptions.



# Applicant Parking Assumptions

Can the applicant and Members of the ARB help us understand the assumptions around parking spaces?

- Why is the first 3,000ft of non-residential space exempt?
- Will the manufacturing or tap-room/restaurant staff not require parking?
- Why is it that only 1-space is needed for every 4 seats? Is it assumed that 3 out of 4 patrons will carpool, walk, bike or rideshare?
- Isn't a general rule of thumb 1 car spot for every 3 seats, which would suggest 35 parking space are required?
- Wouldn't existing rules apply more to commercial districts where a supply of shared parking is already available to offset peak demand?
- Where would the town and applicant expect overflow parking to be redirected? Would patrons adhere to those instructions or take easy route and displace residents?
- If overflow parking persists, where would supplemental permanent parking be found? In interim would occupancy be reduced to account for difference?
- Who will be responsible for enforcing parking habits and monitoring fire lanes?
- How do all of these questions differ for a typical commercial district versus a small mixed-used neighborhood on 3 private ways?

## Support Narrative for Dimensional and Parking Form

Prepared by: Reisen Design Associates

Date: 1.24.2025

### Floor Area Ratio Existing

1<sup>st</sup> floor (4,000GFA) + Mezzanine (1,755GFA) = 5,755GFA

FAR = 5,755/45,687 = .12

### Floor Area Ratio Proposed

1<sup>st</sup> (4,000GFA) + Mezz (1,045GFA) + Addition (960GFA) =6,005GFA

FAR = 6,005/45,687 = .13

### GFA by use

Manufacturing = 960 (walkin) + 1,070 (brewfloor) +103 (boiler rm) = 2,133GFA

Restaurant = 4,000 – 1,070 -103 + 1045 (mezz) = 3,872GFA

### Parking Spaces

Section 6.1.10(C) exempts the first 3,000 square feet of non-residential space in mixed-use developments from the parking requirements of Section 6.1. As per discussion with Director Ricker on January 21, 2025

## Parking Spaces

Section 6.1.10(C) exempts the first 3,000 square feet of non-residential space in mixed-use developments from the parking requirements of Section 6.1. As per discussion with Director Ricker on January 21, 2025 the following is how the mixed-use parking reduction is applied: 3,000 GFA reduction = 2,133sq.ft. GFA from Manufacturing and 867sq.ft. GFA from Restaurant

Uses: Manufacturing = 2,133GFA, Restaurant = 3,872GFA

- Manufacturing = 2,133 sq.ft. GFA – 2,133 sq.ft. = 0 spaces required
- Restaurant = 1 space per 4 seats total seating capacity excluding seasonal , including above reduction: 106 seats total – 38 seat reduction (seating at Mezzanine = 867 sq.ft.) = 68 seats  
**68 /4 = 17 parking spaces**



## Mismanaged Deliveries

Although the 1165R approval was contingent upon *prohibiting* deliveries during middle school commute hours, enforcing this measure was entirely ineffective with multiple violations weekly, which is why we are emphatic that a Freight Broker be engaged to be accountable for ensuring adherence to dock hours and delivery windows.





## Case Study – Great American Beer Hall



Insufficient on-site parking results in cars circling for space to open up, adding noise and congestion with impatient patrons parking in adjacent neighborhood.



# QUALITY OF LIFE | ODOR



# Odors | Brewing

Brewery odors, often described as smelling like rotten eggs or sulfur, are primarily due to the production of hydrogen sulfide ( $H_2S$ ) during fermentation, especially with certain yeast strains.

**Cause:** During fermentation, yeast naturally produce various compounds, including hydrogen sulfide ( $H_2S$ ), which has a characteristic "rotten egg" or "sulfur" smell.

**Anaerobic Conditions:** In wastewater treatment systems, brewing byproducts ferment in anaerobic (oxygen-deprived) conditions, leading to the formation of hydrogen sulfide.

**Yeast Strains:** Certain yeast strains, particularly those used for lagers, are more prone to producing higher levels of sulfur compounds.

**Mitigation:** To reduce sulfur aromas in beer, brewers can consider using different yeast strains, avoiding high sulfur content in brewing water, and allowing the beer to age longer.

**Wastewater Issues:** Hydrogen sulfide in brewery wastewater can also lead to corrosion of metal structures and concrete due to the formation of sulfuric acid when exposed to oxygen.

**Other Odors:** Besides sulfur, other odors can arise from fermentation, such as fruity esters, or even off-flavors like stale cardboard.



# Odors | Restaurant

Restaurant nuisance odors, often stemming from grease buildup, plumbing issues, or improper ventilation, can be addressed by focusing on odor control systems, regular maintenance, and addressing the source of the problem.

## Causes of Restaurant Odors

**Grease and Oil Buildup:** Accumulation of grease and oil in traps, drains, and other areas can lead to foul smells.

**Plumbing Problems:** Issues with floor drains, dishwasher drains, or plumbing stacks can allow sewer gases to escape.

**Improper Ventilation:** Inadequate or poorly maintained kitchen ventilation systems can trap odors.

**Food Waste:** Proper disposal of food waste is crucial to prevent odors from developing.

**Cooking Emissions:** Certain cooking methods and ingredients can produce strong, sometimes unpleasant odors.

Is it the assumption of the board that residents will be responsible for reporting complaints? If so how will those complaints be substantiated? How many complaints will it take to trigger remediation/intervention?

## Solutions and Prevention:

**Odor Control Systems:** Air Purifiers: Use air purifiers with activated carbon filters to absorb odors.

**Activated Charcoal:** Activated charcoal can be used to absorb odors in various areas.

**Molecular Filtration Systems:** Effective kitchen exhaust filtration systems can remove odors from the air.

**Regular Maintenance:** Clean Drains and Grease Traps: Regularly clean and maintain drains, grease traps, and other plumbing fixtures.

**Maintain Ventilation Systems:** Ensure that kitchen ventilation systems are properly maintained and functioning.

**Address the Source:** Proper Food Waste Disposal: Ensure that food waste is disposed of properly and promptly.

**Cleanliness:** Maintain a clean and sanitary environment in the kitchen and dining areas.



## Odors | Smoking

The new 130-unit Artemis is a non-smoking campus, which displaces second-hand smoke and discarded butts onto adjacent properties.

How might ABC designate a desirable space for patrons who smoke to enjoy their behavior in a dignified manner that minimizes exposure to neighboring properties?

# 11.6%

Estimated that 28.8M of US Adults currently smoke cigarettes

# 12

Number of ABC patrons at max capacity that could require smoking.



One-week of discarded cigarette butts on doorsteps of multiple Ryder homeowners, collected by Ryder residents and guests.



# COMMUNITY SOLUTIONS



## Finish The Job... with Best Practices

Address the insufficient installation of features agreed to in the 1165 development. Unclear if the issue lies with Inspectional Services or the Developer, but either way the issue exists on town land and creates a liability for the town.

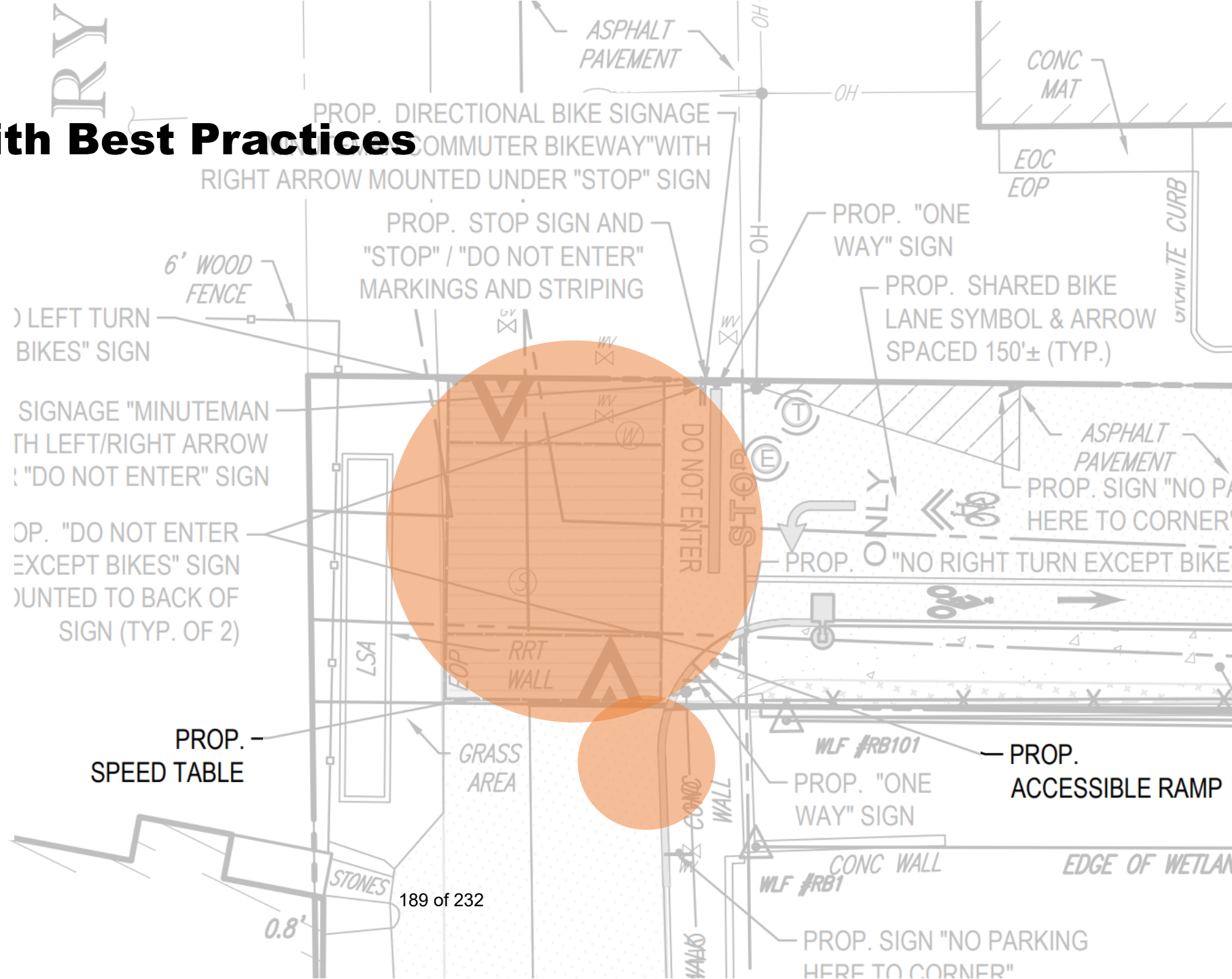
## Speed Controls

Rebuild the speed table with an **effective** design that has sufficient approach angle and height to control speed, or introduce alternative measures (e.g. series of speed bumps).

We do not lack examples of best (or even adequate) practices being used in nearby Somerville, Cambridge, and Lexington.

## Sidewalk Access

Conduct engineering evaluation to determine if the rebar discovered during site prep is integral to the structural integrity of bridge, so that the sidewalk can be lowered to the necessary height to enable an accessible ramp.





# A Little Paint Goes a Long Way

When proposing an end-to-end pedestrian pathway, many folks imagine formal lanes, curbing and cost.

Yet an alternative approach using asphalt art has been proven to make streets safer at a fraction of the cost.

How might we try creative approaches like these, as semi-permanent interventions to validate the design before longer-term solutions are implemented, if at all?

The Town and the landlord of the proposed ABC have the opportunity to vastly improve safety for our community members along their properties.

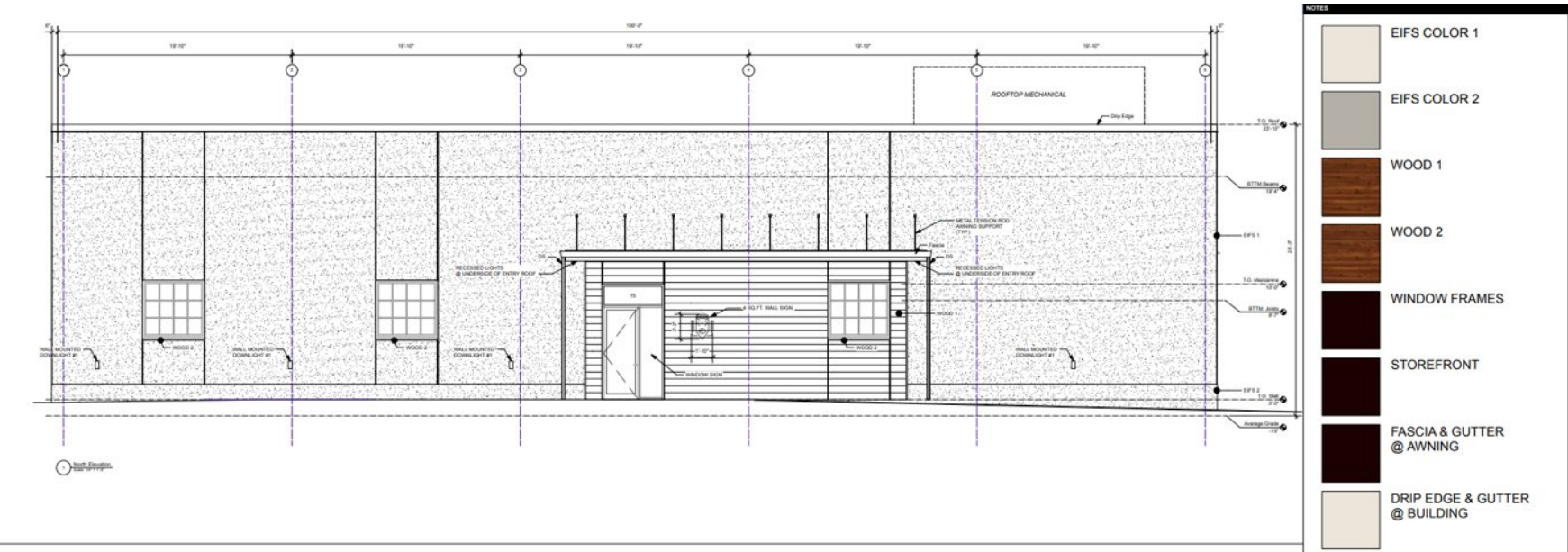




# Visual Vibrance

The 1165R Development displaced a number of artists and in support of ABC’s mission to create a welcoming community space, we would welcome using the façade of ABC as a canvas to welcome back these displaced community members.

There are a number of wonderful examples from within Arlington and beyond and believe this would benefit both ABC and the community at large.





## Re-Greening

Over the past 10-years, Ryder Street has lost a lot of green as mature growth coupled with increasing winds have necessitated removal for user and infrastructure safety.

We applaud ABC's vision to bring green back into our community and see an opportunity to provide visual continuity end-to-end through a pattern of green spaces.

We want the ARB to ensure these plantings do not further impede sight lines or produce odor during bloom.





# NEXT STEPS



## The Risk of Forging Ahead Is Real.

We know Arlington wants amenities like ABC, with overwhelming passing of MBTA Communities Act.

As a community, what we have yet to figure out is how to achieve this vision while navigating the inevitable growing pains.

Our aim is to be ‘Yes And...’ collaborators, that contribute to find right-sized solutions that move us forward. We believe our voice is critical as no one is closer to the problems than the residents of Ryder and Beck, because we live this 24/7.

For this process to work it requires openness, candor, and accountability from all sides. Especially those stakeholders—representatives the Town and highly influential developers—who hold disproportionate power to others.

We are a small group of neighbors who have invested hundreds of hours of our personal time in advocating for the safety and needs of the broader community who use Ryder Street and hope that this forum serves as a more effective platform for these real concerns.

**This project cannot move forward until there is accountability that the risks and burden will be managed.**



# Preliminary Checklist

## Brewery Approval

- ☐ Delivery Enforcement (Dock Hours & Vehicle Type)
- ☐ Noise Enforcement (Chillers, Forklift)
- ☐ Odor (Brewing)
- ☐ Pest Control (Grain Pick-Up)
- ☐ Lighting (Indirect, Hours)

## Tap Room Approval

- ☐ Impaired Driver Control & Enforcement
- ☐ Noise Enforcement (Patron)
- ☐ Wayfinding (Business, Parking, Overflow Parking)
- ☐ Parking Enforcement (Overflow)
- ☐ Odor Enforcement (Smoking)
- ☐ Off Street Accommodation for Rideshare
- ☐ ABC Parking Exit Sight-Lines (DPW Blocks)
  
- ☐ Painted Sidewalk & Fire Lanes
- ☐ Paint Lanes & Bike Sharrows
- ☐ Wrong Way Traffic 1165 Driveway
- ☐ Block Vehicles Crossing Bike Path
- ☐ Rebuild Speed Table (or alternative measure)
- ☐ Sidewalk Accessible Ramp

## Restaurant Approval

- ☐ Odor (Cooking)
- ☐ Pest Control (Food Waste)

## General Needs

- ☐ Trees & Green Space (DPW, 15 Ryder, 9 Ryder)
- ☐ Asphalt Art
- ☐ Mural on 15 Ryder
- ☐ Muzzioli Management
- ☐ DPW Exit Sightlines (Relocate Blocks & Bins)
- ☐ Bike Path Entrance to Ryder (Widen Path)
- ☐ DPW Removal Barbed Wire



# Suggested Next Steps

## Walk The Neighborhood (Please! We Welcome YOU)

Build a shared understanding of the competing priorities, users and risks. Ideally conducted during 7:30-8:30am weekdays with following participants:

- ABC Representatives
- ARB
- Neighborhood Representatives
- John Alessi (guidance on best practices)

## Quantify Risk of Alcohol

Understand the incremental risk we are accepting as a community in terms of the relative or absolute risk, by adding impaired drivers to the mix of unsafe conditions on Ryder/Beck.

## Traffic/User Study

Understand the existing patterns on Ryder and Beck, inclusive of multiple user types (industrial, residential, delivery, rideshare, adult pedestrian, child pedestrian, cyclists) and across different times of day.

Also suggest observing traffic volume and flow at comparable breweries in comparison to demand to anticipate peak volume.

## Noise Baseline Readings

Conduct a longitudinal measure of noise at 14 or 18 Ryder street to provide some objective measures to work from.

## Survey Town Residents

Understand value of ABC in comparison to other recently opened or proposed community amenities to provide point of comparison.

## Survey Residential Abutters to New Breweries

Reach out to abutters of Medford Brewing Company and Great American Beer Hall to better understand latent needs and the

**The challenges outlined here will not be up to one person or entity to solve.**

**This process will require time, energy, and iteration, but Ryder Street in its current form cannot support ABC's current proposal safely or equitably.**

**From:** Don Seltzer

**Sent:** Saturday, February 22, 2025 11:40

**To:** Rachel Zsemlery; Kin Lau; Eugene Benson; Stephen Revilak; Shaina Korman-Houston

**Cc:** Claire Ricker; Disability Comm

**Subject:** Docket #3838, 15 Ryder St - Accessibility Concerns

To: Arlington Redevelopment Board

I call to the Board's attention the following concerns related to state and federal laws on Accessibility.

**Safe Egress** - The applicant needs to indicate the two required Accessible Means of Egress from the second floor.

521 CMR 20.11

20.11.1 All *spaces* or *elements* required to be accessible by 521 CMR shall be provided with no less than one accessible *means of egress*.

Where more than one *means of egress* is required under 780 CMR (The Massachusetts State Building Code) from any *accessible space* or *element*, each space or element shall be served by not less than two accessible *means of egress*.

**Parking** - The plans show a single HP space with an Access aisle approximately 5' wide. State law requires that this space be Van Accessible with an Access aisle of at least 8' width.

**521 CMR 23**

23.2.2 One in every eight *accessible* spaces, but not less than one, shall be van *accessible*, See **521 CMR 23.4.7**

The Board should also consider that the proposed parking plan creates a potentially dangerous situation for this accessible aisle. The lack of a curb means that there is no clear demarcation of just where the driveway begins. The aisle is highly vulnerable to drivers who enter the lot by turning just after the utility pole, cutting the corner. The request to reduce the setback of the park space from the required 10' only increases the danger.

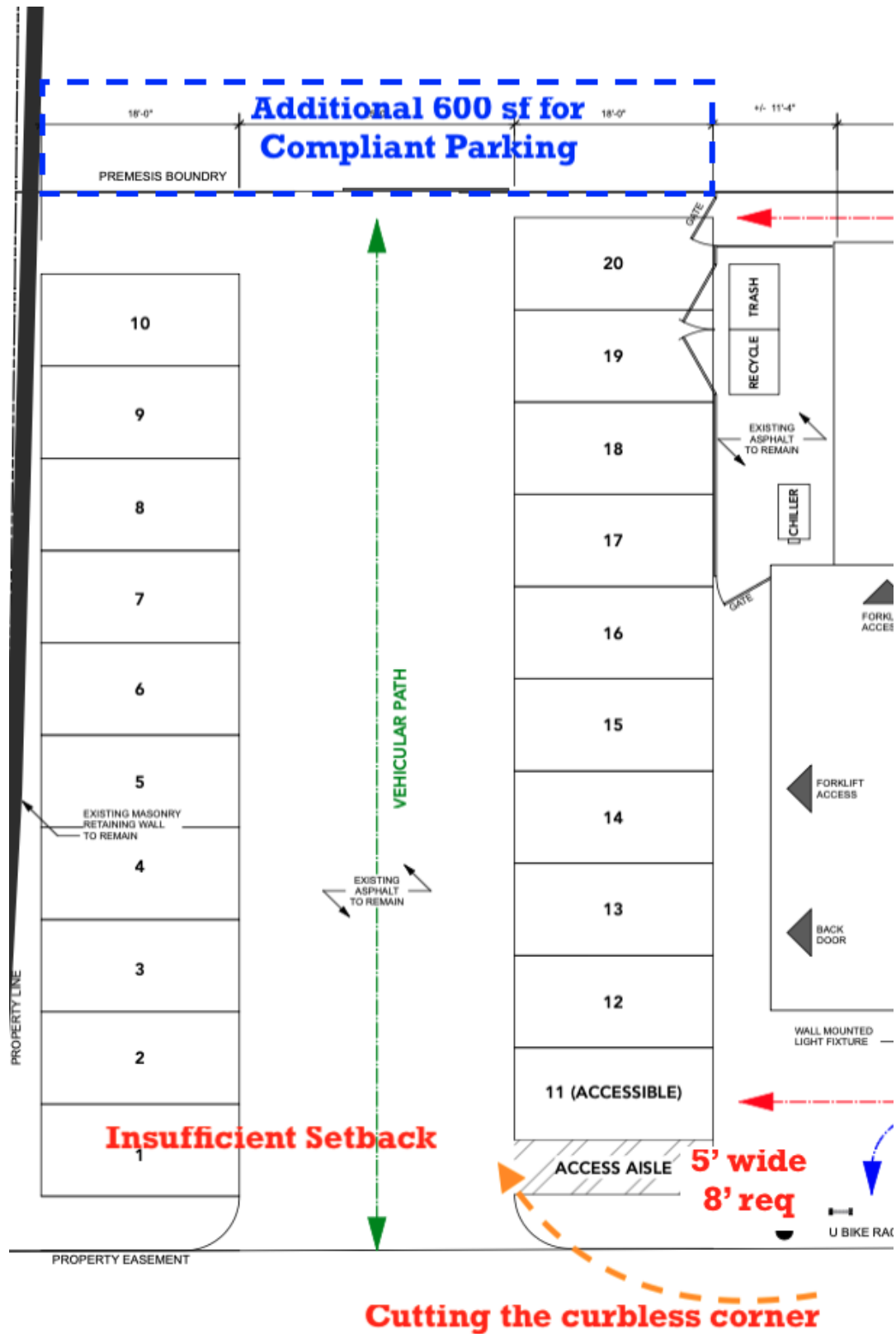
That request is for a waiver of 6.1.10 F of the bylaw

The parking area shall be located to the rear or side of the primary building. **No parking shall be permitted in the front yard** nor shall any driveways directly in front of a structure be permitted without a finding by the Board of Appeals or the Redevelopment Board, as applicable, that the parking or driveway is necessary and convenient to the public interest.

How can it be claimed that parking in the front yard is necessary?

The actual lot at 15 Ryder is 45,687 ft. The applicant is leasing just 39% of the lot, with the rest remaining vacant for an undetermined future use. The solution to the parking 'problem' is to extend the proposed leased parking area by 10' to the rear, or just 1.3% of the lot area (600 sf). That slight adjustment will bring this development into compliance with the bylaw, make the entrance safer for pedestrians and bicyclists, and reduce the visual impact on the neighbors across Ryder St.





Thank you for your consideration,  
Don Seltzer

**From:** Carly Campbell

**Sent:** Sunday, February 23, 2025 2:04 PM

**To:** Claire Ricker

**Subject:** Support for Arlington Brewing Company

Hello,

I am writing to express my support for the Arlington Brewing Company plans at 15 Ryder St. As a 10+ year resident of Arlington, I feel the addition of a brewery to the Ryder St neighborhood will be a great benefit to the town. We very much need indoor and outdoor gathering spaces for families and social groups, which the brewery will provide. Arlington residents' excitement about ABC has been clear to me through the attendance at various events such as the summer beer gardens at the Rez. I am thrilled the location has such easy access to the bike path, and look forward to meeting up with friends and family there - I hope it will be open soon.

Thanks

Carly Campbell

50 Westmoreland Ave

**From:** Laura Gitelson  
**Sent:** Sunday, February 23, 2025 11:08 AM  
**To:** Claire Ricker  
**Cc:** Josh Arnold; Tom Allen  
**Subject:** Support for Arlington Brewing Company

Dear Ms. Ricker -

I am writing to share my family's strong support for the Special Permit request for the Arlington Brewing Company plans on Ryder Street.

We first met Tom Allen in 2021 when our children attended pre-school together. Those same pre-schoolers are now in second grade at the Peirce School. I remember clearly Tom talking about his hope to open a family friendly brewery in Arlington and how excited we were to hear about it. Since 2021 we have followed the plans of ABC with great interest. It has been so exciting to watch this local business grow from a home brewing set up in Tom's basement to the incredibly successful Reservoir Beach Beer Gardens and the availability of ABC beer in many local establishments. We are so incredibly excited about the brewery opening a permanent home on Ryder Street, walking distance from our house.

One of the things we have been most impressed about (aside from the quality of the beer!) is Tom's commitment to building this business in Arlington. It has been a long journey of looking for a permanent home - it would not have been unreasonable for ABC to look beyond Arlington for a home after all that time. Arlington needs to do everything it can to encourage this kind of commitment to growing local business - it shouldn't be this hard.

We are not able to attend the meeting on the 24th, but are hopeful that it will work out positively both for Arlington Brewing Company and the people of Arlington. This has been a long time coming.

Best,

Laura Gitelson & Josh Arnold (Josh is a Town Meeting representative for Precinct 17, where the brewery would be located)  
56 Bow Street

--

Laura Gitelson  
Pronouns: she, her, hers ([what's this?](#))

(646) 279 - 3265



From: Khalid Lawrence  
Sent: Sunday, February 23, 2025 3:45 PM  
To: Claire Ricker  
Subject: Arlington Brewery Meeting - 2/24/25

Hi Claire,

Hope this message finds you well

Wanted to send a note as I may not be able to make the town meeting tomorrow but wanted to voice my support for the building of the Arlington Brewery. My wife and I love to support our local businesses and enjoy craft beers. I have been a fan of ABC since its inception and would love this addition to our town.

Appreciate you taking the time to read this and have an excellent week.

Thanks,  
Khalid Lawrence

**From:** Lauren Magoun  
**Sent:** Sunday, February 23, 2025 4:59 PM  
**To:** Claire Ricker  
**Subject:** Support for 15 Ryder

Hello!

I am unable to attend the 2/24 meeting.

I am an Arlington resident and I am so excited about this new addition to the community! I am in support of the plans as included on the website. They seem well thought out, with sufficient parking and easy access to parking on Mass Ave (if needed) and the bike path!

Kind regards-

Lauren Magoun

**From:** Michael Basham  
**Sent:** Monday, February 24, 2025 12:40 PM  
**To:** Claire Ricker  
**Subject:** Support for 15 Ryder St Application

Hi,

I wanted to register support for the special permit application for 15 Ryder St. I believe the brewery will be a fantastic amenity for Arlington and a much needed "third space" for people to hang out in town in the evening.

I do have one comment: I think the planned bicycle parking amount is too low given the brewery's proximity to the bike path, and what I assume would be an intended preference for people to get to the site without a car. The Reservoir Beer Garden events were overflowing with bicycles that couldn't be locked at the existing racks, and I think the town in general doesn't provide enough bicycle parking in public places or near businesses. I would recommend that one of the car parking spots gets reassigned to a bicycle corral that would fit another 12 bicycles, or that a capacity increase be accommodated elsewhere on-site, at least seasonally.

Thank you,  
Michael Basham  
103 Bow St Unit #2, Arlington MA



**From:** Dan Brownell  
**Sent:** Monday, February 24, 2025 1:50 PM  
**To:** Claire Ricker  
**Subject:** Support for Arlington Brewing Company

Hello,

I am writing in support of the Arlington Brewing Company's special permit for their new space in Arlington. This will be an amazing addition to our town, something we can all be proud of. I know that Tom and his team are working hard to provide an excellent gathering space that will be respectful to our community and neighbors.

I hope that the permit approvals will go through quickly to allow this new space to open asap.

thank you!

Daniel Brownell  
485 Appleton St, Arlington

## Responses to the Special Permit Under Environmental Design Review Docket 3838

Mariah Contreras | February 22, 2025

To: Claire Ricker

---

Hello. I live on a very unsafe street called Ryder Street. It is a street where large contractor trucks with trailers take up lots of space and rumble loudly, where our Town's DPW drivers tend to drive a bit too fast, where large box trucks set up their exit strategy, and amidst all this middle schoolers walk to and from Ottoson, preschool groups lined up from the Greek church are going to a playground near the ice arena, and cyclists of all varieties are getting on the bike path. Oh and it's a private way, which in Arlington, can be a very confusing kind of street to live on. It is a street where nearly every day I worry about a person getting hurt. This concern first ticked up when I became a parent six "and a half" years ago and I more fully realized that the large contractor trucks and speeding drivers cannot see strollers or toddlers on the street level. The concern next ticked up as the Artemis developers paved our formerly bumpy Ryder with fresh asphalt and the smooth surface increased vehicle and bike speeds. After having my house hit by large trucks and seeing over a dozen close calls over the years, most with children, I feel pretty worn down that adults in our community don't care enough to somehow make this street safe. If I get one statement it is this: It is critical to know just how unsafe Ryder Street is as the Town considers the application for 15 Ryder Street Docket #3838.

But! In this ARB process it sounds like I get more than one statement. I thought that provided the submitted Application for Special Permit is the perspective of one stakeholder in this neighborhood (the applicant's), I could review the application and provide you and the Board Members another stakeholder's perspective (mine) and one that represents the local expertise I've gained from nearly a dozen years of residing on Ryder Street. I tried to fit my responses into 3 primary types:

*Point of Correction-* factual correction (can be objectively measured)

*Point of Information-* as an abutter, I am providing my personal knowledge of the neighborhood/perspective

*Point of Interest-* as an abutter/community member I hope the Board considers this frame in their deliberations with the applicant.

### MEMORANDUM

*Correction to an Omission:* On the December 12 meeting, abutter Naren was also in attendance and provided professional experience and in-depth information regarding ventilation requirements and commercial use cases of area towns, which I would ask the Board to consider as a direct concern, not "high-level".

*Point of information:* What the applicant cites as "high level concerns, particularly about vehicles driving too fast... and need for traffic calming measures" I would ask the Board to consider as *direct* concerns

about the applicant's role of serving alcohol to patrons who will then be walking, cycling, or driving among and adding to the unsafe conditions of Ryder and Beck Streets.

*Point of information:* the closing of the Memorandum states the applicant has "gone to great lengths... to address the concerns" and that the current "design will dramatically improve the overall safety, attractiveness, and quality of life in the neighborhood." I and many of my neighbors feel the applicant has made *some* (but not "great") efforts to gain information from abutters, which many of us appreciate, but our position is that as proposed, the design of ABC

- 1) does not dramatically improve the overall safety of our street: our view is this proposal does not yet address how a commercial enterprise centered on serving alcohol to its patrons directly and seriously erodes the safety of a street already known to be unpredictable, unsafe, and used by a broad array of community members.
- 2) does not dramatically improve the overall attractiveness of our street: although the proposed greenery is a welcome addition to the latest version of the plans, the modest changes to the exterior of the building are a minimal, not dramatic, improvement.
- 3) does not dramatically improve the overall quality of life in the neighborhood: our position is that this proposal will dramatically erode the quality of life for residential abutters of the three private ways that surround the proposed brewery and restaurant; residents who historically and currently know that they can rely on relative calm after 5PM on weekdays and most all of the weekends, who are currently not burdened by brewery and restaurant odor and exhaust, and whose concern for the safety of our families and community members has never had to account for an influx of people who have been imbibing alcohol.

## **SPECIAL PERMIT CRITERIA**

### **2. The requested use is essential or desirable to the public convenience or welfare.**

*Point of information:* The applicant cites under Local Economic Growth "the brewery will contribute to the local economy by... attracting patrons who will also visit neighboring businesses". The neighboring businesses are: a landscape contractor, a master plumber, a moving company, and a rug/carpet cleaner; all with no or limited weekend hours. Otherwise, without a clear boundary of "neighboring" it is difficult to understand what businesses patrons will visit.

*Point of information:* The applicant cites under Enhancing Public Convenience "the brewery reduces the need for residents to travel out of town...minimizing vehicle trips" however if this is to be a successful brewery, it is reasonable that interested parties from other towns will seek to drive to the brewery which does not enhance the public convenience of those patrons. Additionally, *any* use of vehicles in and out or awaiting rideshare customer at the 15 Ryder site will be a new, inconvenient addition to the traffic congestion of the hyper-local neighborhood of Ryder and Beck, and also the much-studied traffic intersections of Appleton-Mass Ave-Forest.

*Point of interest:* The applicant cites under Public Support that "Arlington residents...demonstrated a strong desire for a local brewery with a brick and mortar presence". The proposal goes well beyond a brewery to also include a tap room, beer garden, and restaurant operations.



Is there a rationale for taking on so many operations simultaneously, especially as a first-time proprietor. [Please see Table 1 for initial list of differences potentially experienced across neighborhood types]

Many Arlington residents may indeed desire a brewery in yes/no absolute terms, but would they be willing to have said brewery-restaurant nestled 100ft from their home? It seems reasonable to hypothesize that the response would shift if burden was incorporated into the polling (unclear whether there was a formal or informal poll).

*Point of interest:* Under Operating Experience the applicant cites they have operated beer gardens and reviews the quality of their product. Product quality does equal operational experience. This is not addressing the inquiry of operating a brick and mortar micro-brewery, a tap room serving alcohol to patrons, or a restaurant. Any one of these operations alone is a serious undertaking, even for a seasoned proprietor. It would be of interest to understand:

Who are the consultants and best practices/peer models the applicant has conferred with on each of these operations?

How does the Board understand an application like this from an applicant with industry operation experience and an applicant without?

Likewise, how is the Town/Board using counsel and input from other municipalities who have experience with micro-breweries and/or restaurants in heavily residential areas?

### **3. The requested use will not create undue traffic congestion or unduly impair pedestrian safety.**

*Point of information:* The applicant cites “the current use of the (15 Ryder) site generates significant large-vehicle traffic, including construction and landscaping vehicles accessing on-site storage which contributes to congestion and potential safety concerns” and this is a misrepresentation of the historical and current use of the 15 Ryder site. The site does not generate significant traffic of any type and historically is not among the entities responsible for the congestion and serious safety concerns that our neighborhood has tried to relay to the applicant. (Those entities are the DPW and the landscape contractor at the end of Ryder, among other Beck businesses).

*Historically:* since 2007/2008, 15 Ryder was owned by Bobby and Guy Lalicata. The Lalicatas leased office space to a contracting firm (whose 2-3 office employees drove personal vehicles and parked on the southwest side of the building) and other parking lot space to various other small contractor businesses. Bobby Lalicata used the large portion of the building as a garage for his beloved muscle cars. In total, there were very limited cars and trucks exiting/entering the site, most typically through the SW gate where the Artemis driveway is and doing so in a predictable manner and hours. The *one* (more recent) landscaping business there had extremely considerate vehicle operators, again out of the SW gate.

*More recently:* In September 2021, the site was sold to ABCJ Land LLC, presumably a holding company of the applicant’s landlord who was/is the primary developer of the 1165R 40B in the site behind 15 Ryder. Throughout the construction of the 1165R, workers, large tracked construction vehicles, and large delivery vehicles heavily relied on and accessed the gate on the NE side of the building until about May 2024. The tenants on the SW side operated as was usual until their leases were terminated late 2024.

*Current use:* For nearly a year there has been very limited use of the 15 Ryder site in terms of vehicles.

Thus, any new use of 15 Ryder that involves vehicles, especially drivers unfamiliar with the private ways of Ryder and Beck and rideshare drivers, will cause more traffic congestion and impair pedestrian safety on an already unsafe street. The proposed operating hours of the brewery/restaurant will only add new traffic to the times of day and week where there is calm for pedestrians and cyclists.

*Point of information:* The applicant reviews the problems of the “existing parking configuration” however that was historically not parking, but storage. The applicant’s landlord removed the storage and created the current parking configuration in 2022 when she needed to take over the full width of Ryder Street for daily, large construction deliveries, displacing resident and Work Bar patron parking, and since Artemis has opened and had insufficient parking for its tenants, the landlord has more recently used 5 of those spaces for tenant parking. As such, there is no precedent to honor those parking spaces in any new design.

*Point of information:* The applicant also cites that pedestrians are forced into Ryder because of the parking configuration along 15 Ryder and that is not accurate. Pedestrians are forced into Ryder Street because there is no sidewalk from Artemis driveway to the bike path; and where there is sidewalk up to Forest from the Artemis driveway it lacks accessibility to Ryder Street. Although the proposed corridor indicates a step in a positive direction, it will have to be seen if community users view the proposed walkway as private or public use due to its location off the street.

*Point of interest:* **Missing entirely from this inquiry on traffic congestion and pedestrian safety is the applicant’s understanding and plan for operating a business that serves alcohol to its patrons, who then exit onto the already unsafe roads of Beck and Ryder.** If this is to be a successful business, it is reasonable to imagine that many prospective patrons will drive (adding congestion), bike, and walk for its beer product. ***Alcohol only adds risk.*** **The lack of the applicant’s understanding or acknowledgement of the role that alcohol use will play on these unsafe private ways and add in a novel way to the bike path users is highly concerning.** I hope the Board will take seriously the immediate unsafe conditions surrounding 15 Ryder as a uniquely concerning context for a proposal of a business whose main purpose is to serve beer to its customers.

##### **5. Any special regulations for the use as may be provided in this Bylaw are fulfilled.**

*Point of interest:* In a site that has residences on three sides, all very close in range to the proposed taproom and beer garden (less than 100ft for two of my neighbors), does this consideration differ from a site that is in a more typical commercial district? Our neighborhood becomes quiet after the 5pm contractor trucks and DPW vehicles park for the night. I personally do not see how an 11pm closing time will comply with the Bylaw: All noise, smells, traffic from 5pm until 11pm (or later because employees will close up for awhile after) will pose a disruption to the residential quality of life.

What are the noise abatement levels and times of day that are permitted in a residential neighborhood? Is the applicant responsible for establishing baseline readings of the neighboring properties?

What is the local precedent for commercial versus very residential areas regarding operating hours? The difference is important here because the proposed site is surrounded by families on three sides on a private way in very close proximity, not a commercial district.

The bike path closes at 9pm: if cyclists are cited as a “benefit” enhancing public convenience (Q2), how is their safety being considered with these proposed hours of operation?

*Point of interest:* The times listed for deliveries are not consistent throughout the application and it would be of interest to ensure that deliveries are not made during the Middle School Commute times. The ZBA decision for the 1165R site set these restrictions and when observed (meaning, there was an active on-site freight manager) it absolutely made for safer conditions for our community’s young people.

**6. The requested use will not impair the integrity or character of the district or adjoining districts, nor be detrimental to the health, morals, or welfare.**

Although the aesthetic of 15 Ryder site is weathered and lacking visual attractiveness, its use historically has not burdened our quality of life in our tiny neighborhood- it has not provided congestion, traffic, or lots of noise and odors. I look directly at it all day long from my home office. I would much rather have a grey nondescript building with a calm and quiet for my family time and that of my neighbors’ families than a site that has a bit of fresh paint and decimates that calm and quiet.

*Point of information:* The applicant cites they foster safety and “demonstrates care...for the public health” and I am curious how our Town’s public health officials view the use of alcohol and the impairment caused by alcohol on safety and public health. I have only a cursory review of that literature but am including a copy of a 2013 journal article I found helpful, titled “A review of alcohol-impaired driving: the role of blood alcohol concentration and complexity of the driving task” from the Journal of Forensic Sciences for the Board’s interest.

## **ENVIRONMENTAL DESIGN REVIEW CRITERIA**

### **2. Relation of Buildings to Environment**

*Point of correction.* The applicant here starts to use the term/phrase “back of the building” or the like (see also Q3, Q9, and Q12) which I can reasonably understand as the side of the building that is not on the street side of the building. However, it is important to note that in relation to the 2 private ways it abuts, there is really only a back corner, which is simply a corner. It is important to note that in relation to the residences surrounding the building on three sides, there is no “back of the building” and this notion that noisy or odorous components of the operations are “behind” the building means that they face and can be viewed wholesale by the families in the Artemis complex.

### **3. Open Space.**

See Q2 directly above.

### **4. Circulation.**

*Point of correction.* The applicant cites their pedestrian corridor (which is on their property, not the street) “ensures pedestrians can travel safely from the bike path to Forest Street without needing to walk in the street”. This is not true and contradicts information they provided in Q3 of the Special Permit Section where they qualified “along our frontage”. The corridor they propose may provide ~160ft of corridor, but the applicant cannot ensure pedestrians can travel the remaining ~230 ft from their driveway to the bike



path, nor cross the Artemis driveway safely, nor get onto the sidewalk in an accessible manner to make the ~110ft of sidewalk en route to Forest Street.

*Point of information.* The parking space configuration was a very recent remedy for vehicle parking that the applicant's landlord applied to 1165R construction challenges and does not need to serve as a precedent. If anything, the applicant's critique provides his insights into the degree to which the developer/applicant's landlord has regarded pedestrian safety.

*Point of interest.* Although it is desirable to remove the parking spaces alongside Ryder, remove chain link fences, and revitalize with greenery and a pedestrian corridor, such removal of parking spaces will inevitably add to traffic congestion should there not be enough parking in the lot.

*Point of information:* Ryder Street is essentially a one-lane private way that supports two-way traffic. This is complicated by the fact that it is also a dead-end and intersects the 1165 driveway, which operates as a one-lane, one-way road that supports two-way traffic as well as Beck Road which is a one-lane private way that supports two-way traffic.

This non-standard setup makes it very difficult for vehicles to see and anticipate vehicles turning onto Ryder, leading to frustrating bottlenecks that cause frustration and more aggressive driving tactics. Unfamiliar drivers, such as rideshare or delivery drivers, compound this issue further.

Understanding how peak demand will be managed, the impact of additional volume will be critical. Our neighborhood requested a traffic study be conducted by the applicant in our Dec 12 meeting and the applicant relayed a traffic study would present a financial burden. (A resident informed him many of the items that will present him financial burden are the true cost of running a business). Nevertheless, it remains that the following use-cases need to be better understood and accommodated with the design:

- Space for rideshare pick-up and drop-off location in a manner that does not involve creating a bottleneck in the street itself.
- Wayfinding for new drivers coming from either direction provides clear glanceable instructions for drivers to identify:
  - The Establishment (suggest adding prominent signage on top of building on south side as is shown on north side in renderings)
  - Parking Entrance (Reduce confusion and reserve cognitive demand for anticipating other traffic & pedestrians)
  - Parking Availability (Full vs. Vacancy to avoid users creating intra-lot traffic jam that requires one or more to back out)
  - Drop-off Locations (Doesn't require drivers to occupy/block street while waiting for customers)
  - Alternate Parking Locations (ensure that drivers do not attempt driving across bike path to park in Ed Burns which happens 3-4 times/year already)
- Understand visibility pulling out/backing out of 15 Ryder parking as the sightlines are obstructed by parked cars and the block wall of DPW that could impede seeing speeding cars coming down Ryder from Forest as well as speeding bikes coming downhill from bike path.

- While less frequent, understanding how deliveries will be managed is critical. The seasoned brewer we spoke with said that managing shipping companies is a perpetual challenge as controlling for delivery windows and type of truck will be critical to ensuring healthy flow on Ryder Street. They strongly recommended hiring a freight broker as enforcing the agreement can be a full time job.

In our experience, 53ft trailers have a particularly difficult time as they entirely block traffic for the time that they are onsite and then find it difficult to exit having to back down the entirety of Ryder Street at times. We all know that sound. Similarly avoiding early morning (3am) deliveries or deliveries that conflict with Middle School Commute hours is also imperative, both of which were regularly violated during the 1165R construction, underscoring how difficult it is to coordinate with shipping companies even when an agreement is in place.

## 9. Special Features.

Please see Q2 regarding “back of building”.

## 12. Microclimate.

*Point of interest.* It will be important to gain the specific equipment for the brewery, restaurant, and docking operations and the degree/quality of screening; it will be important that an accountability checklist is put in place or a path for abutters to submit grievances with local accountability office. These are not clear to the public, unfortunately.

On the following pages, I reviewed the correspondence our neighborhood has had with the applicant and provided a couple of resources I hope the Board may find useful in framing the concerns and needs of the proposal to-date. The use of a majority residential, private way for this type of business is an extraordinarily special case that I hope the Board will appreciate is distinct from a commercial district or public roadway: Table 1 presents a cursory, incomplete overview of distinctions our neighborhood has reviewed with the applicant—many of which remain concerns. Table 2 presents many of the concerns and questions that we formally presented the applicant last fall—many of which remain concerns but also present an opportunity for making informed decisions.

I appreciate the opportunity to present another stakeholder’s perspective on this application. I am sure you will receive many letters of support for a brewery in Arlington. Support or not support is not a sufficient question for me: When it comes to the safety and well-being of community members, we need to understand the local context surrounding proposals and collect data/best practices to understand the best of the possible alternatives. I hope this can be helpful.

Thank you.

**Table 1. Initial Issues of Safety and Burden to Consider Across Operation Type and Context**

	<b>Brewery</b>	<b>Brewery + Taproom</b>	<b>Brewery + Taproom + Beer Garden</b>	<b>Brewery + Taproom + Beer Garden + Restaurant</b>
<b>Typical commercial district</b>	Brewery ventilation (odor, noise) Waste Management Rodent Management	+Patron alcohol use (safety) +Added operation hours +Patron Parking +Patron (noise) +Entertainment (noise) +Lighting (light)	+Patron outdoor (noise) +Entertainment outdoor (noise) +Lighting (light) +Waste Management outdoor	+Restaurant ventilation (odor, noise) +Extra Waste Management +Extra Rodent Management
<b>3 Private ways, heavily residential area</b>	+Docking equipment (noise) +Deliveries (safety, noise) +Extra timely waste collection	+Community users of road (safety) +Bike path safety +Reduced hours of residents' Quality of Life +Added staff parking +Entertainment parking +Wayfinding +Congestion on small dead end (safety, noise, light) +Taproom (noise) +Smokers (odor, litter)	+Added staff parking +Added exposure and proximity to noise +Added exposure and proximity to light +Added exposure to smoking	+Added deliveries +Added exposure and proximity to odor and noise



**Table 2. Initial List of Data Elements that would (Start to Help/) Enable Determining Best Practices for Proposed Site**

**Safety Concerns**

	<b>Seek to understand...</b>	<b>What is the result and suggested best practice?</b>
General	Understanding types of users	
	Understanding usage patterns	
	Understanding typical patron patterns at peer establishments	
	Understanding anticipated freight/commercial patterns (dock hours)	
	Construction deliveries	
	Unclear wayfinding	
Pedestrian & Cyclist	Speed of vehicles on Ryder	
	Sidewalk failure on Ryder	
	Lack of sidewalk	
	Lack of traffic adherence for Artemis driveway (unpredictable traffic flow, irresponsible driving)	
	Poor sightlines	
	Dangerous entry/exit from bike path	
	Adding alcohol ←→ impaired users	

## Neighborhood Burden

	Seek to understand...	What is the result and suggested best practice?
Parking	Construction worker parking	
	Anticipated parking demand for patrons 100+ seats	
	Anticipated parking demand for staff	
	Anticipated parking demand for entertainment	
	Anticipated ride share/taxi demand	
	Cars idling / idling laws	
	Homeowners rights on a private way in Arlington	
	Ed Burns overflow	
Noise	Construction deliveries	
	Construction days and duration	
	Freight/commercial docking hours	
	Enforcement of dock and dock hours (employing effective freight broker)	
	Size of delivery vehicles	
	Baseline dB levels of abutting residential properties	
	Chiller type (noise rating)	
	Chiller placement and screening	
	Forklift type (noise rating)	
	Pallet Jack (noise rating)	
	Restaurant ventilation (noise rating)	
	Patrons in outdoor/indoor transition areas	
	Patrons inside, operable windows	
	Patrons in beer garden	
	Entertainment inside	
	Entertainment in beer garden	
	Hours of operations	
	Hours for staff closing and transition out	
	Trash and spent grain pick up times	

Lighting	Parking lot	
	Signage	
	Exterior building lights	
	Beer Garden lights	
Smoking	On-site location	
	Patron litter on Ryder	
Odor/ Environmental	Brewing odor mitigation plan	
	Food/Kitchen odor mitigation plan	
	Placement of ventilation systems when surrounded by residences on 3 sides	
	Smoking odors	
	Portable toilet usage	
	Accessible and adequate toilets	
	Adequate protections of abutting waterways	
	Snow removal plan	
Pests	Dumpster types and placement	
	Dumpster pick up frequency	
	Spent grain storage	
	Spent grain patterns (frequency)	
	Spent grain pick ups / Batch size	
Road surface	Disproportional wear on road surfaces that are privately maintained	

## PAPER

## TOXICOLOGY

Teri L. Martin,<sup>1</sup> M.Sc.; Patricia A. M. Solbeck,<sup>1</sup> M.Sc.; Daryl J. Mayers,<sup>1</sup> Ph.D.; Robert M. Langille,<sup>1</sup> Ph.D.; Yvona Buczek,<sup>1</sup> Ph.D.; and Marc R. Pelletier,<sup>1</sup> Ph.D.

## A Review of Alcohol-Impaired Driving: The Role of Blood Alcohol Concentration and Complexity of the Driving Task

**ABSTRACT:** The operation of a motor vehicle requires the integrity of sensory, motor, and intellectual faculties. Impairment of these faculties following the consumption of alcohol has been studied extensively through laboratory, closed-course and on-road driving, and epidemiological studies. The scientific literature was reviewed critically, with a focus on low-to-moderate blood alcohol concentrations (BAC  $\leq$  0.100%), to identify the most reliable determinants of alcohol-impaired driving. Variables such as age, gender, driving skill, and tolerance were shown to have limited impact on impairment. It was concluded the most relevant variables are BAC and complexity of the driving task. The scientific literature provides a high degree of confidence to support the conclusion that a BAC of 0.050% impairs faculties required in the operation of a motor vehicle. Whether impairment is apparent depends upon the complexity of the driving task, which applies to both study design and actual driving.

**KEYWORDS:** forensic science, forensic toxicology, ethanol, automobile driving, psychomotor performance, impairment, blood alcohol concentration, tolerance

It has been recognized for over a century that alcohol (the unmodified term “alcohol” refers to ethanol) produces deficits in the ability to operate a motor vehicle. The relation between alcohol and vehicular death and injury was identified in a 1904 editorial of the *Quarterly Journal of Inebriety*, “The general palsy and diminished power of control of both the reason and senses are certain to invite disaster in every attempt to guide such [automobile] wagons.” (1) The advent of chemical testing, along with increased understanding of the pharmacology of alcohol, subsequently allowed for more specific characterizations regarding the impairing effects of alcohol.

Over 60 years of alcohol research has examined the effects of alcohol on driving through (i) laboratory experiments and driving simulations, (ii) closed-course and on-road driving experiments and (iii) epidemiological studies. From these studies, it has long been observed that alcohol degrades skills required in the operation of a motor vehicle, as well as actual driving performance, culminating in an increased risk of being involved in a motor vehicle collision.

Past research has tended to focus on assessing the scientific merit of *per se* limits. The first *per se* limit was set by Norway in 1936 at a blood alcohol concentration (BAC) of 0.050% (2). In North America, it was more than three decades later in 1969 that Canada legislated a *per se* limit of 0.080% (2). Only recently has the U.S.A. adopted a national standard of 0.080%,

replacing older *per se* legislation of 0.10% (3). This focus on BACs of 0.080% and 0.100% has resulted in a relative paucity of laboratory studies dealing with low-to-moderate BACs (i.e.,  $\leq$  0.100%). As observed by Moskowitz et al. (4), “[l]aboratory examinations of skills performance at blood alcohol levels (BALs) below 50 mg/dL (0.050%) are uncommon in the literature.” *Per se* limits are based on a variety of influences including determinants that are not based on science (e.g., legal aspects, political pressure). Therefore, *per se* limits are not entirely scientific and should not be construed as the threshold BAC at which impairment in the ability to operate a motor vehicle occurs. As concluded by Moskowitz and Robinson (5), “...from a scientific point of view, there appears no lower BAC below which impairment cannot be said to exist. The legislature is free to prohibit driving at any BAC, since such a limit would not contradict the scientific data demonstrating no lower alcohol limit to impairment.”

A number of reviews have summarized the effects of alcohol on driving (6–8). Literature reviews focussing on driving impairment at low BACs have also been conducted (9–11). The advancement and refinement of technology, coupled with political will and societal pressure, has provided scientific research demonstrating impairment at progressively lower BACs. An overwhelming number of scientific investigations now provide convergent evidence supporting impairment of faculties required to operate a motor vehicle at low-to-moderate BACs. The objective of this review is to crystallize the most important concepts in determining the effects of alcohol on driving ability. The relation between BAC and psychomotor impairment is examined, with emphasis on experimental research concerning low-to-moderate BACs. In addition, the role of experimental variables (i.e., complexity of task) and subject characteristics (e.g., age, gender,

217 of 232

<sup>1</sup>Toxicology Section, Centre of Forensic Sciences, 25 Morton Shulman Ave., Toronto, Ontario, Canada.

Received 12 June 2012; and in revised form 21 Sept. 2012; accepted 6 Oct. 2012.

[Correction added on March 25, 2023, after first online publication: Re-use of this article is permitted in accordance with the Terms and Conditions set out at: <http://olabout.wiley.com/WileyCDA/Section/id-828080.html>]



functional tolerance) in predicting alcohol-related impairment of driving skills is critically evaluated.

### Definition of Impairment

Legally, impairment is an issue for the trier of fact to determine. The current Canadian legal authority is provided by the Supreme Court of Canada in *R. v. Stellato* (12), which upheld that impairment comprises "...any degree of impairment ranging from slight to great." Moskowitz (13) defined alcohol-induced impairment as "any change in performance level from that exhibited at zero BAC." For the purposes of this review, impairment is defined as "a decreased ability to perform a task." Specifically, impairment relates to a diminished ability to operate a motor vehicle and is based on an individual's abilities comparable to their own performance in an alcohol-free state. Furthermore, psychomotor impairment, as it relates to driving, is separate from the physical effects produced by alcohol consumption (e.g., slurred speech, diminished coordination, staggered gait). While such effects may be present in an alcohol-impaired driver, they are nonspecific and subjective attributes of "drunkenness" or "intoxication," and are distinguishable from the complex behaviors required for the operation of a motor vehicle. Appreciation of this creates an important distinction between impairment and intoxication: While an "intoxicated" individual will definitely be impaired in their ability to operate a motor vehicle, an absence of *intoxication* should not be taken as an absence of *impairment*.

### Laboratory Studies

Scientific understanding of the specific impairing effects of alcohol on driving performance has evolved primarily from studies conducted in the laboratory, which are far more prevalent when compared with either on-road or closed-course driving experiments. Laboratory studies assess the effects of alcohol on individual, albeit interrelated, skills and faculties relevant to the operation of a motor vehicle including simple and complex reaction time (14), tracking ability (15), and the ability to perform divided attention tasks (14,16). Laboratory studies provide an opportunity for greater control over experimental conditions thereby maximizing internal validity. This promotes confidence that an observed decrement in performance is attributable to an effect produced by alcohol.

#### *Divided Attention and Complex Reaction Time*

A broad range of faculties related to driving have been examined through laboratory studies. The ability to divide attention and complex reaction time (i.e., choice reaction time) has been identified by many investigators as being particularly sensitive to the effects of alcohol. For example, impairment of these abilities has been demonstrated consistently at BACs lower than 0.050% (7,17).

The ability of a driver to divide their attention between multiple operations and sources of information is essential to driving. When impaired, the available attention of the driver is allocated to fewer aspects of driving such as maintaining vehicle position or speed. In the laboratory, divided attention tasks typically combine both tracking skills and a visual search component. In one study, impairment in a divided attention task was observed in all subjects at the lowest BAC tested (0.015%) while impairment of visual information processing was

observed in all subjects at BACs of 0.022–0.035% (4). The degree of impairment in both tasks increased as BAC increased. These findings were supported by a later study that demonstrated impairment in simulated driving and divided attention tasks at BACs as low as 0.020% (18). Other laboratory studies have yielded similar findings, providing reliable evidence that alcohol-related impairment of divided attention tasks occurs at a BAC <0.050% (19–24).

Complex reaction time refers to the time required to respond in a scenario when there are two or more stimuli and several possible responses. A short complex reaction time allows a driver to react quickly and appropriately to emerging situations. When demonstrated experimentally, statistically significant increases in complex reaction time may be measured as millisecond (ms) changes. These increases in response time might therefore be misinterpreted as inconsequential; however, a 100 ms increase in overall reaction time at a driving speed of 100 km/h would result in an additional 2.8 m of distance travelled before a response is elicited. Such an example illustrates the utility of complex reaction time as a measure of alcohol effects given the complex nature of the driving task.

Laboratory studies of complex reaction time following low-to-moderate doses of alcohol demonstrate an increase in both response time (14,25–27) and the number of errors made during the task (19,28,29). The conclusion from such work is that alcohol both slows an individual's ability to respond to a situation and increases the likelihood of incorrect decision-making in the scenario. The role of task complexity in revealing psychomotor impairment relates to the increase in test sensitivity that follows. Accordingly, impairment becomes demonstrable at a lower BAC. One recent laboratory study, which examined response time to visual and auditory stimuli, measured significantly longer reaction times at ethanol doses corresponding to BACs ranging from c. 0.013% to 0.038% relative to an alcohol-free condition (27). In the same study, total response time was significantly slower for a complex reaction time task (where multiple stimuli were present) when compared with a simple reaction time task (single stimulus) during both visual and auditory testing, illustrating the importance of task complexity on impairment of this faculty.

Early recognition of this concept includes Mead (30) who postulated that when attempting to accurately describe the physiological effects of alcohol, "we must find valid and reliable measurements of performances of different complexities." Experimentally, it was observed that a moderate dose of alcohol more readily affected performance on a complex task, requiring intellectual activity, when compared with a simple reflex functioning task (30). Other authors have also emphasized the impact of task complexity in detecting psychomotor impairment. Maylor et al. (14) reported that mean ( $\pm$ standard deviation; SD) BACs ranging from 0.060% ( $\pm$ 0.019) to 0.080% ( $\pm$ 0.020) significantly impaired speed of detection on an auditory stimulus detection task. When concurrently challenged with a tracking task during the stimulus detection test, the impairing effect of alcohol was even greater. Breitmeier et al. (26) examined the effects of a low dose of alcohol (0.3 g/kg) on cognitive functions in 16 healthy males using a double-blind, placebo-controlled design. The authors reported that at a BAC of c. 0.030%, cognitive functions, especially those relying on perception and processing of visual information, were significantly impaired. Neuropsychological impairment became increasingly apparent with increasing complexity and urgency of the task (26).

### Tracking Ability

Tracking, as it relates to the operation of a motor vehicle, can be described as the ability to maintain position on a roadway and is determined by both within-lane lateral position and excursions out of lane. An early demonstration of the effects of alcohol on steering accuracy and lateral lane position was provided by Drew et al. (31) who showed deterioration in these tasks at a BAC as low as 0.020% and a linear increase in impairment with increasing BAC. There was no evidence for a threshold effect, with measurable increases in error at all measurable quantities of alcohol in the blood (31).

In a study of 14 male subjects, Evans et al. (15) observed BAC-dependent decreases in tracking ability, as determined by a pursuit meter task, at concentrations <0.100%. Once again, when the complexity of the task was increased through the addition of a simultaneous distraction task, further impairment was revealed: a mean ( $\pm$ SD) BAC of 0.077% ( $\pm$ 0.008), produced impairment in 13 of 14 test subjects.

Tracking ability is most often assessed by calculating the standard deviation of lane position (SDLP): a composite index that is a measure of weaving, swerving, and overcorrecting. Standard deviation of lane position may be assessed during closed-course and on-road actual driving experiments, but is also well suited to laboratory studies employing computerized driving simulators. Standard deviation of lane position is consistently identified as a particularly sensitive measure of alcohol-related impairment. Significant performance deficits were identified on a lane-change task at a peak BAC of 0.080% by Huemer and Vollrath (32). The impaired performance was primarily attributed to variations in lane keeping (i.e., SDLP), whereas the actual performance during the lane change, that is, deviation from the ideal trajectory was unaffected (32). A number of other researchers have substantiated the relation between alcohol and impairment of tracking ability. For example, Veldstra et al. (33) and Mets et al. (34) conducted independent examinations of the effects of increasing BAC on SDLP with similar results. Veldstra et al. (33) employed BACs of 0.030%, 0.050%, and 0.080% and demonstrated increased SDLP at all concentrations when compared with SDLP following placebo. Similarly, Mets et al. (34) demonstrated concentration-dependent increases in SDLP at 0.050%, 0.080%, and 0.11%. Rakauskas et al. (35) and Fillmore et al. (36) had previously published similar findings. In these studies, subjects with mean BACs ranging from 0.045% to 0.108% demonstrated increased lane position variability accompanied by increased effort as measured by steering variability or steering rate. The impairing effect of alcohol on SDLP was greatest when drivers were challenged by an accompanying distraction condition (35) or increased motivational conflict (36), which provides additional evidence of the importance of task complexity in predicting impairment. Task complexity was evaluated by Rupp et al. (37) in a study that measured lane variability at low BACs under a dual-task condition following a period of extended wakefulness. Impairment was present for all conditions, but performance deterioration was greater during the dual-task condition. Further performance deterioration occurred following an extended period of wakefulness (37). More recently, Harrison and Fillmore (38) examined the interaction between a BAC *c.* 0.080% and driver distraction on simulated driving performance. In this study, driving precision (SDLP) was impaired both with and without the presence of a secondary, distracting task; however, impairment was greater when subjects were required to simultaneously complete a divided attention task. This effect

was measurable as a mean increase in SDLP, relative to placebo, of 5.18 cm for the alcohol condition, compared with a mean increase of 10.97 cm for the combined alcohol/divided attention task condition (38). These studies substantiate a concentration-dependent relation between alcohol and impaired tracking ability at low BACs. Furthermore, these experiments illustrate that impairment is readily apparent when the complexity of the driving task increases. As illustrated by the diversity of these studies, task complexity can be altered by a variety of parameters, for example, motivation, distraction, increased mental workload, extended wakefulness, the latter being a concept described by Ogden and Moskowitz (8) who stated "the effects of alcohol are dependent both on the quantity consumed and the nature of the performance required."

### Other Faculties

Other faculties relevant to driving that have been studied via laboratory experiments and demonstrated impairment at low-to-moderate BACs include: driving speed and acceleration (36), vision and eye effects [e.g., glare recovery time (39)], saccadic and smooth pursuit eye movements (40,41), peripheral attention (42), and vigilance (21,25,43–46). Significant impairment due to alcohol has been consistently demonstrated in critical tracking (21,25,47–49). The critical tracking task is described as a measure of a "subject's ability to concentrate attention and to integrate visual input with motor skills" (21).

Hazard perception latency, which includes elements of complex reaction time, has also been evaluated (50,51). For example, Deery and Love (51) assessed the perception and reaction to a hazard in 32 drivers. The authors reported that at BACs ranging from 0.040% to 0.060%, subjects took longer to detect a hazard, and when subjects did react, they reacted more abruptly. Similarly, West et al. (50) demonstrated that BACs between 0.025% and 0.075% produced a detectable increase in traffic hazard latency in a double-blind, placebo-controlled simulator study.

Laboratory research is an invaluable means of identifying alcohol-related changes to the faculties that are integral to the operation of a motor vehicle. Such research has also contributed to elucidating the mechanisms by which the alcohol-related changes occur. It is beyond the scope of the current review to describe these mechanisms in detail; however, other researchers have provided such explanations. For example, Howat et al. (52) described a model for driving that comprised four phases of information processing: (i) seeing the situation develop, (ii) recognizing the stimulus, (iii) making a decision and (iv) executing an appropriate response. A comprehensive explanation of the automatic and controlled processes required for driving was described by Harrison and Fillmore (38): automatic processes were defined as the over-learned aspects of driving that require little conscious effort; controlled aspects of driving were defined as those that demand greater cognitive resources.

Despite the important contributions of laboratory-based research, there are limitations associated with these types of experiments. The operation of a motor vehicle is a complex task requiring the coordination of a variety of interdependent processes that occur nearly simultaneously. Laboratory simulations typically represent only one aspect of actual driving and require extrapolation of the effects as a predictor for other secondary tasks. Because the complex nature of driving cannot be characterized by a single variable, those studies that focus on a single measure of driving behavior may fail to demonstrate statistically significant impairment due to alcohol. Perrine (53) also

identified that determining BAC limits for driving on the basis of laboratory experimentation alone risked setting limits that were too high due to insufficient complexity of experimental situations and the unnatural situation that a laboratory study presents. Starmer (54) judged insufficient complexity as the underlying cause of a “credibility gap” between what can be demonstrated experimentally and what has been observed empirically when crash-risk assessments are performed. As excerpted from Starmer (54), “unexpected high demand situations which might be intuitively considered to be more sensitive to the deleterious effects of alcohol are infrequent and difficult to duplicate in the laboratory or on the track.”

Recent laboratory studies have attempted to simulate the complexity of actual on-road driving situations by incorporating distractions and multiple task requirements in their simulated driving experiments (17,35,38). Nevertheless, the experimental context of the laboratory study remains a limitation of this type of work. Even the most sophisticated driving simulators can only approximate actual driving conditions and suffer from lack of car dynamics and inadequacy of the visual scene. Furthermore, although a driving simulator may better model complex driving scenarios when compared with laboratory psychomotor tests, simulated driving is subject to both over- and underestimation of driving performance. Underestimation of performance, as noted by Fillmore et al. (36), can arise because simulated driving does not engender the same degree of driver motivation as actual driving, given that there are no salient consequences, such as risk of personal injury. Conversely, overestimation of performance can arise from the absence of distractions typically encountered in real-life driving (36,38). As observed by Harrison and Fillmore (38), technology-based distractions such as mobile telephones, global-positioning systems, and entertainment systems overburden the capacity of a driver's attention. The removal of such elements from simulated driving studies is critical to ensuring experimental control, which is the primary benefit of laboratory-based research, but may reduce the relevance of the study to actual on-road driving.

While it is important to identify the limitations of laboratory-based studies, such analysis should not detract from the overall contribution of the research. Through the resulting scientific literature, the effects of alcohol on driving have been described and the importance of BAC and task complexity is buttressed further. The limitations of laboratory research illustrate why these studies alone should not provide the basis for an opinion concerning the effects of alcohol on driving. Rather, such opinions should consider the results of a variety of types of investigation: laboratory, closed-course, on-road, and epidemiological, to increase the scientific confidence associated with the opinion.

### Closed-Course and On-Road Driving Studies

Studies assessing the impairing effects of alcohol in subjects during the actual operation of a motor vehicle have high external validity; results from these studies generalize readily to actual driving behavior. For safety reasons, these studies are typically conducted on a closed course. Attempts to simulate realistic driving conditions have been made through the introduction of unpredictable or emergency traffic situations. For example, Laurell (55) incorporated a “surprise situation” on a closed course comprising the sudden appearance of a human-shaped obstacle blocking the roadway. Half the participants in the alcohol group (BACs <0.050%) collided with the obstacle, whereas only one participant in the control group was unable to stop safely.

Subjects were also tested in an emergency situation, which required an evasive maneuver to be elicited in response to the appearance of two red brake lights. Significant increases in number of pylons hit and stopping distance were observed, including for 10 subjects with BACs ranging from 0.024% to 0.042%. Thus, the results of this study provide direct evidence of alcohol impairment on driving performance in emergency situations at a BAC <0.050% (55).

Closed-course studies often assess the effects of alcohol on the integrated task of driving and define impairment based on an aggregate degradation in the driver's skill. A review of closed-course studies on alcohol and driving impairment conducted between 1950 and 1973 by Huntley (56) noted most studies relied on human observation and judgment and performance measures such as number of pylons upset, elapsed time on course, or smoothness of operation. It was reported that alcohol changes driving behavior “on almost all dimensions observed,” including increased time to complete tasks, increased number of pylons upset, decreased stopping accuracy, decreased driving smoothness, and decreased cornering ability. Nevertheless, Huntley (56) concluded that the effects of alcohol on psychological components of driving required further study. Alcohol-related impairment of intellectual function, that is, decision-making related to driving, has received comparatively less attention in closed-course and on-road studies. A notable exception is that of Cohen et al. (57) who evaluated the effects of alcohol on risk-taking and judgment. Subjects were experienced bus drivers whose skill had been recognized previously with awards for safe driving. Three groups of drivers (control, low dose, moderate dose) were required to predict their success in driving their bus through gaps of various widths. While all subjects, including the control group, attempted to drive their bus through gaps narrower than the width of their bus, as drivers consumed more alcohol, they were prepared to drive through increasingly narrower gaps. Thus, while there was underestimation of the risk associated with a driving task, driving ability was overestimated and even in elite drivers judgment was deemed impaired at BACs below 0.050%. The closed-course driving performance of professional drivers at low-to-moderate BACs was also assessed by Lovibond and Bird (58). In this study, a composite score representing general driving performance and using measures such as braking reaction distance was calculated. Impaired performance was established for both “ordinary drivers” as well as for 16 drivers with racing and rally competition experience at BACs of 0.050%, 0.080%, and 0.100%. The authors concluded their results are “consistent with the conclusion derived from laboratory studies that any measurable quantity of alcohol in the blood impairs, in some measure, the skills important for driving” (58).

A broad approach, examining the effects of alcohol on overall driving performance, was employed by Robbe (59) who conducted an on-road experiment on city streets open to other traffic. Studies of this type are rare, due to safety concerns, but have a high degree of external validity given that they encompass the high degree of complexity that is associated with the typical driving environment, for example, traffic, pedestrians, traffic signals. It was concluded that even a modest dose of alcohol, yielding a BAC of *c.* 0.040%, produced significant impairment in 16 drivers when compared with their alcohol-free performance. Specific areas of deficiency were identified as vehicle handling and traffic maneuvers.

Other closed-course and on-road driving studies demonstrating impairment have focussed on car-following tasks as a dependent variable (49,60–62). Although a stopping task was found to be



insufficiently sensitive to identify intoxicated drivers, Attwood et al. (60) reported that the car-following task was highly sensitive to any amount of alcohol, with all subjects classified as impaired at an average ( $\pm$ SD) BAC of 0.033% ( $\pm$ 0.008).

Closed-course studies have provided complementary data to the numerous laboratory studies that identify SDLP as sensitive to the impairing effects of alcohol (49,61–64). Gawron and Ranney (64) employed a within-subject, placebo-controlled design to evaluate the performance of six males on a closed-course and a driving simulator. Alcohol effects observed on-road replicated the effects observed in the laboratory: tracking skills, lane-departure frequency, and SDLP were significantly impaired at a BAC of 0.070%. Kuypers et al. (49) documented significant increases in SDLP at a mean ( $\pm$ SD) BAC of *c.* 0.030% ( $\pm$ 0.010) 4 h postalcohol administration. Another example, which confirmed the sensitivity of SDLP as a measure of alcohol impairment and further explored the concentration-dependent nature of the relation, includes Louwerens et al. (63) who observed an exponential increase in SDLP with increasing BAC, with significant impairment at a BAC of 0.060% and greater. The impairing effects of alcohol on SDLP are sufficiently well established that ethanol has been employed as a positive control against which to compare the effects of other drugs on human psychomotor performance (65).

In contrast, the effects of alcohol on driving speed are equivocal. Numerous authors have indicated that despite the clear effect of BAC on the ability to maintain lane position, speed perception, and speed control are far less sensitive to the effects of alcohol. For example, West et al. (50) failed to demonstrate a significant effect of alcohol on the time required to drive a fixed route. Kearney and Guppy (66) also reported a lack of significant effect of alcohol on perception of speed in a closed-course driving study despite an average ( $\pm$ SD) subject BAC of 0.095% ( $\pm$ 0.004). Laboratory studies measuring driving speeds have largely concurred with these observations (33,67), although Mets et al., (34) measured a significant increase in standard deviation of speed once a BAC of 0.080% or higher was attained. A simulated driving study conducted by Veldstra et al. (33) also demonstrated no significant effect of alcohol on driving speed when faced with a highway or a rural road condition; however, an urban driving task did produce significant increases in driving speed at BACs of 0.050% and 0.080%. This finding may reflect increased driving complexity in the urban condition resulting in increased sensitivity of the measure (33).

In a recent study, Harrison and Fillmore (38) concluded that the *absence* of a significant effect of alcohol on driving speed may itself represent impairment. Forty adults were tested on a driving simulator with and without the presence of a distraction task under alcohol or placebo. Blood alcohol concentrations of *c.* 0.080–0.090% did not significantly alter the driving speed of participants; however, drivers under placebo slowed their driving speed significantly when faced with the additional burden of the distraction task (38). The authors postulated that the alcohol group may have failed to adopt compensatory strategies to mitigate increased task complexity. This may be the result of an overestimation of driving ability while under the influence of alcohol.

Therefore, while it may be concluded that speed perception and speed control are relatively resistant to impairment at low-to-moderate BACs, caution must be applied to this interpretation. The limitations of closed-course and on-road driving studies may be a factor. Due to ethical and legal considerations, studies of this type are typically conducted at limited speeds [e.g., maximum

50 km/h (66)]. Similarly, on-road highway and rural driving studies, and the speeds associated with such driving are rare, resulting in a lack of information regarding the effects of low-to-moderate BACs on driving speed in these environments. Robbe (59) and Ramaekers et al., (62) have conducted on-road research in normal traffic but identify the potential for reliance on the accompanying instructor and redundant vehicle controls as a possible artifact of this type of experiment.

In addition to slow driving speeds, legal and ethical considerations produce another potential experimental confound in this type of research. Task complexity is often limited by the removal of interactions with other vehicles, hazards, and pedestrians (63). Additionally, on-road driving studies in actual traffic may be limited to alcohol concentrations below established *per se* limits. Finally, for practical reasons and to ensure a relatively stable BAC throughout the test sequence, the duration of driving may be short and may not adequately address the vigilance requirements of real-world driving. Even with this limitation, on-road driving studies may more realistically reflect the increased complexity of actual traffic situations as demonstrated in the results reported by Parks et al. (68), who observed significantly increased SDLP in 60 subjects taking part in an on-road driving study where BACs averaged 0.042% before driving and dropped to an average of 0.031% after driving.

Given these constraints, it is clear that closed-course and on-road driving studies are likely to underestimate the true differences between driver performances under the influence of alcohol versus an alcohol-free state. The foundation for this conclusion includes (i) modification of subject behavior due to awareness of participation in an experiment (“Hawthorne effect”) and (ii) decreased task complexity due to practice effects, as identified by Laurell (55). Milner (69) asserted the following, “it is likely that if a drug is shown to affect driving skills in an experimental situation, its effects are probably even more pronounced in general driving behaviour.”

## Epidemiological Studies

Epidemiologic data assess the probability of a driver’s involvement in a motor vehicle collision as a function of BAC. These studies have consistently demonstrated an exponential increase in crash risk with increasing BAC.

The Grand Rapids study (70), the most extensive and widely recognized study of this type, compared drivers in 5988 motor vehicle collisions with 7489 control drivers. Borkenstein et al. (70) concluded, “[b]lood alcohol concentrations (BACs) over 0.040% are definitely associated with an increased accident rate.” Specifically, the risk of causing a crash was *c.* 2, 3, 7, and 25 times greater at BACs of 0.060%, 0.080%, 0.100%, and 0.150%, respectively, when compared with control drivers. More recent epidemiological studies (71,72) have reproduced the risk function previously described by the Grand Rapids data. For example, Blomberg et al. (72) employed a case-control design, similar to the method employed in the Grand Rapids study, in examining drivers in 4919 motor vehicle collisions compared with 10,066 control drivers and demonstrated a statistically significant increase in crash risk at 0.040% with an exponential increase occurring from 0.100% onward (72).

Even greater crash risks have been observed for collisions resulting in fatal injury to the driver, which have been shown to become significant at a BAC of 0.020% (73). At BACs ranging from 0.050% to 0.079%, the relative risk of driver fatality is increased to *c.* 3.6–17 times that of alcohol-free drivers (73).



The increased risk observed in studies of alcohol in fatal collisions likely reflect the decreasing ability to process and to respond to highly complex driving situations, even at low BACs.

Recently, Phillips and Brewer (74) examined injury severity for nearly 1.5 million crashes reported to the Fatality Analysis Reporting System (FARS) from 1994 to 2008. Using the ratio of serious to nonserious injuries as a measure, the authors reported that BACs of 0.010% and greater were associated with increasing collision severity when compared with alcohol-free drivers. The authors further reported that at a BAC of 0.010%, drivers were “significantly more likely to speed, to be improperly restrained and to drive the striking vehicle.” These conditions were associated with more severe motor collisions. Connor et al. (75) also reported increased crash risk at very low BACs. In a case-control study of drivers involved in crashes in which at least one occupant of the vehicle was killed or hospitalized, driving with BACs of 0.003% to 0.050% were associated with more than 10 times the risk of driving with no measurable alcohol. Drivers with a BAC > 0.050% had 40 times the serious injury crash risk (75).

Epidemiological studies can be limited by missing data (e.g., BAC, case history), lack of appropriate controls, and potentially confounding variables such as age, seatbelt use, and time of day. Failure to account for these variables can produce a distorted relative risk probability curve, the most notable example being the “Grand Rapids Dip,” which at face value implies a decrease in relative crash risk at BACs between 0.010% and 0.040%. Further analysis has established the Grand Rapids Dip to be an artifact of single variable analysis (76). Subsequent epidemiologic studies that control for covariates (e.g., gender, age, education, ethnicity, employment status, vehicle type) in their analyses have also negated the implied decrease in risk of collision at low BACs (72,73,75).

Analysis of crash statistics following the revision of *per se* alcohol limits can reveal whether the change has been effective in reducing the number of alcohol-related crashes. Where a reduction in crashes occurs, an inference can be made that higher BACs contribute to a larger fraction of alcohol-related motor vehicle collisions. For example, comparisons among alcohol-related crash statistics in jurisdictions with differing *per se* limits indicated that a reduction in the permissible BAC from 0.100% to 0.080% (or lower) was effective in reducing alcohol-related crashes (77–79).

## Subject Variables

There is variability in the impairing effects of alcohol: Faculties are not impaired equally, and study participants do not demonstrate equal impairment (15,31,36). One study reported that while 82% of subjects exhibited “highly significant deterioration in driving performance” after alcohol consumption when compared with their control performance, 9% of the subjects showed no difference, and an additional 9% apparently improved (80). Sources of variability in alcohol-related impairment can be attributed to experimental design, for example, dosing regimen, task complexity, and practice effects. As noted by Flanagan et al. (80), the drivers in their study were not permitted to become too familiar with the course prior to testing and therefore would be expected to improve their performance on their second run, which represents a confound in differentiating between continued learning and the effects of alcohol on driving.

Other sources of variability that must be considered are subject variables such as age, gender, tolerance to alcohol, and driving proficiency. Practically, these variables may be offered in courts of law as factors that reduce the extent of impairment at a measured BAC. Thus, it is essential that the impact of subject variables on alcohol-related impairment be reviewed.

## Age

Motor vehicle crash and fatality risks are greatest for young and elderly drivers with these populations exhibiting a higher risk of collision even in the absence of alcohol consumption (72,81–83). Young drivers in particular have been shown to contribute to a disproportionate number of road and traffic collisions. In 2001, drivers under the age of 25 accounted for *c.* 25% of all U.S. traffic fatalities (84).

Increased risk of crash involvement for younger and older drivers is not due to a greater impairing effect of alcohol in these groups, but to the presence of additional risk factors. In the aged, the diminution of psychomotor faculties (e.g., vision, attention, perception/reaction time) has been offered as explanation of increased risk of collision (85,86). Simulated driving studies illustrate the influence of age on driving performance with and without the administration of alcohol. Quillian et al. (87) observed that male drivers over age 60 performed more poorly when compared with drivers between 30 and 50 years. Even in an alcohol-free state, the older drivers drove more slowly, had more variable speed, failed to stop fully when required, had more crashes, and braked inappropriately. More relevant to this review, however, the authors observed that all subjects were impaired by the consumption of alcohol to a similar degree, regardless of age. Similar conclusions were reached by Tupler et al. (88) who demonstrated that subjects aged 59–65 years, performed more poorly than middle-aged and young subjects when alcohol-free. After the ingestion of alcohol, impairment of the magnitude of observed performance decrements was the same across all age groups.

In young drivers, higher crash risk has been attributed to, among other factors, adolescent characteristics such as increased risk-taking behavior (89,90). Following the consumption of alcohol, young drivers are at even greater risk relative to the risk of alcohol-related collision for mature drivers (73). Blomberg et al. (72) observed a trend toward increased crash risk in drivers under 21, which was not statistically significant. Subsequent reanalysis of these data by Peck et al. (91) concluded the interaction between age and BAC was statistically significant. The interaction was described as a synergistic effect whereby the relation between BAC and crash risk was greater for drivers under 21 years of age when compared with older drivers. Estimated relative risks for drivers in this age group were elevated at all BACs, even those as low as 0.010%. For example, the relative risk of crash involvement for drivers under 21 years of age was 1.13, 2.75, and 16.0 at BACs of 0.010%, 0.050%, and 0.100%, respectively. In comparison, for drivers aged 21 and older, the relative risks were 0.94, 1.07, and 2.43 at the same BACs (91).

A review by Mayhew et al. (89) considered the mechanism for the combined effect of age and alcohol consumption on increased crash risk. The authors concluded there was little support for the hypothesis that higher crash risk in young drinking drivers is due to inexperience with drinking, driving, or a combination thereof. Rather, they proposed that young drivers are more likely to undertake certain risky behavior, and the

consumption of alcohol further contributed to the likelihood of risk-taking behavior. This effect may result in an increased likelihood that young drivers will attempt maneuvers beyond their capabilities (89). It has been reported that for drivers aged 15–20 years, a greater percentage of their fatal motor vehicle collisions occur at night, without the use of seat belts, and with positive BACs, relative to other age groups (92). Williams et al. (93) recently reviewed the circumstances of fatal motor vehicle collisions between drivers aged 16 and 17 years and concluded that alcohol-positive drivers were “more likely than those in non-alcohol-related crashes to be male, unbelted, in single vehicles, and speeding, and their collisions were more likely to have occurred on Saturday or Sunday, at night, and when passengers were present” (93). Such analyses demonstrate the risk-taking behavior that is inherent to crash risk in young drivers; behavior that may be exacerbated by the presence of alcohol.

Unfortunately, experimental studies typically fail to provide information on alcohol and driving impairment in the youngest of drivers (i.e., ages 16–18) given the legal and ethical constraints of administering alcoholic beverages to individuals who have not yet reached minimum legal drinking age. This gap in the experimental research prevents full elucidation of the source of elevated crash risk among young drivers. Where research is conducted in areas bound by higher minimum legal drinking ages, (e.g., 21 years of age in the U.S.A.), an even larger gap may exist.

Preventative measures such as legal restrictions on alcohol consumption and deterrents such as zero-tolerance laws for alcohol in novice drivers have been shown to be effective in reducing crashes among young drivers (94). Much like reductions in alcohol-related crashes following the establishment of lower *per se* limits a reduction in alcohol-related crashes among young people after the introduction of these deterrents suggests that alcohol is a contributing factor to these crashes.

### Gender

Systematic investigation of the effect of gender on alcohol-related psychomotor impairment is rare, and the results of the available studies are equivocal. Data from epidemiological studies infer a reduced crash risk for female drivers when compared with male drivers; however, laboratory experiments of the behavioral and cognitive effects of alcohol are inconclusive regarding gender-based differences in impairment. Miller et al., (95) compiled findings from seven previous studies that investigated gender differences and the effects of BACs between 0.067% and 0.088% on a variety of driving-related tasks. While the results of this study demonstrated that impairment was present in both men and women on all tasks at the BACs tested, it was evident that women displayed greater alcohol-related impairment when compared with men on dependent measures such as simulated driving, tracking ability, divided attention, and speed of information processing (95). Given that women exhibited significantly higher SDLP when compared with men at an equivalent BAC, Louwerens et al., (63) also postulated that women were more sensitive to the behavioral effects of alcohol.

In contrast, Friedman et al. (96) found that the male participants in their study displayed greater impairment than a group of women at approximately the same BAC. Low BACs (*c.* 0.050%) were achieved prior to laboratory testing of cognitive processes in a group of 10 men and compared with the performance in 11 women. All participants demonstrated higher error rates in the alcohol condition relative to placebo; however,

the increase in error rate was statistically significant only for the males.

Other studies have revealed no significant contribution of gender to alcohol-related impairment of driving-related skills. A relatively large study of 84 men and 84 women provided no evidence of differential alcohol effects between gender groups. (18). Parks et al. (68) reported no effect of gender on actual driving performance or laboratory tests of divided attention, and Drew et al. (31) reported that the effects of alcohol in 40 volunteers tested on a driving simulator were not affected by differences in age, gender, previous driving experience, or drinking habits. Because the subjects in this study varied widely in their initial driving skill, a within-subject design was used to control individual differences in driving skill (31).

Data from some epidemiological studies, including the Grand Rapids Study, suggest reduced crash risks for female drivers when compared to male drivers at similar BACs (70,73); however, the Grand Rapids Study revealed “females to be over-represented in accidents when no alcohol was present” resulting in an *apparent* reduced risk of crash involvement when alcohol was present (70). Peck et al. (91) provided relative-risk curves for males and females that were “almost identical” thus neither males nor females had any greater risk of being involved in a collision when compared to the other group. Connor et al. (75) also indicated that the gender of the driver had little effect on risk of serious injury crash.

Zador (97) observed a significant increase in fatality risk in young drivers and female drivers with positive BACs when compared with male drivers, 25 years of age and older; however, fatality risk increased in all groups when compared to control drivers. Accordingly, the authors concluded that “BAC is the most important factor followed by age and then by the BAC-by-sex interaction” (97).

### Driving Proficiency

For many adults, the operation of a motor vehicle is performed on a daily basis and has been described as an over-learned, automatic behavior. Moreover, large interindividual differences in driving proficiency are inevitable in the population. It might be assumed that the ability of a highly experienced driver would be less disrupted by the impairing effects of alcohol. As suggested above, this has been postulated for young drivers, where driving inexperience has been often cited as the possible cause of increased crash risk following the consumption of alcohol.

Few studies have specifically included driving skill as a consideration in the experimental design. Where this variable has been considered, there is little support for the hypothesis that driving proficiency can overcome alcohol-related impairment. Harrison and Fillmore (98) documented wide variations in the baseline performance of subjects on a simulated driving task. Following the administration of alcohol sufficient to achieve a target BAC of 0.080%, participants demonstrating the greatest decrease in driving precision, as measured by within-lane deviation, were those with the highest deviation scores while alcohol-free (98). Although only two measures of driving skill were assessed (speed and within-lane deviation), the authors suggested that poor baseline driving skill can intensify alcohol-related impairment on a driving-related task. Nevertheless, it should be noted there was a significant increase in within-lane deviation for the alcohol group relative to their pretreatment score *in general*, illustrating that alcohol produced impairment in this measure regardless of driving proficiency.

A review of the literature conducted by Mayhew et al. (89) concluded that alcohol decreases driving performance in both skilled and less-skilled drivers to the same extent. Lovibond and Bird (58) also demonstrated that increasing BACs resulted in progressive impairment of driving performance even among a group of skilled race and rally car drivers. Although the overall driving performance of the experienced, competitive drivers was superior relative to the noncompetition drivers, both groups showed a marked deterioration in driving performance at a BAC of 0.050%.

It is important to note studies such as these have shown that while the *relative* proficiency of the drivers may be retained pre- and postalcohol consumption, impairment is still demonstrable in both groups following the administration of alcohol. It follows that an alcohol-impaired “skilled” driver may perform better than an alcohol-impaired “lesser-skilled” driver. The most important consideration is that alcohol impairment is still present relative to the individual’s ability at a zero BAC.

When assessing the likelihood of impairment in an individual, it is apparent that subject variables such as age, gender, and driving proficiency are of limited value. The most reliable objective determinant of alcohol-related psychomotor impairment is BAC. It is also important when assessing the impairment of an individual to compare their performance to their own alcohol-free performance and not to the performance of other drivers.

### Functional Tolerance

Under controlled experimental conditions, it has been demonstrated that repeated performance of certain tasks while under the influence of alcohol may make a subject less sensitive to the effects of alcohol, referred to as drug state-dependent learning (99) or functional tolerance.

Repeated consumption of alcohol may increase the functional tolerance of an individual resulting in a decreased response to the drug. Behavioral research concerning functional tolerance has demonstrated a relation between the expectation of drug effects, compensatory responses, and the consequence of the compensatory responses. For example, an experienced drinker can predict with assurance the effect that alcohol will produce (expectation), and if the consequence of the compensatory response is desirable, it is more likely to occur, for example, successfully operating a motor vehicle (99). While this theory may, in part, explain observations that individuals can display a high degree of tolerance to some effects of alcohol and no tolerance to other effects, the literature is equivocal. Criticisms include reliance upon anecdotal reports, variable definitions for heavy drinking, experimental confounds (e.g., different degree of novelty of test settings), and poor external validity (e.g., research performed using dogs or rats).

One study reported that heavy drinkers demonstrated less impairment of driving performance when compared with light drinkers at similar BACs; however, as the authors observed the driving task was well practiced, the attitude of the subjects was similar to that of a competitor in a game, and speed never exceeded 24 km/h (100). The authors concluded “that under actual driving conditions ... the effect of alcohol on driving performance would be more pronounced than that observed in these experiments.”

Therefore, while impairment from alcohol may not be evident during simple repetitive tasks, where the outcome is predictable and desirable, it is highly unlikely that functional tolerance to alcohol exists in tasks that require a rapid response in unexpected

situations. King and Byars (101), in their laboratory study of performance effects of low and moderate BACs in heavy episodic and light social drinkers, noted that impairment was generally comparable between the two groups. Where exceptions were noted, the authors speculated that heavy drinkers may have gained tolerance at a given BAC to tasks of a stereotyped and repetitive nature, but did not acquire tolerance to more complex tasks, such as those requiring the processing and execution of new strategies. The authors concluded that an “assumed lack of performance impairment ... in regular heavy alcohol consumers is not valid” (101).

In a review of the literature concerning functional tolerance to alcohol and driving, DeLuca (102) concludes “the most that can be said is that at moderate BACs (50–70 mg/100 mL [0.050–0.070%]) more frequent drinkers appear to be less effected [*sic*] and at a lower risk of accident involvement. At higher BACs, the risk of an accident and the impairment of driving ability were both significantly increased in all drinking frequency groups.”

There have been reported differences in a drinker’s ability to accurately self-assess their degree of impairment dependent upon their classification as a social or heavy drinker. Brumback et al. (103) demonstrated that habitual binge social drinkers show comparable alcohol-induced behavioral impairment, but less self-rated perception of impairment when compared with light social drinkers. The authors concluded that experienced habitual binge drinkers might have false beliefs in their functional tolerance to the psychomotor impairing effects of alcohol. Other researchers have studied similar populations with similar findings. In one study, binge drinkers and nonbinge drinkers were found to be impaired in multiple aspects of driving performance; however, the binge drinkers overestimated their driving ability following alcohol consumption (104). In this study, lack of awareness to the decreased ability to perform the driving task was attributed to a decrease in self-reported sedation among the binge drinkers; a potential cue to impaired psychomotor skills.

In a simulated driving study designed to determine the influence of binge-drinking frequency on driving behavior and subjective ratings of impairment, Bernosky-Smith et al. (105) observed a significant increase in the number of collisions and number of center-line crossings at a BAC of *c.* 0.080% regardless of binge-drinking history. High-frequency and low-frequency binge drinkers were observed to make a similar number of errors on a test of simulated driving following an alcohol binge that produced a BAC of *c.* 0.080%.

The existing scientific evidence supports the concept of functional tolerance, albeit only at low-to-moderate BACs, and in simple, predictable driving tasks. Additionally, because functional tolerance is learned through practice, it can be inferred its acquisition would require the repeated operation of a motor vehicle at the same BAC, and repeatedly facing driving tasks with predictable complexity. Functional tolerance cannot mitigate the impairing effects of alcohol when the driving task is complex and unpredictable.

### Acute Tolerance

Acute tolerance (i.e., the Mellanby effect) is defined as reduced sensitivity to the effects of alcohol on the descending limb of the BAC curve relative to the ascending limb of the BAC curve. Acute recovery results in reduced subjective ratings of intoxication and/or reduced physical signs of intoxication (e.g., slurred speech, ataxia, motor incoordination) when the



BAC is declining (106,107). Consequently, it has been postulated that psychomotor impairment may also be mitigated if a subject is in the elimination phase of the BAC curve. Supporting evidence for this hypothesis would have implications in the application of laboratory and driving studies to forensic casework. Studies performed in test subjects on the descending limb of the BAC curve may underestimate the alcohol-related impairment potential of a given measure. Conversely, because the vast majority of drinking drivers have been demonstrated to be in the plateau or postabsorptive phase of the BAC curve at the time of driving (108), impairment studies confined to the ascending limb of the BAC curve may overestimate impairment at the time of arrest/incident. Some researchers have controlled this variable by ensuring that study participants are in the postabsorptive phase at the time of testing (4,49,50,55,62). Parks et al. (68) documented significant increases in the time required to respond to a peripheral stimulus during a laboratory-based divided attention task and similarly observed significant increases in SDLP in an on-road driving study comprising 60 subjects despite a decline in the mean BAC from 0.042% to 0.031% throughout the exercise, that is, in the presence of acute tolerance. In such experiments, an observed decrease in ability to perform a task will likely underestimate alcohol-related impairment when the BAC is rising.

The relevant scientific literature provides little support for acute tolerance ameliorating psychomotor impairment on the descending limb of the BAC curve. As is the case for the functional tolerance characteristic among chronic, heavy users of alcohol, motor-based skills appear to be most subject to acute tolerance effects. As such, acute tolerance to the impairing effects of alcohol on the complex faculties and cognitive processes inherent to the operation of a motor vehicle has not been described (28). Fogarty and Vogel-Sprott (109) observed impairment of a motor skill task was subject to acute tolerance, but that an information processing task was equally impaired at equivalent BACs on both the ascending and descending limbs of BAC curve. Schweizer et al. (110) found that errors during an information processing task remained in the postabsorptive phase despite a return in the speed of information processing to alcohol-free levels.

In an examination of how acute tolerance affects the ability to self-evaluate intoxication, Cromer et al. (111) demonstrated that although individuals report lower levels of subjective intoxication on the descending limb of the BAC curve compared with equivalent BACs on the ascending portion of the curve, executive functions (i.e., error monitoring and spatial working memory) were equally impaired. Thus, while perceived level of intoxication may be subject to the development of acute tolerance, there is continued impairment in the ability to perform complex decision-making and problem-solving tasks. The authors concluded that "subjective perception of intoxication is a poor indicator of sobriety and the ability to operate a motor vehicle" (111). Weafer and Fillmore (107) corroborated this work in a driving simulator study that demonstrated diminished driving precision in response to alcohol (demonstrated by increased within-lane deviation and steering rate) on both the ascending and descending limbs of the BAC curve. Acute tolerance to alcohol on driving performance was not observed; however, motor coordination and perceived intoxication ratings showed marked acute tolerance. Consequently, willingness to drive was greater when subjects were on the descending limb of the BAC curve compared with the ascending limb.

Marczinski and Fillmore (112) similarly reported that although acute tolerance to the subjective effects of alcohol occurred

among binge drinkers, this was not accompanied by acute tolerance to the impairing effects of alcohol. Indeed, driving errors such as within-lane deviation, number of center-line crossings and number of excursions to the driving shoulder actually increased on the descending limb of the BAC curve relative to the ascending limb. The authors concluded that while the "intensity of subjective intoxication diminished under dose ... the intensity of the impairing effect of alcohol on driving performance increased."

The most relevant aspect of acute and functional tolerance in drinking drivers may be the accompanying reduction in self-rated perception of intoxication. Psychomotor effects such as motor incoordination, clumsiness, and sedation are highly salient cues that may be influential in the decision whether to drive following the consumption of alcohol. Thus, while acute and functional tolerance has negligible impact on mitigating performance decrements, there may be effects on self-perception of intoxication and willingness to operate a motor vehicle.

Self-rated perception of intoxication may also be affected by the concurrent administration of stimulant drugs. A number of researchers have examined the effects of caffeinated "energy" drinks mixed with alcohol on subjective reports of intoxication and objective measures of impairment (e.g., information processing, motor coordination, visual reaction time) (113–115). The dominant finding of this research is that, subjectively, coadministration of an energy drink reduces mental fatigue and diminishes feelings of intoxication, but there is no amelioration of alcohol-caused deficits in the objective measures. Others have performed similar experiments where the combined effects of ethanol and the stimulant drug methylenedioxymethamphetamine (MDMA, "Ecstasy") were examined (33,49). Again, the coadministration of the stimulant drug was found to counteract the subjective ratings of impairment due to alcohol, but was insufficient to overcome the psychomotor impairment objectively determined by measures such as a car-following task. Participants in the study by Veldstra et al. (33), for example, rated their driving performance as only "slightly worse" than normal, when in fact, the quality of driving had "seriously deteriorated" with significant increases in SDLP at each target BAC (0.030%, 0.050%, and 0.080%) compared with placebo. Similarly, at a BAC of 0.050%, 29 subjects who took part in an on-road driving test performed significantly worse than they did while alcohol-free in spite of reporting no change in alertness before driving and no change in driving quality or mental effort after driving (116).

These reports underscore the unreliability of subjective ratings of intoxication as a measure of impairment while supporting BAC as the most appropriate predictor of driving impairment. Subject variables affecting self-perceived intoxication, including functional and acute tolerance, cannot be offered as viable explanations that a person was unimpaired by alcohol at a given BAC. Impairment has been repeatedly demonstrated to exist independent of subjective feelings of intoxication. Moreover, in one closed-course driving study, 76% of the subjects who had shown deterioration after consuming alcohol actually thought that their driving had improved (80).

## Discussion

The findings from laboratory, on-road, closed-course, and epidemiologic studies should be used to crystallize the most important concepts regarding the effects of alcohol on driving performance. Laboratory-based studies have played a critical role in isolating mechanisms for impairment; epidemiological studies



have demonstrated the impact of these deficits on crash risk; and on-road and closed-course studies have provided empirical evidence as to the effects of alcohol on the integrated task of driving. When the totality of the scientific evidence is assessed the weight of most subject variables is diminished and the relevant variables are reduced to BAC and complexity of the driving task. The effects of alcohol on driving impairment can thus be reduced to the following two points:

- as BAC increases the magnitude of impairment also increases,
- whether impairment is observable depends upon the complexity of the task.

With respect to task complexity, the key issue is not whether the person is impaired from a scientific perspective, but simply whether the impairment is observable. Moreover, given the inherently complex and unpredictable nature of the operation of a motor vehicle, it follows that the single most important predictor of impairment is BAC.

Previous reviews have reached analogous conclusions (11,53,56). For example, Perrine (53) identified the equivocal nature of the effects of alcohol "on the highway" as being related to (i) the pharmacologic effects of alcohol and (ii) the variable nature of the driving task (i.e., the wide range of driving task demands).

In addition to elucidating the key determinants in predicting alcohol-related impairment, an objective of this work was to review the effects of low-to-moderate BACs ( $\leq 0.100\%$ ) on driving ability. The relevant scientific literature provides confidence that BACs at or in excess of 0.050% produce alcohol-related impairment of driving ability.

Scientifically, there is no evidence for a threshold effect for alcohol. Ogden and Moskowitz (8) reported that "some impairment of performance occurs at the lowest levels that can be measured" with no level at which a sudden transition from unimpaired to impaired expected. Whatever the level of BAC examined, at least some skills can be demonstrated to be significantly impaired (8). A review of 177 studies similarly concluded there is no lower limit of BAC at which impairment does not exist (7). Howat et al. (52) focussed on a statutory limit of 0.050% and concluded that there is "sufficient experimental evidence to demonstrate that BACs of 0.050% and higher can produce impairment of the major components of driver performance for most people." Moskowitz and Fiorentino (117) enumerated the vast scientific literature in this area and determined that 27% of studies reported impairment by 0.039%, 47% by 0.049%; 92% of studies reporting impairment by 0.079%. Similarly, Mercer et al. (118) concluded that "virtually all drivers are substantially impaired" at a BAC of 0.080% in tasks such as braking, steering, lane changing, and judgment, and that these impairments began at BACs as low as 0.020%.

Both the Canadian and American Medical Associations (CMA and AMA, respectively) have responded to the scientific evidence and endorse a legal BAC limit of 0.050% (119,120). Other organizations have issued statements that align with the recommendations of the CMA and the AMA. For example, it has been the long-standing position of the U.S. National Safety Council Subcommittee on Alcohol and Other Drugs (121) that "every person, regardless of that person's previous experience with alcoholic beverage consumption is impaired in driving performance if that person's alcohol concentration is 0.08 g of alcohol per 210 L of breath (0.080%) or more." The American College of Emergency Physicians (122,123) has declared that all

drivers are impaired at a BAC of 0.08% and that a BAC of 0.05% is presumptive evidence of impaired driving. More recently, the Centre for Addiction and Mental Health, in Toronto, Canada (124) stated, "it has become clear that the effects of alcohol on performance can begin with the first drink and are measurable at BACs of 20 mg% (0.020%) and lower. Thus there is no question that at 50 mg% (0.050%) driving skills are significantly impaired." These opinions are fully supported by critical review of the scientific literature.

It is uncontroversial that alcohol-related impairment is concentration dependent; the magnitude of impairment increases with increasing BAC. As demonstrated by the present review, there is convergent evidence, therefore a high degree of scientific confidence, in support of the conclusion that a BAC of 0.050% impairs faculties required in the operation of a motor vehicle. This conclusion notwithstanding, the present review also demonstrates that impairment may occur at BACs  $< 0.050\%$ . Whether impairment is apparent depends upon the complexity of the driving task; however, because driving is an inherently complex behavior it can be asserted scientifically that the faculties required in the operation of a motor vehicle will be impaired at a BAC of 0.050% or greater.

#### Acknowledgments

The assistance of M. Brenton and the Centre of Forensic Sciences Library staff in retrieving numerous references is gratefully recognized. The efforts of J.G. Wigmore in founding the Centre of Forensic Sciences alcohol literature database are also acknowledged.

#### References

1. Editorial. *Quart J Inebr* 1904;26:308–9.
2. Mann RE, Macdonald S, Stoduto G, Bondy S, Jonah B, Shaikh A. The effects of introducing or lowering legal per se blood alcohol limits for driving: an international review. *Accid Anal Prev* 2001;33(5):569–83.
3. National Highway Traffic Safety Administration. Traffic Safety Facts (2007 Data): alcohol-impaired driving. Washington, DC: US Department of Transportation, National Highway Traffic Safety Administration, 2008; Report No.: DOT HS 810 985.
4. Moskowitz H, Burns MM, Williams AF. Skills performance at low blood alcohol levels. *J Stud Alcohol* 1985;46(6):482–5.
5. Moskowitz H, Robinson C. Driving-related skills impairment at low blood alcohol levels. In: Noordzij PC, Roszbach R, editors. Alcohol, drugs and traffic safety T86. Proceedings of the 10th International Conference on Alcohol, Drugs and Traffic Safety; 1986 Sep 9–12; Amsterdam, Netherlands. Amsterdam, the Netherlands: Elsevier Science, 1987;79–87.
6. Carpenter JA. Effects of alcohol on some psychological processes: a critical review with special reference to automobile driving skill. *Q J Stud Alcohol* 1962;23:274–314.
7. Moskowitz H, Robinson CD. Effects of low doses of alcohol on driving-related skills: a review of the evidence. Washington, DC: US Department of Transportation, National Highway Traffic Safety Administration, 1988; Report No.: DOT HS 807 280.
8. Ogden EJ, Moskowitz H. Effects of alcohol and other drugs on driver performance. *Traffic Inj Prev* 2004;5(3):185–98.
9. Ferrara SD, Zancaner S, Giorgetti R. Low blood alcohol concentrations and driving impairment. A review of experimental studies and international legislation. *Int J Legal Med* 1994;106(4):169–77.
10. Holloway FA. Low-dose alcohol effects on human behaviour and performance. *Alcohol Drugs Driving* 1995;11:39–56.
11. Kerr JS, Hindmarch I. The effects of alcohol alone or in combination with other drugs on information processing, task performance and subjective responses. *Hum Psychopharmacol* 1998;13(1):1–9.
12. R. v. *Stellato*. *CarswellOnt* 74, 18 C.R. (4th) 127, 43 M.V.R. (2d) 120 (Ont. C.A.), affirmed 1994 *CarswellOnt* 1159, 1994 *CarswellOnt* 84, 3 M.V.R. (3d) 1, 31 C.R. (4th) 60 (SCC); <http://canlii.org/226-01-232>

13. Moskowitz H. Alcohol effects and driver impairment. In: Garriott JC, editor. *Medical-legal aspects of alcohol*, 4th edn. Tucson, AZ: Lawyers and Judges Publishing Co., Inc., 2003;253–68.
14. Maylor EA, Rabbitt PMA, James GH, Kerr SA. Effects of alcohol and extended practice on divided-attention performance. *Percept Psychophys* 1990;48(5):445–52.
15. Evans MA, Martz R, Rodda BE, Kiplinger GF, Forney RB. Quantitative relationship between blood alcohol concentration and psychomotor performance. *Clin Pharmacol Ther* 1974;15(3):253–60.
16. Moskowitz H, Burns M. Effects of alcohol on driving performance. *Alcohol Health Res World* 1990;14(1):12–4.
17. Verster JC, Wester AE, Goorden M, van Wieringen J-P, Olivier B, Volkerts ER. Novice drivers' performance after different alcohol dosages and placebo in the divided-attention steering simulator (DASS). *Psychopharmacology* 2009;204(1):127–33.
18. Moskowitz H, Burns M, Fiorentino D, Smiley A, Zador P. Driver characteristics and impairment at various BACs. Washington, DC: US Department of Transportation, National Highway Traffic Safety Administration, 2000; Report No.: DOT HS 809 075.
19. Vuchinich RE, Sobell MB. Empirical separation of physiologic and expected effects of alcohol on complex perceptual motor performance. *Psychopharmacology* 1978;60(1):81–5.
20. Landauer AA, Howat P. Low and moderate alcohol doses, psychomotor performance and perceived drowsiness. *Ergonomics* 1983;26(7):647–57.
21. Wilkinson CJ, Moskowitz H. Acute effects of loratadine, diphenhydramine and placebo, alone and with alcohol, on skill performance. In: Perrine MWB, editor. *Alcohol, drugs and traffic safety T89. Proceedings of the 11th International Conference on Alcohol Drugs and Traffic Safety*; 1989 Oct 24–27; Chicago, IL. Itasca, IL: National Safety Council, 1990;476–82.
22. Grant SA, Millar K, Kenny GN. Blood alcohol concentration and psychomotor effects [published erratum appears in *Br J Anaesth* 2001;86(2):302]. *Br J Anaesth* 2000;85(3):401–6.
23. Tzambazis K, Stough C. Alcohol impairs speed of information processing and simple and choice reaction time and differentially impairs high-order cognitive abilities. *Alcohol Alcohol* 2000;35(2):197–201.
24. Liu Y-C, Ho CH. Effects of different blood alcohol concentrations and post-alcohol impairment on driving behavior and task performance. *Traffic Inj Prev* 2010;11(4):334–41.
25. Vermeeren A, O'Hanlon JF. Fexofenadine's effects, alone and with alcohol, on actual driving and psychomotor performance. *J Allergy Clin Immunol* 1998;101(3):306–11.
26. Breitmeier D, Seeland-Schulze I, Hecker H, Schneider U. The influence of blood alcohol concentrations of around 0.03% on neuropsychological functions—a double-blind placebo-controlled investigation. *Addict Biol* 2007;12(2):183–9.
27. Cheng S-Y, Lee H-Y, Lee J-C, Tsai S-Y. Comparing the effects of light alcohol consumption on human response to auditory and visual stimuli. *Percept Mot Skills* 2010;111(2):589–607.
28. Gengo FM, Gabos C, Straley C, Manning C. The pharmacodynamics of ethanol: effects on performance and judgment. *J Clin Pharmacol* 1990;30(8):748–54.
29. Maylor EA, Rabbitt PMA, James GH, Kerr SA. Effects of alcohol, practice and task complexity on reaction time distributions. *Q J Exp Psychol A* 1992;44(1):119–39.
30. Mead LC. The effects of alcohol on two performances of different intellectual complexity. *J Gen Psychol* 1939;21:3–23.
31. Drew GC, Colquhoun WP, Long HA. Effect of small doses of alcohol on a skill resembling driving. *Br Med J* 1958;2(5103):993–9.
32. Huemer AK, Vollrath M. Alcohol-related impairment in the lane change task. *Accid Anal Prev* 2010;42(6):1983–8.
33. Veldstra JL, Brookhuis KA, de Waard D, Molmans BHW, Verstraete AG, Skopp G, et al. Effects of alcohol (BAC 0.5‰) and ecstasy (MDMA 100 mg) on simulated driving performance and traffic safety. *Psychopharmacology* 2012;222(3):377–90.
34. Mets MAJ, Kuipers Ede, Senerpont Domis LM, Leenders M, Olivier B, Verster JC. Effects of alcohol on highway driving in the STISIM driving simulator. *Hum Psychopharmacol* 2011;26(6):434–9.
35. Rakauskas ME, Ward NJ, Boer ER, Bernat EM, Cadwallader M, Patrick CJ. Combined effects of alcohol and distraction on driving performance. *Accid Anal Prev* 2008;40(6):1742–9.
36. Fillmore MT, Blackburn JS, Harrison ELR. Acute disinhibiting effects of alcohol as a factor in risky driving behavior. *Drug Alcohol Depend* 2008;95(1–2):97–106.
37. Rupp TL, Acebo C, Seifer R, Carskadon MA. Effects of a moderate evening alcohol dose. II: performance. *Alcohol Clin Exp Res* 2007;31(8):1365–71.
38. Harrison ELR, Fillmore MT. Alcohol and distraction interact to impair driving performance. *Drug Alcohol Depend* 2011;117(1):31–7.
39. Adams AJ, Brown B. Alcohol prolongs time course of glare recovery. *Nature* 1975;257(5526):481–3.
40. Holdstock L, de Wit H. Ethanol impairs saccadic and smooth pursuit eye movements without producing self-reports of sedation. *Alcohol Clin Exp Res* 1999;23(4):664–72.
41. Fransson P-A, Modig F, Patel M, Gomez S, Magnusson M. Oculomotor deficits caused by 0.06% and 0.10% blood alcohol concentrations and relationship to subjective perception of drunkenness. *Clin Neurophysiol* 2010;121(12):2134–42.
42. Buikhuizen W, Jongman R. Traffic perception under the influence of alcohol. *Q J Stud Alcohol* 1972;33(3):800–6.
43. Erwin CW, Wiener EL, Linnoila MI, Truscott TR. Alcohol-induced drowsiness and vigilance performance. *J Stud Alcohol* 1978;39(3):505–16.
44. Linnoila M, Erwin CW, Ramm D, Cleveland WP. Effects of age and alcohol on psychomotor performance of men. *J Stud Alcohol* 1980;41(5):488–95.
45. Rohrbaugh JW, Stapleton JM, Parasuraman R, Frowein HW, Adinoff B, Varner JL, et al. Alcohol intoxication reduces visual sustained attention. *Psychopharmacology* 1988;96(4):442–6.
46. Koelega HS. Alcohol and vigilance performance: a review. *Psychopharmacology* 1995;118(3):233–49.
47. Klein RH, Jex HR. Effects of alcohol on a critical tracking task. *J Stud Alcohol* 1975;36(1):11–20.
48. Dawson D, Reid K. Fatigue, alcohol and performance impairment. *Nature* 1997;388(6639):235.
49. Kuypers KPC, Samyn N, Ramaekers JG. MDMA and alcohol effects, combined and alone, on objective and subjective measures of actual driving performance and psychomotor function. *Psychopharmacology* 2006;187(4):467–75.
50. West R, Wilding J, French D, Kemp R, Irving A. Effect of low and moderate doses of alcohol on driving hazard perception latency and driving speed. *Addiction* 1993;88(4):527–32.
51. Deery HA, Love AW. The effect of a moderate dose of alcohol on the traffic hazard perception profile of young drink-drivers. *Addiction* 1996;91(6):815–27.
52. Howat P, Sleet D, Smith I. Alcohol and driving: is the 0.05% blood alcohol concentration limit justified? *Drug Alcohol Rev* 1991;10(2):151–66.
53. Perrine MWB. Rapprochement between epidemiology and experimentation concerning driving-after-drinking problems. In: Utzelmann HD, Berghaus G, Kroj G, editors. *Alcohol, drugs and traffic safety T92. Proceedings of the 12th International Conference on Alcohol, Drugs and Traffic Safety*; 1992 Sep 28–Oct 2; Cologne, Germany. K In, Germany: Verl. TÜV Rheinland, 1993;809–24.
54. Starmer GA. Effects of low to moderate doses of ethanol on human driving-related performance. In: Crow KE, Batt RD, editors. *Human metabolism of alcohol: Vol I, pharmacokinetics, medicolegal aspects, and general interests*. Boca Raton, FL: CRC Press, 1989;101–30.
55. Laurell H. Effects of small doses of alcohol on driver performance in emergency traffic situations. *Accid Anal Prev* 1977;9:191–201.
56. Huntley MS. Alcohol influences upon closed-course driving performance. *J Safety Res* 1973;5(3):149–64.
57. Cohen J, Dearnaley EJ, Hansel CEM. The risk taken in driving under the influence of alcohol. *Br Med J* 1958;1(5085):1438–42.
58. Lovibond SH, Bird K. Danger level—the Warwick Farm Project. In: Kiloh LG, Bell DS, editors. *Proceedings of the 29th International Congress on Alcoholism and Drug Dependence*; 1970 Feb; Sydney, Australia. Sydney, NSW, Australia: Butterworths Publishers, 1971; 299–305.
59. Robbe H. Marijuana's impairing effects on driving are moderate when taken alone but severe when combined with alcohol. *Hum Psychopharmacol* 1998;13(Suppl. 2):S70–8.
60. Attwood DA, Williams RD, Madill HD. Effects of moderate blood-alcohol concentrations on closed-course driving performance. *J Stud Alcohol* 1980;41(7):623–34.
61. de Waard D, Brookhuis KA. Assessing driver status: a demonstration experiment on the road. *Accid Anal Prev* 1991;23(4):297–307.
62. Ramaekers JG, Robbe HWJ, O'Hanlon JF. Marijuana, alcohol and actual driving performance. *Hum Psychopharmacol* 2000;15(7):551–8.

63. Louwerens JW, Gloerich ABM, d. Vries G, Brookhuis KA, O'Hanlon JF. The relationship between drivers, blood alcohol concentration (BAC) and actual driving performance during high speed travel. In: Noordzij PC, Roszbach R, editors. Alcohol, drugs and traffic safety T86. Proceedings of the 10th International Conference on Alcohol, Drugs and Traffic Safety; 1986 Sep 9-12; Amsterdam, Netherlands. Amsterdam, the Netherlands: Elsevier Science, 1987;183-6.
64. Gawron VJ, Ranney TA. The effects of alcohol dosing on driving performance on a closed course and in a driving simulator. *Ergonomics* 1988;31(9):1219-44.
65. Vermeeren A, Riedel WJ, van Boxtel MP, Darwish M, Paty I, Patat A. Differential residual effects of zaleplon and zopiclone on actual driving: a comparison with a low dose of alcohol. *Sleep* 2002;25(2):224-31.
66. Kearney SA, Guppy A. The effects of alcohol on speed perception in a closed-course driving situation. *J Stud Alcohol* 1988;49(4):340-5.
67. Burian SE, Ligouri A, Robinson JH. Effects of alcohol on risk-taking during simulated driving. *Hum Psychopharmacol* 2002;17(3):141-50.
68. Parks V, Leister C, Patat A, Troy S, Vermeeren A, Volkerts ER, et al. Effects of ethanol at a blood alcohol concentration of 0.4 g/L on actual driving and memory (Abstract). *Eur Neuropsychopharmacol* 2002;12 (Suppl. 3):S432-3.
69. Milner G. Drugs and Driving: a survey of the relationship of adverse drug reactions, and drug-alcohol interaction, to driving safety. In: Avery GS, editor. Monographs on drugs. Vol. 1. Basel, Switzerland: S. Karger, 1972;52.
70. Borkenstein RF, Crowther RF, Shumate RP, Ziel WB, Zylman R. The role of the drinking driver in traffic accidents (The Grand Rapids Study). *Blutalkohol* 1974;11(Suppl. 1):1-131.
71. Krüger HP, Vollrath M. The alcohol-related accident risk in Germany: procedure, methods and results. *Accid Anal Prev* 2004;36(1):125-33.
72. Blomberg RD, Peck RC, Moskowitz H, Burns M, Fiorentino D. The Long Beach/Fort Lauderdale relative risk study. *J Safety Res* 2009;40 (4):285-92.
73. Zador PL, Krawchuk SA, Voas RB. Alcohol-related relative risk of driver fatalities and driver involvement in fatal crashes in relation to driver age and gender: an update using 1996 data. *J Stud Alcohol* 2000;61(3):387-95.
74. Phillips DP, Brewer KM. The relationship between serious injury and blood alcohol concentration (BAC) in fatal motor vehicle accidents: BAC = 0.01% is associated with significantly more dangerous accidents than BAC = 0.00%. *Addiction* 2011;106(9):1614-22.
75. Connor J, Norton R, Ameratunga S, Jackson R. The contribution of alcohol to serious car crash injuries. *Epidemiology* 2004;15(4):337-44.
76. Hurst PM, Harte D, Frith WJ. The Grand Rapids dip revisited. *Accid Anal Prev* 1994;26(5):647-54.
77. Hingson R, Heeren T, Winter M. Lowering state legal blood alcohol limits to 0.08%: the effect on fatal motor vehicle crashes. *Am J Public Health* 1996;86(9):1297-9.
78. Hingson R, Heeren T, Winter M. Effects of recent 0.08% legal blood alcohol limits on fatal crash involvement. *Inj Prev* 2000;6(2):109-14.
79. Fell JC, Voas RB. The effectiveness of reducing illegal blood alcohol concentration (BAC) limits for driving: evidence for lowering the limit to 0.05 BAC. *J Safety Res* 2006;37(3):233-43.
80. Flanagan NG, Strike PW, Rigby CJ, Lochridge GK. The effects of low doses of alcohol on driving performance. *Med Sci Law* 1983;23 (3):203-8.
81. Ryan GA, Legge M, Rosman D. Age related changes in drivers' crash risk and crash type. *Accid Anal Prev* 1998;30(3):379-87.
82. Grabowski DC, Morrissey MA. The effect of state regulations on motor vehicle fatalities for younger and older drivers: a review and analysis. *Milbank Q* 2001;79(4):517-45.
83. Grabowski DC, Campbell CM, Morrissey MA. Elderly licensure laws and motor vehicle fatalities. *JAMA* 2004;291(23):2840-6.
84. National Highway Traffic Safety Administration. Traffic Safety Facts 2000: a compilation of motor vehicle crash data from the fatality analysis reporting system and the general estimates system. Washington, DC: US Department of Transportation, National Highway Traffic Safety Administration, 2001; Report No.: DOT HS 809 337.
85. Wilson AS, Barboriak JJ, Kass WA. Effects of alcoholic beverages and congeners on psychomotor skills in old and young subjects. *Q J Stud Alcohol* 1970;5(Suppl. 5):115-29.
86. Mayhew DR, Simpson HM, Ferguson SA. Collisions involving senior drivers: high-risk conditions and locations. *Traffic Inj Prev* 2006;7 (2):117-24.
87. Quillian WC, Cox DJ, Kovatchev BP, Phillips C. The effects of age and alcohol intoxication on simulated driving performance, awareness and self-restraint. *Age Ageing* 1999;28(11):59-66.
88. Tupler LA, Hege S, Ellinwood EH Jr. Alcohol pharmacodynamics in young-elderly adults contrasted with young and middle-aged subjects. *Psychopharmacology* 1995;118(4):460-70.
89. Mayhew DR, Beirness DJ, Donelson AC, Simpson HM. Why are young drinking drivers at greater risk of collision? In: Benjamin T, editor. Young drivers impaired by alcohol and drugs. Royal Society of Medicine International Congress and Symposium Series No. 116. Amsterdam, the Netherlands: Royal Society of Medicine Services Ltd., 1987;64-71.
90. Ferguson SA. Other high-risk factors for young drivers—how graduated licensing does, doesn't, or could address them. *J Safety Res* 2003;34(1):71-7.
91. Peck RC, Gebers MA, Voas RB, Romano E. The relationship between blood alcohol concentration (BAC), age, and crash risk. *J Safety Res* 2008;39(3):311-9.
92. Phebo L, Dellinger AM. Young driver involvement in fatal motor vehicle crashes and trends in risk behaviors, United States, 1988-95. *Inj Prev* 1988;4(4):284-7.
93. Williams AF, West BA, Shults RA. Fatal crashes of 16- to 17-year-old drivers involving alcohol, nighttime driving, and passengers. *Traffic Inj Prev* 2012;13(1):1-6.
94. Hingson R, Heeren T, Winter M. Lower legal blood alcohol limits for young drivers. *Public Health Rep* 1994;109(6):738-44.
95. Miller MA, Weafer J, Fillmore MT. Gender differences in alcohol impairment of simulated driving performance and driving-related skills. *Alcohol Alcohol* 2009;44(6):586-93.
96. Friedman TW, Robinson SR, Yelland GW. Impaired perceptual judgment at low blood alcohol concentrations. *Alcohol* 2011;45(7):711-8.
97. Zador PL. Alcohol-related relative risk of fatal driver injuries in relation to driver age and sex. *J Stud Alcohol* 1991;52(4):302-10.
98. Harrison ELR, Fillmore MT. Are bad drivers more impaired by alcohol? Sober driving precision predicts impairment from alcohol in a simulated driving task. *Accid Anal Prev* 2005;37(5):882-9.
99. Vogel-Sprott M. Alcohol tolerance and social drinking: learning the consequences. New York, NY: The Guilford Press, 1992.
100. Coldwell BB, Penner DW, Smith HW, Lucas GHW, Rodgers RF, Darroch F. Effect of ingestion of distilled spirits on automobile driving skill. *Q J Stud Alcohol* 1958;19(4):590-616.
101. King AC, Byars JA. Alcohol-induced performance impairment in heavy episodic and light social drinkers. *J Stud Alcohol* 2004;65 (1):27-36.
102. DeLuca L. Alcohol tolerance and its significance in driving. *Can Soc Forensic Sci J* 1985;18(1):1-23.
103. Brumback T, Cao D, King A. Effects of alcohol on psychomotor performance and perceived impairment in heavy binge social drinkers. *Drug Alcohol Depend* 2007;91(1):10-7.
104. Marczinski CA, Harrison ELR, Fillmore MT. Effects of alcohol on simulated driving and perceived driving impairment in binge drinkers. *Alcohol Clin Exp Res* 2008;32(7):1329-37.
105. Bernosky-Smith KA, Shannon EE, Roth AJ, Liguori A. Alcohol effects on simulated driving in frequent and infrequent binge drinkers. *Hum Psychopharmacol* 2011;26(3):216-23.
106. Hurst PM, Bagley SK. Acute adaptation to the effects of alcohol. *Q J Stud Alcohol* 1972;33(2):358-78.
107. Weafer J, Fillmore MT. Acute tolerance to alcohol impairment of behavioral and cognitive mechanisms related to driving: drinking and driving on the descending limb. *Psychopharmacology* 2012;220 (4):697-706.
108. Levine B, Smialek JE. Status of alcohol absorption in drinking drivers killed in traffic accidents. *J Forensic Sci* 2000;45(11):3-6.
109. Fogarty JN, Vogel-Sprott M. Cognitive processes and motor skills differ in sensitivity to alcohol impairment. *J Stud Alcohol* 2002;63 (4):404-11.
110. Schweizer TA, Vogel-Sprott M, Dixon MJ, Jolicoeur P. The stage-specific effect of alcohol on human information processing. *Psychopharmacology* 2005;178(1):52-7.
111. Cromer JR, Cromer JA, Maruff P, Snyder PJ. Perception of alcohol intoxication shows acute tolerance while executive functions remain impaired. *Exp Clin Psychopharmacol* 2010;18(4):329-39.
112. Marczinski CA, Fillmore MT. Acute alcohol tolerance on subjective intoxication and simulated driving performance in binge drinkers. *Psychol Addict Behav* 2009;23(2):238-47.



113. Ferreira SE, de Mello MT, Pompéia S, de Souza-Formigoni ML. Effects of energy drink ingestion on alcohol intoxication. *Alcohol Clin Exp Res* 2006;30(4):598–605.
114. Marczinski CA, Fillmore MT, Henges AL, Ramsey MA, Young CR. Effects of energy drinks mixed with alcohol on information processing, motor coordination and subjective reports of intoxication. *Exp Clin Psychopharmacol* 2012;20(2):129–38.
115. Howland J, Rohsenow DJ, Arnedt JT, Bliss CA, Hunt SK, Calise TV, et al. The acute effects of caffeinated versus non-caffeinated alcoholic beverage on driving performance and attention/reaction time. *Addiction* 2011;106(2):335–41.
116. Verster JC, Roth T. Drivers can poorly predict their own driving impairment: a comparison between measurements of subjective and objective driving quality. *Psychopharmacology* 2012;219(3):775–81.
117. Moskowitz H, Fiorentino D. A review of the literature on the effects of low doses of alcohol on driving-related skills. Washington, DC: US Department of Transportation, National Highway Traffic Safety Administration, 2000; Report No.: DOT HS 809 028.
118. Mercer SL, Sleet DA, Elder RW, Cole KH, Shults RA, Nichols JL. Translating evidence into policy: lessons learned from the case of lowering the legal blood alcohol limit for drivers. *Ann Epidemiol* 2010;20(6):412–20.
119. CMA demands lower legal blood-alcohol level. *CMAJ* 2002;166:79.
120. Council on Scientific Affairs. Alcohol and the driver. *JAMA* 1986;255(4):522–7.
121. Harding P, Forney R Jr. Report of the subcommittee on alcohol: technology, pharmacology and toxicology. Committee Statement on Driving Impairment at Alcohol Concentrations of 0.08 and Above. Orlando, FL: Subcommittee on Alcohol and Other Drugs, National Safety Council, 1999.
122. American College of Emergency Physicians. Blood alcohol concentration and driving. *Ann Emerg Med* 1988;17(11):1252.
123. American College of Emergency Physicians. Guidelines for undergraduate education in emergency medicine. *Ann Emerg Med* 1997;29:835.
124. Centre for Addiction and Mental Health, Mann R. Reducing alcohol-related deaths on Canada's roads. Ottawa, ON: Standing Committee on Justice and Human Rights, 2008.

Additional information and reprint requests:

Teri L. Martin, M.Sc.  
 Toxicology Section  
 Centre of Forensic Sciences  
 25 Morton Shulman Ave  
 Toronto  
 Ontario  
 Canada  
 E-mail: [teri.martin@ontario.ca](mailto:teri.martin@ontario.ca)



From: nicole green  
Sent: Monday, February 24, 2025 1:59 PM  
To: Claire Ricker  
Subject: Regarding development of Brewery at 15 Ryder Street

To whom it may concern:

I am writing to state my concerns about problems that exist as well as potential issues this development could increase. I am not opposed to the proposed business but there are several concerns I think need to be noted .

Ryder street allows cyclists and pedestrians access to the bike path but there is much congestion on the street creating safety concerns. Visibility of those on foot or bike is high due to large landscaping vehicles moving about and parked, unmanned recycling dumpsters in the street (3 in total), parked cars for residents on opposite sides of the street and the drive from the Artemis exiting on Ryder Street. This existing issue will be complicated by increased people to the area by all methods. The concern is that the congestion could lead to accidents and injuries. There is an additional concern about patrons cutting through the Artemis driveway increasing potential injury as there are many on foot with animals and children and cyclists complicating a very dense collection of residents. And proper signage to prevent mis-entry or improper use of the roadways and paths in the areas as it is not always clear.

With outdoor service areas there is an increase of both light and noise to the residents in the neighborhood and less privacy. It should be noted that prior businesses were not operating late into the evening. The concern about noise is due to hours of operation and also delivery times which should not interfere with people having a neighborhood where noise is not interfering with sleep or appropriate peace and quiet. Our building has quiet hours which is very important to myself and other residents. Despite the area having an industrial purpose it is in a dense residential area. We all need to be good neighbors to each other for everyone to be successful and happy with residing in this area as a business or resident.

Parking is a concern so ensuring patrons of the business know where to park and not to park so not to cause use disruptions as well.

The last concern is related to environmental issues. As this is a highly densely populated area there is a concern for increased smoking on nonsmoking residential properties , increased trash and need for proper disposal to prevent increased rodents , and lastly loitering on adjacent properties .

Thank you for hearing my concerns  
Nicole Green  
Artemis resident

**From:** Laura Wiener

**Sent:** Tuesday, February 11, 2025 17:07

**To:** Rachel Zsembery; Kin Lau; Eugene Benson; Shaina Korman-Houston; Stephen Revilak

**Cc:** Claire Ricker

**Subject:** ARB meeting 2/10

Dear ARB members,

The 2/10 ARB meeting was so discouraging to me and other supporters of affordable housing. The Board completely shut down any discussion of an Affordable Housing Overlay on its merits, because of a procedural issue. My disappointment is that there was no discussion of possible solutions to the problem regarding notice of a public hearing. Did anyone look at whether this had ever come up before, here or in any other community? Does this mean that there can never be a citizen initiative for rezoning? I really object to the way discussion was swiftly shut down without any discussion of options or potential solutions.

Also, I disagree with the Chair's characterization that Town Meeting has spoken, that there should be no loss of any commercial or industrial space in town. I believe that a few have spoken many times, but there has never been a full discussion on that point, nor a consensus reached nor vote taken. But one thing I have heard many times over many years is strong and consistent support for affordable housing. So, given the choice, many Town Meeting Members might choose to encourage more affordable housing in underutilized commercial and industrial property. But now that discussion will have to wait.

Laura Wiener

73 Jason St.

TMM P.8

**From:** Mellela Marx  
**Sent:** Friday, February 14, 2025 11:10  
**To:** Rachel Zsembery  
**Subject:** Concerns about AHO plans

Hi Rachel,

My name is Mellela Marx. I am a long time Arlington resident and home owner for over 20 years. I attended your last meeting on Monday, Feb. 10 when a draft of the Affordable Housing Overlay was presented to your board and have deep concerns about what was presented.

Although I am sympathetic to wanting to create more affordable housing in Arlington, I think allowing such developments in the entire town will completely change the character of the town. It will only add to the challenges the town is already having with parking, services and schools in a community where we are already having overrides every year just to keep up with the current population needs.

The specific amendments requested to the zoning seem quite draconian from allowing 2 stories above existing zoning, reducing setbacks, FAR and open space requirements while not allowing for adequate off street parking (especially in places not close to public transportation). Allowing these types of projects by right, with no consideration for the surrounding community, seems quite ridiculous. I also don't see how, in most circumstances, these projects will be of adequate size to meet affordable housing needs and, therefore, would not be solving the problem they claim it would be solving. I'm afraid it would simply open the doorway for unbridled dense development in Arlington driving costs up not down and with no public infrastructure in place to support it.

Furthermore, I'm not sure why we are jumping to town-wide affordable housing projects when we have barely even started to implement the MBTA overlay? I think this alone will have a huge impact on our community and the areas that are the most logical areas for affordable housing since they are close to public transport.

Lastly, I would hope that any such drastic changes to zoning, especially when it affects businesses and/or extends into the residential neighborhoods, would involve more robust public engagement outlining clearly in layman's terms how this will actually change the town and putting it to a town-wide vote, rather than being crafted in private and only voted on in town meeting.

We moved to Arlington because it is a small but vibrant town with parks and open space and NOT a dense city. I believe these proposed changes to the zoning would completely change the character of the town and the community and hope you and the board will consider this before you make changes to the zoning that are irreversible.

Thanks so much for your attention to this matter.

Best,  
Mellela Marx

Mellela Marx | mellela@marxfertik.com | +1(617)229-5148 | [www.marxfertik.com](http://www.marxfertik.com)